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AGENDA

Planning Committee Meeting to be held
at Darebin Civic Centre,
350 High Street Preston
on Monday, 15 April 2019
at 6.00pm.

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Agenda

1. MEMBERSHIP

Cr. Susan Rennie (Mayor) (Chairperson)

Cr. Steph Amir

Cr. Gaetano Greco

Cr. Tim Laurence

Cr. Kim Le Cerf

Cr. Trent McCarthy

Cr. Lina Messina

Cr. Susanne Newton (Deputy Mayor)

Cr. Julie Williams

2. APOLOGIES

3. DISCLOSURES OF CONFLICTS OF INTEREST

4. CONFIRMATION OF THE MINUTES OF PLANNING COMMITTEE

Recommendation

That the Minutes of the Planning Committee Meeting held on 12 March 2019 be confirmed as a correct record of business transacted.

5. CONSIDERATION OF REPORTS

5.1 DEVELOPMENT PLAN POD/1/2007/E 1056-1140 AND 1142 PLENTY ROAD, BUNDOORA (POLARIS - FORMER LARUNDEL PSYCHIATRIC HOSPITAL)

Author: Principal Planner

Reviewed By: General Manager City Sustainability and Strategy

Applicant	Owner	Consultant
SJB Planning	<i>Deal Corporation P/L (Dealcorp)</i>	SJB Planning Clarke Hopkins Clarke Architects Best Hooper Lawyers

SUMMARY

- It is proposed to amend the ‘Polaris 3083’ Precinct Development Plan (**PDP**) approved under the provisions of the Development Plan Overlay – Schedule 1.
- The Applicant has initiated proceedings with the Tribunal pursuant to section 149(1) (a) of the *Planning and Environment Act 1987 (Act)*.
- The amendments sought generally relate to the redevelopment of Lot S3 and S9 in the Polaris town centre.
- The site is in the Mixed Use Zone (**MUZ**) and covered by (either wholly or in part) by the Development Plan Overlay – Schedule 1 (**DPO1**) and Heritage Overlay (**HO107**).
- A series of section 173 Agreements apply across the land.
- No submissions have been received in response to the application being advertised.
- The proposal is generally consistent with the provisions of the DPO1, and allows for the substantial retention and refurbishment of the building protected under the HO111.
- It is recommended that the application not be supported.

CONSULTATION:

- Public notice was given via letters sent to surrounding owners and occupiers and signs on respective lots.
- Notice was given directly to VicRoads, Public Transport Victoria, Yarra Trams and the Minister for Education and Training (Latrobe University).
- This application was referred internally to the following Council units for comment/review:
 - Strategic Planning (Urban Design);
 - Public Places and Design;

- Tree Management; and
- Traffic Engineering.

Recommendation

That the amended Polaris Development Plan (amended December 2018) not be supported on the following grounds:

- (1) The height, scale and massing of Building 1B-16 is excessive, fails to respond to its context and will unreasonably impact the desired built form character and amenity of the area.
- (2) The amendments remove a key pedestrian link across Lot S9 between the corner of Plenty Road/Main Drive and the town centre.
- (3) The proposed Polaris Development Plan (amended December 2018) fails to accurately and consistently document key details including:
 - a) The proposed land use of Building 1B-02 shown in Figures 56 and 57;
 - b) The extent (length) of the sheer 7 storey element of Building 1B-01;
 - c) Techniques for activating the Plenty Road frontage where the basement projects notably above natural ground level;
 - d) Treatment of the Galileo Gateway South interface of Lot S9;
 - e) Inconsistencies between the text description and Figure 60 (both Page 44) regarding the use of Building 1B-16;
 - f) Details of the pedestrian environment for the Galileo Gateway South/car park route and the covered pedestrian walkway nominated in Figure 80; and
 - g) Failure to update tree retention/removal diagrams (Figures 81-83) to reflect outcomes clearly expressed elsewhere in the proposed in the amendments.

That officers advise the Tribunal and parties of its position to be relied on at the hearing scheduled for 22 August 2019.

INTRODUCTION AND BACKGROUND

Site History – Lancaster Gate

After the Hospital closed in 1999, Places Victoria (then known as the Urban and Regional Land Corporation (**URLC**)), purchased the Lancaster Estate for redevelopment.

In November 2001, Council approved the Lancaster Gate Development Plan. This plan was prepared by the URLC and divided the Lancaster Estate into three parts; a Residential Precinct (approx. 15.38h hectares), the Village Precinct which included the former hospital buildings and the Mixed Use Precinct (approx. 10.61 hectares).

The Residential Precinct was subsequently subdivided, developed into individual dwelling lots and sold off.

The residual Village Precinct and Mixed-Use Precinct (now Town Centre) were sold by Places Victoria (then VicUrban) via expression of interest in 2006. These precincts now form 'Polaris'.

Polaris

The Polaris 3083 Development Plan (**PDP**) was approved in its current state on 23 September 2009, and subsequently amended 8 October 2012. Development under the PDP commenced in 2010 comprising the mixed use town centre.

This is a general, over-arching document which sets out principles on how the site should be developed including mix of uses, building height, general layout and form, tree retention and removal, car parking and other matters.

Relevant planning permits have issued for the town centre under the PDP include:

- Planning Permit D/240/2012 for the substantive town centre; and
- Planning Permit D/366/2015 for the child care centre within Linacre's Cottage.

Current Application

The current application to amend the PDP was lodged with Council in April 2018.

Notice of the PDP amendment was given in August/September 2018. No objections or submissions were received.

Following a number of meetings between Council officers and the applicant team (generally relating to urban design issues), a revised PDP was submitted to Council in December 2018. The revised PDP resolved officer concerns with respect to Lot S9; however the parties remained at a fundamental impasse with respect to Lot S3.

Given the respective positions of the parties, the Applicant elected to file an appeal against Council's failure to determine the matter rather than continue negotiations regarding Lot S3.

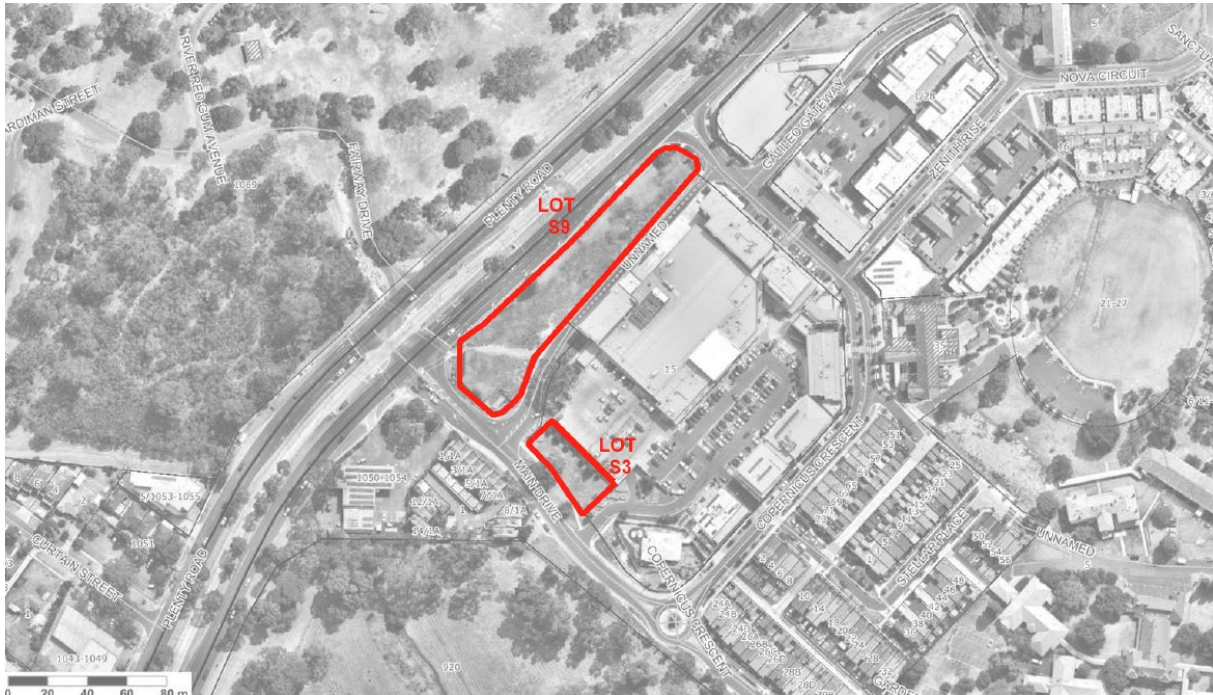
The matter is set down for the following appearances:

- Compulsory Conference – 24 June 2019; and
- Hearing – 22 August 2019 (2 days).

ISSUES AND DISCUSSION

Subject site and surrounding area

- The subject site is the Polaris Site (Lancaster Gate) comprising the former Larundel Psychiatric Hospital in Bundoora.
- The site is currently being redeveloped and comprises two (2) sub-precincts – the Town Centre and the Village Precinct.
- The Town Centre (to which this amendment is relevant) relates to the mixed-use area on the south-west side of the site. The town centre provides a full retail offering including full line supermarket and associated retail, food and drink premises and community services.
- Lot S3 comprises 931 square metres with a 46.48 metre frontage to Main Drive. The rear of the site abuts the two (2) storey car park of the town centre. The land is currently vacant.
- The S9 comprises 3,939 square metres with a 164.35 metre frontage to Plenty Road. The rear of the site abuts Galileo Gateway South. The land is currently vacant.



Proposal

Lot S9 (Plenty Road)

- Increase the height of building 1B-01 from 3 storeys to 6-7 storeys;
- Provision of a four (4) storey street wall condition for a majority of the Plenty Road frontage with a minimum 3 metre setback above. A sheer 7 storey corner element is entertained at the corner of Main Drive;
- Increase the height of building 1B-02 from 2 storeys to 4-5 storeys;
- Reduction of the setback of building 1B-02 to Plenty Road from 4.75-14 metres to 3 metres;
- Provision of two (2) levels of basement car parking accessed from Galileo Gateway South.

Lot S3 (Main Drive)

- Increase the height of building 1B-16 from 2-3 storeys to 6-7 storeys;
- Extend the building envelope at the lower four (4) levels to abut the angled lot boundary to Main Drive;
- Introduction of a basement car park;
- Use of the building defined as ‘student accommodate uses’ as per the proposed revised Figure 60.

Other

- Updated indicative floor area calculations for Precinct 1B;
- Updated indicative parking rates for Precinct 1B.

Objections

No objections have been received in relation to the proposed amendments.

PLANNING ASSESSMENT

The principal requirement of the DPO1 is that the Development Plan should address the elements of the North East Corridor Strategic Plan (**NECSP**) relevant to the site. It is important to note that this plan has been largely superseded by the work undertaken through Amendment C137 for the Plenty Road Corridor, particularly in relation to height controls.

Sub-clause 1.0 of the DPO1 sets out the requirements for what should be included in the development plan, which have been provided for in the amended PDP submitted by the applicant.

Sub-clause 2.0 sets out the guidelines for the consideration of the development plan, including:

The responsible authority will discourage industrial uses from locating on the land.

The responsible authority should consider whether the development plan or permit application is consistent with the North East Corridor Strategic Plan.

Any applicable heritage study and any applicable conservation policy should be considered. Whether the location, bulk, height and appearance of any proposed buildings or works will be in keeping with the character of the area.

Whether any proposed landscaping or removal of vegetation will be in keeping with the character and appearance of adjacent buildings, the streetscape or the area.

The layout and appearance of areas set aside for car parking, access and egress, loading and unloading and the location of any proposed car parking.

Whether subdivision will result in development which is not in keeping with the character and appearance of adjacent buildings, the streetscape or the area will adversely affect the significance, character or appearance of the heritage place.

This NECSP is the current key reference point in the Scheme for the development of this site. The strategic vision of the plan that, amongst other things, the Larundel Urban Village be redeveloped to accommodate medium density residential development with mixed use development fronting Plenty Road (ancillary retail premises outside the local centre).

Varied forms of medium density housing are contemplated, including townhouses and apartments, in order to maintain a level of activity and safety in the area at night. The Larundel Urban Village is also identified as the most appropriate area for student housing – potentially in the heritage buildings. Like uses should face each other across every street.

With regards to vegetation the development should seek to retain as many trees as possible. Exotic trees of importance to the area should also be protected unless they place a significant restriction on development. All development proposals must be based on an accurate survey of existing native trees and provide justification for the removal of any native trees. Where possible, the development should be planned to incorporate trees identified for protection within the public domain. Any trees identified for protection remaining on private lots should be protected by appropriate building siting controls.

The key considerations for the current amendment relates to land use and the proposed built form outcomes on the respective lots.

Lot S9 (Plenty Road)

Lot S9 will contain two (2) buildings – 1B-01 and 1B-02. These share a common basement car park accessed from Galileo Gateway South. It is proposed to amend the land use, building height and building setbacks/massing set out in the approved PDP.

Land Use

The proposed land use composition now indicates the delivery of 12,660 square metres of residential hotel floor area within Precinct 1B. While it not indicated on the indicative sections within the revised PDP, its evident from various guidelines that are sought to be amended this will be located within Building 1B-01. While the PDP presently discourages residential uses from locating adjacent to Plenty Road this has become a common planning outcome in the intervening years between the original approval of the PDP and today. The introduction of a residential hotel within Lot S9 with a frontage to Plenty Road is considered an appropriate land use, subject to details to be resolved in any future planning permit application.

Building 1B-02 will be a commercial office building. This is appropriate to the location of the building within the town centre. Figures 56 and 57 in the proposed PDP should be updated to reflect the land use proposed in the more up-to-date sections on Page 43a.

Built Form

One of the key changes in terms of built form character that has occurred on Plenty Road since the original approval of the PDP is the introduction of the Design and Development Overlay – Schedule 17 (**DDO17**) (informed by the *St Georges Road and Plenty Road Corridors Urban Design Framework 2015 (UDF)*). The DDO17 and UDF contemplate the renewal and redevelopment of the Plenty Road corridor from Preston through to Bundoora. Although the DDO17 does not apply to the Subject Site, Lot S9 unarguably forms part of the Plenty Road corridor (given its immediate abuttal) at the northern bookend.

Building 1B-01 will be the southern-most building on Lot S9 at the intersection of Plenty Road and Main Drive. It is proposed to increase the height of this building from 3 storeys to 6-7 storeys (with the change in height a consequence of topography). In isolation, the proposed height of this building is appropriate given the broader context established by the DDO17 and UDF.

The setbacks of the PDP will be reduced from 3-9.6 metres to effectively zero. This again is consistent with the consistent active street edge requirements of the DDO17 and UDF.

Council's Senior Urban Designer previously expressed concern with what the proposed PDP indicated was a sheer form to the street. In conjunction with the reduced setback noted above, such an outcome would unreasonably erode the quality of the public realm which is already compromised by abuttal to Plenty Road. The revised proposed PDP reserves the sheer 7 storey form at the southern end of the building with levels 5-7 being set back a minimum of 3 metres from the street. This outcome is illustrated in the new sections A-A and B-B of the proposed PDP and results in an outcome that adequately balances the street wall requirements of the DDO17 and UDF with the landmark expectations of the Plenty Road/Main Drive gateway to the activity centre.

For clarity, the proposed PDP ought to set out principles regarding the extent of the sheer corner component of before a setback is provided from the 4 storey street wall.

Building 1B-02 will be increased from 2 storeys to 4 storeys. This height is appropriate for Plenty Road as it will match the desired street wall condition envisaged for the corridor, and also continue the prevailing street wall height of building 1B-01 to the south. The 4.75-11.9 metre setback under the approved PDP will be reduced to bring the building closer to the street edge. A 3 metre setback will be retained to Plenty Road by virtue of an easement in the north-west of Lot S9.

Ground Level Treatment

The existing and proposed PDP provide the following statement with respect of activating the Plenty Road frontage:

A strong built edge that defines the urban block and that punctuates the corners at landmark opportunities is a proper and valid urban design response, that promotes better activation of the street, passive surveillance, increased commercial exposure and more efficient use of land (ie avoidance of 'no man's land').

This outcome will be bolstered by the ground floor retail and food/drink premises offering within building 1B-01.

While this approach is valid and correct, one of the key difficulties with Lot S9 is the rather significant change in topography laterally across its 164 metre length. As a result, both buildings 1B-01 and 1B-02 will have a partial projection of the basement car parking above ground for approximately half of their respective facades to Plenty Road. While this is accepted as an inevitable outcome, the revised PDP is silent on how this situation will be managed in urban design terms (in order to inform a future planning permit application).

The PDP is silent on how the Galileo Gateway South interface of Lot S9 will be treated - for example, whether it is intended to be activated or serve as a back of house environment (given its abuttal to the town centre loading facilities).

Lot S3 (Main Drive)

Lot S3 will contain Building 1B-16 which will front Main Drive and abut the two-level car park to the rear. It is proposed to amend the land use, building height and building setbacks/massing set out in the approved PDP.

Land Use

The text within the PDP continues to state that the building “*may possibly be used for either commercial or residential uses. However, the use of this building is not determined at this stage, and will be proposed at the planning permit stage of the development*”. Notwithstanding, the revised Figure 60 provided by the Applicant indicates that the building will comprise student accommodation uses.

The provision of student accommodation is supportable given the nexus to the adjacent Latrobe University, and also in line with the overall yield calculations for the PDP area. It's noted that a previous amendment to the village precinct removed student accommodation from Lot 8 and the provision of this use within Building 1B-16 would retain the overall mix of uses envisaged.

For consistency, the text within the PDP should be updated to reflect the clear intention of Figure 60.

Built Form

The proposed amendments seek to increase the preferred height of building 1B-16 from 2-3 storeys to 6-7 storeys. While initially proposed as a sheer form to Main Drive, the most recent revision of the proposed PDP indicates a de facto street wall treatment where the south-eastern section of the building at the bottom four (4) levels will protrude out to the tapered lot boundary to present the sense of a street wall.

The applicant purports that this most recent version gives effect to the outcome(s) sought by the DDO17 and the UDF.

While officers agree that the built form outcome should draw its inspiration from the surrounding context, building 1B-16 does not have a frontage to Plenty Road. Notwithstanding, the proposed PDP seeks a height and scale consistent with what is proposed on Lots S9 (discussed previously).

To this end, the built form outcome ought to respond to the more relevant context of Main Drive's local road status, the townhouses constructed in Precinct 1A, Linacre's Cottage, the Latrobe Master Plan and the 2-3 storey townhouses across Main Drive.

Given the above factors, it is difficult (if not impossible) to understand how such a fundamental departure from the surrounding context in terms of the height of scale of built form is justified. While the sleeving of the adjacent car park is a positive urban design outcome of the lots redevelopment, this is achieved by a height of approximately 3 storeys. Main Drive benefits from a number of built form benchmarks which provide a clear height datum to which to base a response. This in combination with the envisaged 3 storey street wall proposed at the Latrobe University interface to Main Drive results in the proposed height and scale of building 1B-16 being excessive and overwhelming to the public realm.

Council's Senior Urban Designer has expressed concerns regarding the proposed height and scale of building 1B-16.

Insofar as the Applicant has projected the lower part of the building outward to align the tapered boundary, such an approach further imposes the built form on the public realm and inhibits any potential view lines to Linacre's Cottage. A rectilinear floor plate as presently approved under the PDP is more appropriate.

The building will be constructed to abut the adjacent car park to the rear. No guidelines are provided in the PDP regarding how this interface will be treated so as to protect the amenity of future occupiers.

While it is Council's role to assess rather than formulate the outcome, it would seem obvious to utilise a 3 storey base/street wall as the starting point. Any additional height ought to be recessed and distinguished from the lower part of the building (noting the relatively shallow depth of the site).

Ground Level Treatment

The proposed PDP continues to refer to "*a continuous landscape treatment along the length of the boundary to create a sensitive interface with the adjoining use and development across Main Drive*". While this is comforting and indeed supported, the street wall protrusion at the south-east end of building 1B-16 and the likely removal of all vegetation make the final outcome somewhat unclear.

The proposed PDP continues to call for an active frontage to Main Drive which is supported.

Other Matters

Pedestrian Link

The approved PDP provides a public access link (over private land) from Plenty Road through to Galileo Gateway South and in turn the town centre beyond. This pedestrian link was retained (albeit relocated further south) in the first proposed PDP submitted by the Applicant.

The proposed PDP currently under review has removed this pedestrian link (see, for example Figure 84). Rather, no pedestrian permeability is proposed through the expansive frontage of Lot S9, and pedestrian access will occur around the southern side via Main Drive. Arriving at the town centre will then be either via continuing along Main Drive or turning up Galileo Gateway South to the existing car park stairs.

The removal of the pedestrian link through Lot S9 is seen as a poor outcome of the proposed PDP and will inhibit movement and integration of the existing town centre with both the proposed redevelopment of Lot S9 and the nearby services/infrastructure.

While a 'covered pedestrian walkway' continues to be noted on the plans (see Figure 80), this appears to a legacy annotation from the proposed PDP originally lodged with Council for consideration.

The proposed PDP under review by the Tribunal is silent on any urban design guidance which will assist in inviting pedestrians to utilise the car park entry which is presently an uninviting environment.

Vegetation

The approved PDP sets out the tree retention and removal across the Polaris development plan area. Many of the trees listed for removal and a number of additional trees have already been removed from the respective lots. In some cases the additional trees removed are street trees that have been removed by Council, while in other instances subsequent approval was sought or the cause of removal is unknown.

On Lot S9, Trees 221 and 260 are the only trees remaining. Tree 260 is already approved for removal under the approved PDP. Tree 221 was noted for retention however upon review by Council's Planning Arborist is now showing signs of declining health. This tree can be removed.

On Lot S3, Trees 195 and 199 are the only trees remaining. Tree 195 is already approved for removal under the approved PDP. Tree 199 remains shown for retention in the revised PDP.

Despite the changes to the tree retention/removal across Lots S3 and S9, the proposed PDP currently under review does not include updated information of same. Namely, Figures 81-83 are unchanged. These key details should be updated to reflect the outcome sought on the ground.

Traffic and Car Parking

The proposed PDP includes vehicle access from the local road network to the relevant lots rather than introducing vehicle access from Plenty Road. This is appropriate.

The proposed PDP seeks to introduce new car parking rates for the new uses not previously accounted for. These are:

- A peak rate of 0.45 spaces per room is recommended for Residential Hotel; and
- A peak rate of 0.2 spaces per room is recommended for Student Accommodation.

With respect of the residential hotel, it's observed that there is no statutory parking rate defined in the Scheme by which to benchmark the proposed empirical rate. In considering the introduction of serviced apartments at 1091 Plenty Road (opposite Polaris) the Tribunal heard evidence that typical rates of 0.3 spaces (inner areas) up to 0.6 spaces (outer areas) per serviced apartment are typical. The Tribunal ultimately accepted a rate of 0.4 spaces per serviced apartments (see *Parc Vue Projects Pty Ltd v Darebin CC (Corrected)* [2017] VCAT 1602).

While there are discrete differences between serviced apartments and a full service residential hotel, the car parking profile is considered consistent across similar temporal or short term accommodation uses. The 0.45 spaces per room rate proposed is considered appropriate.

The rate of 0.2 spaces per room for the student accommodation use is consistent with the rate approved under the Transport Management Plan (February 2014) prepared by *Cardno* and approved under the village precinct VPDP.

In terms of the broader parking supply within the town centre, the proposed PDP noted “[a] review of applicable car parking rates and temporal demand and supply in the town centre will be undertaken at the planning permit stage for each site”.

Housekeeping’ Amendments

The proposed PDP includes a number of amendments which are reasonably considered as ‘housekeeping’ changes to reflect the current state of affairs on the ground. Examples include:

- Updating the floor area of the supermarket to reflect the constructed and operational Woolworths within building 1B-08;
- Update the proposed use for Linacre’s Cottage form Tavern to Childcare Centre to reflect Planning Permit D/366/2015.

These changes are acceptable; however as noted throughout the assessment above, the amended PDP currently under review fails to update a number of other key details to reflect what is proposed. Given there are a number of outcomes known (such as tree removal on Lot S9), the PDP should adequately reflect what is to be undertaken to ensure any future planning permit applications are generally in accordance with the proposed PDP.

REFERRAL SUMMARY

Department/Authority	Response
Strategic Planning (Urban Design)	No objection to Building 1B-01 and 1B-02. Objection to the height, scale and massing of Building 1B-16.
Public Places and Design	Objection to the loss of landscaping relative to the approved PDP, failure of the proposal to improve public realm amenity along Plenty Road, and inadequate details of pedestrian links.
Tree Management	No objection.
Traffic Engineering	No objection. Details to be fully resolved in future planning permit application(s).

PLANNING SCHEME SUMMARY

Darebin Planning Scheme clauses under which a permit is required

- Pursuant to Clause 43.04, a development plan must be prepared to the satisfaction of the Responsible Authority.

Schedule 1 applies to the land and sets out requirements for the development plan (sub-clause 1.0) and guidelines for consideration (sub-clause 2.0).

Applicable provisions of the Darebin Planning Scheme

Section of Scheme	Relevant Clauses
PPF	11.01, 11.02, 11.03, 15.01, 15.02, 15.03, 16.01, 17.01, 17.02, 17.04, 18.01, 18.02, 19.01, 19.02, 19.03, 21.01, 21.02, 21.03, 21.04, 21.05
Zone	32.04
Overlay	43.01, 43.04, 45.06
Particular provisions	52.06
General provisions	65

POLICY IMPLICATIONS

Environmental Sustainability

A Sustainable Management Plan (SMP) will be required with future planning permit applications lodged in accordance with an approved development plan.

Social Inclusion and Diversity

Nil

Other

Nil

FINANCIAL AND RESOURCE IMPLICATIONS

There are no financial or resource implications as a result of the determination of this application.

FUTURE ACTIONS

Nil

RELATED DOCUMENTS

- *Planning and Environment Act 1987* (as amended)
- Darebin Planning Scheme
- Polaris 3083 Development Plan (as amended)

Attachments

- Polaris Development Plan - Amended Documentation (**Appendix A**)
- VCAT Application for Review (**Appendix B**)
- VCAT Initiating Orders (**Appendix C**)

DISCLOSURE OF INTEREST

Section 80C of the *Local Government Act 1989* requires members of Council staff and persons engaged under contract to provide advice to Council to disclose any direct or indirect interest in a matter to which the advice relates.

The Officer reviewing this report, having made enquiries with relevant members of staff, reports that no disclosable interests have been raised in relation to this report.

SJB Planning



Mr Craig Murphy
Principal Planner
City of Darebin
PO Box 91
PRESTON VIC 3072

21 December 2018

**Re: Polaris Development Plan
1056 - 1140 Plenty Road, Bundoora
Amendment to April 2018 Submission**

Dear Craig,

We continue to act for Deal Corporation Pty Ltd (DealCorp), the owner and developer of the land located at 1056-1140 Plenty Road in Bundoora, which is subject to the Polaris Development Plan (PDP), originally endorsed by Council on 23 September 2009 and subsequently, an amended version was endorsed on 21 January 2013.

Following our on-going discussions in relation to feedback received from Council on the PDP, we now seek to make further changes to the amended PDP lodged in April 2018 in response to this feedback. Essentially, the changes proposed under the cover of this letter seek to introduce urban design criteria that are generally consistent with Design & Development Overlay – Schedule 17 (DDO17). As Council is aware, these proposed changes to the PDP are necessary in order to facilitate the proposed development of Lots S3 and S9, the last remaining lots in the Polaris Town Centre. Pursuant to Clause 43.03 of the Darebin Planning Scheme, the endorsed Development Plan may be amended to the satisfaction of the Responsible Authority.

Accordingly, please find enclosed:

- An electronic version (PDF) of the individual pages of the PDP proposed to be amended.

An electronic version (PDF) of the PDP incorporating 2013 amendments and proposed amendments with relevant pages bookmarked can be provided upon request.

The proposed amendments relate to a variety of built form considerations in the Precinct 1B Polaris Town Centre and specifically Buildings 1B-01 and 1B-02 on Lot S9 and Building 01-16 on Lot S3. The amendments are considered in detail below.

For clarity, the table below displays amendments from April 2018 in **blue text**, while the further amendments proposed now (December 2018), are in **red text**. In relation to the PDP pages enclosed, any images amended as part of the December 2018 submission have been clouded and any text has been highlighted in **yellow**.

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Proposed Amendments

With respect to the existing 2009 PDP document (as amended in 2013), a summary of the proposed amendments, listed by page and section heading is provided in the table below.

Table 1 – Summary of Proposed Amendments

Page	Section	Current Wording	Proposed Wording
6	1.2 Scope, Purpose and Objectives of the Polaris 3083 Development Plan After dot points	Except as shown in the amended development plan (lodged July 2012) the layout of Precinct 1A is to be generally in accordance with Figure 49b (page 40) of the Polaris development plan	Except as shown in the amended development plan (lodged July 2012 and December 2018) the layout of Precinct 1A and Precinct 1B is to be generally in accordance with Figure 49b (page 40) of the PDP .
38	Section 4.2.14 Landform and Heights	Buildings have been kept to a maximum of three-storeys so as not to exceed the heights of the existing heritage buildings, and to allow for lower cost domestic construction techniques to be used. Most buildings however are between two and three storeys in height, creating a comfortable human scale.	Buildings have been generally kept to a maximum of three-storeys so as not to exceed the heights of the existing heritage buildings, and to allow for lower cost domestic construction techniques to be used. Most buildings however are between two and three storeys in height, creating a comfortable human scale.
39	Figure 49a	Polaris Development Plan	Replacement of existing figure with respect to building envelopes. Amendment of the building envelopes for 1B-01, 1B-02 and 1B-16.
40	Figure 49b	Polaris Overall Master Plan	Replacement of existing figure with respect to building envelopes, land uses, on-street and basement car parking and setbacks to Main Drive and Plenty Road for Building 1B-16 and Building 1B-01 and 1B-02. Amendment of the building envelopes for 1B-01, 1B-02 and 1B-16.
42	Section 4.4.2 Precinct 1B Table	Land Use Table	Replacement of Land Use Table with updated indicative floorspace for Office and Retail and Residential Hotel.

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Page	Section	Current Wording	Proposed Wording
			Amendment of the floorspace for Showroom and Residential Hotel and correction of the "Total" GFA and NLA calculations.
43	Section 'Plenty Road Frontage' Paragraph 1	It is envisaged that the Plenty Road frontage may be developed with mixed use commercial/retail use and development. The interface of Precinct 1B with Plenty Road will be appropriately treated to create a distinctive character for the new Polaris NAC. Minimal setbacks of Precinct 1B will be encouraged due to the adjacent main arterial road and to achieve an active frontage. Sensitive uses, such as residential, are not considered to be appropriate along this frontage and are discouraged.	It is envisaged that the Plenty Road frontage may be developed with mixed use commercial/retail use and development. The interface of Precinct 1B with Plenty Road will be appropriately treated to create a distinctive character for the new Polaris NAC. Minimal setbacks of Precinct 1B will be encouraged due to the adjacent main arterial road and to achieve an active frontage. Sensitive uses, such as residential but excluding a residential hotel, are not considered to be appropriate along this frontage and are discouraged.
	New Paragraph 2	N/A	The Plenty Road frontage has the potential to support a preferred maximum building height of seven (7) storeys, having regard to local context considerations, including the site's relationship to surrounding development.
	New Paragraph 3	N/A	The functionality of buildings located within Precinct 1B, which forms part of the Polaris NAC, and the layout of the Precinct, have both significantly influenced the front setback of buildings to Plenty Road.
	Paragraph 4	A 3-9 metre front setback from the title boundary along Plenty Road is proposed in addition to 4 metres (from the boundary to the kerb), which equates to a (maximum) 14 metres front setback from Plenty Road (i.e. the kerb), which is generally consistent with	A varied ground level setback of between 0 metres and approximately 5.5 metres from the title boundary along Plenty Road is proposed. The varied setback will create a sense of place and a sense of arrival to the Polaris Town Centre, particularly at the corner intersection of Main Street and Main Drive. The title boundary is set back

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Page	Section	Current Wording	Proposed Wording
		the Lancaster Gate Development Plan Guidelines (1998). The minimum building setback of approximately 4.7 metres from the title boundary is proposed at the corner locations of Precinct 1B to create a sense of place and a sense of arrival to the new Main Street.	4 metres from the Plenty Road kerb.
	Paragraph 5	A strong built edge that defines the urban block and that punctuates the corners at landmark opportunities (with a setback of 3 metres for Building 1B-01) is a proper and valid urban design response, that promotes better activation of the street, passive surveillance, increased commercial exposure and more efficient use of land (i.e. avoidance of 'no man's land')	A strong built edge that defines the urban block and that punctuates the corners at landmark opportunities (with a minimum setback of 3 metres for Building 1B-01) is a proper and valid urban design response, that promotes better activation of the street, passive surveillance, increased commercial exposure and more efficient use of land (i.e. avoidance of 'no man's land')
	Paragraph 6	The Plenty Road setback frontage (along Precincts 1B and 2D) has been designed with the view to creating a setback rhythm of buildings coming forward to mark urban corners and building setbacks in between to accentuate these landmarks. The Plenty Road setback of Precinct 1B varies between 3 metres and approximately 11 metres from the title boundary. Refer to Figure 49b for further details.	The Plenty Road setback frontage (along Precincts 1B and 2D) has been designed with the view to creating a setback rhythm of buildings coming forward to mark urban corners and building setbacks in between to further accentuate these landmarks. The Plenty Road setback of Precinct 1B varies between 0 metres and approximately 5.5 metres from the title boundary. Refer to Figure 49b for details.
	Paragraph 7	Native street trees are proposed for the Plenty Road site frontage. This treatment is a continuation of the existing Plenty Road	Native street trees are proposed for the Plenty Road site frontage, wherever practical . This treatment is a continuation of the existing Plenty Road treatment and

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Page	Section	Current Wording	Proposed Wording
		treatment and complements the native planting environment created by Bundoora Park. It is intended that street trees be planted within the footpath on Plenty Road and wherever possible incorporated into rain gardens that harvest surface run off from the footpath. Trees to the car park on the east side of the commercial buildings will be native species. Refer to the Landscape Plan (Figure 80 for a list of indicative species). Rain gardens are proposed in order to capture street run-off in this area. The walkway from Plenty Road to the car park features small ornamental trees of a pedestrian scale such as Ficus sp.	complements the native planting environment created by Bundoora Park. It is intended that street trees be planted within the footpath on Plenty Road and wherever practical , incorporated into rain gardens that harvest surface run off from the footpath. Trees to the car park on the east side of the commercial buildings will be native species. Refer to the Landscape Plan (Figure 80 for a list of indicative species). Rain gardens are proposed in order to capture street run-off in this area. The walkway from Plenty Road to the car park features small ornamental trees of a pedestrian scale such as Ficus sp.
	Paragraph 8	The adjacent conceptual plan (Figure 56) and cross section (Figure 57) provide a conceptual indication of the form, character, mass and materials of future development as do benchmark images D, E and F.	The adjacent conceptual plan (Figure 56) and cross section (Figure 57 & 57a) provide a conceptual indication of the form, character, mass and materials of future development as do benchmark images D, E and F.
	Figure 55	Location of Figure 56 and 57	Location of Figure 56 and , 57 and 57a. Amendment of the building envelopes for 1B-01, 1B-02 and 1B-16.
	Figure 56	Indicative Plenty Road Interface	Replacement of Figure 56. Amendment of Figure 56 to show indicative building separation.
	Figure 57	Indicative Section through buildings fronting onto Plenty Road	Replacement of Figure 57
43a	New page	Insertion of new page 43a,	This new page provides indicative

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Page	Section	Current Wording	Proposed Wording
		which provides Figure 57a	<p>section plans through buildings 1B-01 and 1B-02 from various angles, under the heading "Indicative Building Sections Figure 57a".</p> <p>Future planning permit applications are to be generally in accordance with Figure 57a.</p>
44	Main Drive Frontage Paragraph 2	<p>A building adjacent to Main Drive (located in the intervening area between Main Drive and the car park) (nominated as Building 1B- 16 in Figure 49a and 59) may possibly be used for either commercial or residential uses. However, the use of this building is not determined at this stage, and will be proposed at the planning permit stage of the development.</p> <p>Opportunities to create an active front to the building will be encouraged and maximised where possible.</p> <p>This building will assist in minimising views of the car park from Main Drive and is envisaged to be single storey, with the future possibility of being two to three levels in height. Appropriate landscape treatment and building circulation of the façade will assist in minimising the visual perception of a blank wall when viewed from Main Drive.</p>	<p>A building adjacent to Main Drive (located in the intervening area between Main Drive and the car park) (nominated as Building 1B-16 in Figure 49a, and 59, 60 and 60a) may possibly be used for either commercial or residential uses. However, the use of this building is not determined at this stage, and will be proposed at the planning permit stage of the development.</p> <p>Opportunities to create an active front to the building will be encouraged and maximised where possible.</p> <p>This building will assist in minimising views of the car park from Main Drive. It has the potential to support a preferred maximum building height of seven (7) storeys, having regard to local context considerations, including the site's relationship to surrounding existing development. is envisaged to be single storey, with the future possibility of being two to three levels in height. Appropriate landscape treatment and building circulation of the façade will assist in minimising the visual perception of a blank wall when viewed from Main Drive.</p>
	Figure 58	Location of Figures 59 and 60	Location of Figures 59, and 60 and 60a.
	Figure 59 Indicative Part Plan of Precinct 1B	Indicative Part Plan of Precinct 1B	<p>Replacement of Figure 59.</p> <p>Update to building envelope on Figure 59.</p>

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Page	Section	Current Wording	Proposed Wording
	Figure 60 Indicative Section through Precinct 1B	Indicative Section through Precinct 1B	Update to building envelope on Figure 60 and modification of annotation as follows: 5 6-7 level building fronting onto Main Drive
44a	New page	Insertion of new page 44a, which provides Figure 60a	This new page provides indicative section plans through building 1B-16 from various angles, under the heading "Indicative Building Sections Figure 60a". Future planning permit applications are to be generally in accordance with Figure 60a.
53	Section 4.7.1 Development Envelope Front Setback	Precinct 1B Plenty Road frontage: Generally between approximately 3 – 11 metres to the title boundary	Generally varying between approximately 0 metres and approximately 5.5 metres to the title boundary at ground level.
	Building Heights	Generally, no building will exceed three storeys in height above ground level. Where it is necessary to exceed a building height of three storeys, for example due to the slope of the land, the proposal must be accompanied by appropriate urban design support to the satisfaction of the responsible authority. Generally, no building will exceed 11.5 metres in height as measured from natural ground level to the satisfaction of the responsible authority. Where the building incorporates a pitched roof, the roof may exceed this height provided it does not contain any habitable space. Where necessary to exceed a building height of 11.5 metres in overall height for example, due to the slope of the land, the proposal must	Generally, no building will exceed three storeys in height above ground level. Where it is necessary to exceed a building height of three storeys, for example due to the slope of the land, the proposal must be accompanied by appropriate urban design support to the satisfaction of the responsible authority. In accordance with Section 3.8 and 3.9, there is an opportunity for greater building height of up to 7 storeys for Building 1B-01 and 5 storeys for 1B-02. Both sites front Plenty Road which has a 42 metre road reserve width and has a designated upper height limit of 6 storeys proximate to the Polaris Estate site under DDO17 and in the Plenty Road Corridor Urban Design Framework. A 6 storey height applies to Lot 2D under the Village Precinct Development Plan. Both Buildings 1B-01 and 1B-02 are also identified as potential new

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Page	Section	Current Wording	Proposed Wording
		<p>be accompanied by appropriate urban design support to the satisfaction of the responsible authority.</p>	<p>landmarks and focal points for the Polaris Town Centre, creating a sense of arrival and place for the activity centre.</p> <p>There is also opportunity for greater building height of up to seven (7) storeys for Building 1B-16, given the topography of the Polaris Estate which slopes down to the south and west.</p> <p>The town centre's lowest topographic values are located along Main Drive and at the Main Drive and Plenty Road intersection.</p> <p>Unless otherwise specified, buildings will not generally exceed 11.5 metres in height as measured from natural ground level to the satisfaction of the responsible authority. Where the building incorporates a pitched roof, the roof may exceed this height provided it does not contain any habitable space.</p> <p>Where an overall building height of 11.5 metres is exceeded, the proposal must be accompanied by appropriate urban design support to the satisfaction of the responsible authority.</p>
60	Figure 80	Landscape Concept Plan	<p>Replacement of existing plan with respect to building envelopes, setbacks, hard and soft public spaces, on-street car parking and landscaping (tree planting).</p> <p>Modification of building envelopes for buildings 1B-01, 1B-02 and 1B-16 and modified alignment of pedestrian link.</p>
64	Figure 84	Public Private Concept Plan – Indicative only	<p>Replacement of existing plan with respect to building envelopes.</p> <p>Modification of building envelopes for buildings 1B-01, 1B-02 and 1B-16 and modified</p>

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Page	Section	Current Wording	Proposed Wording
			alignment of pedestrian link.
76	Section 5.2 Larundel Urban Village Design and Development Considerations 2001 (revised August 2001)	1.1 Residential Student housing (x400 beds). Precinct 2C provides opportunity for the reuse of an existing heritage building for the residential type of student housing. Approximately 125 student housing units are envisaged.	Precinct 2C provides opportunity for the reuse of an existing heritage building for the residential type of student housing. <i>There is also an opportunity for student accommodation in Precinct 1B (e.g. Building 1B-16).</i> Approximately 200 student housing units/beds are envisaged.
77	1.2 Retail	Supermarket 3,000 sqm The floor area for the supermarket is approximately 3,550 square metres.	Supermarket 3,700 sqm The floor area for the supermarket is approximately 3,700 square metres. <i>Above change reflect floor area of completed supermarket.</i>
		Specialty 2,300 sqm (say 20 – 25 shops including newsagent, pharmacy, restaurants (6 x 120 sqm), bakery, medical, hairdresser, video, take-away etc) The total floor area of specialty shops is approximately 3,030 square metres. The Retail and Commercial Economic Assessment (July 2008) prepared by Charter Keck Cramer supports 3,030 square metres of specialty shops on the site.	Specialty 3,700 sqm (say 20 – 25 shops including newsagent, pharmacy, restaurants (6 x 120 sqm), bakery, medical, hairdresser, video, take-away etc) The total floor area of specialty shops is approximately 3,700 square metres. The Retail and Commercial Economic Assessment (July 2008) prepared by Charter Keck Cramer supports 3,030 square metres of specialty shops on the site. <i>Above change reflect floor area of completed supermarket.</i>
		Total site requirement approximately 8,000 – 12,000 sqm. The gross floor area of other retail shops and specialty shops totals 7,680 sqm. The PDP meets this guideline, as	Total site requirement approximately 8,000 – 12,000 sqm. The gross floor area of other retail shops and specialty shops totals 7,680 sqm. The PDP meets this guideline, as the total retail floor area proposed is 12,600 square

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Page	Section	Current Wording	Proposed Wording
		the total retail floor area proposed is 11,790 square metres, which is less than the maximum of 12,000 square metres of this guideline.	metres, which is generally in accordance with less than the maximum of 12,000 square metres of this guideline.
	1.3 Offices	Total 2500sqm There is opportunity within Precinct 1B to accommodate office use. A gross floor area of approximately 5,910 sqm is proposed. Refer to Appendix F.	There is opportunity within Precinct 1B to accommodate office use. A gross floor area of approximately 9,600 sqm is proposed. Refer to Appendix F.
	1.4 Entertainment / Short Term accommodation / recreation	Hotel including bistro(s), function room, takeaway bottle shop (3,000sqm) There is opportunity for a tavern (i.e. hotel use) to be located within Precinct 1B. Including (say) 30-50 rooms/serviced apartments	Hotel including bistro(s), function room, takeaway bottle shop (3,000sqm) There is opportunity for a tavern (i.e. hotel use) to be located within Precinct 1B. There is also opportunity for a residential hotel (12,660 sqm GFA) to be located within Precinct 1B. Including (say) up to 200 rooms/serviced apartments.
78	1.7 Car Parking	Total requirement dependent on project role, function and sizing – estimate 300-400 public spaces plus appropriate on site (private spaces) <ul style="list-style-type: none"> • Supermarket 4.5/100 sqm (public) • Speciality Shop 3.0/100 sqm (public) • Office 3.0/100 sqm (70% on site) • Hotel 50(public) 15 (private) Refer to the Integrated Transport Plan, July 2009	Total requirement dependent on project role, function and sizing – estimate 300-400 public spaces plus appropriate on site (private spaces) <ul style="list-style-type: none"> • Supermarket 4.5/100sqm (public) • Speciality Shop 3.0/100 sqm (public) • Office 3.0/100 sqm (70% on site) • Hotel 50 (public) 15 (private) Refer to the Integrated Transport Plan, July 2009 (Appendix C) prepared by Ratio Consultants for a detailed empirical assessment of the future car parking demand for the site

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Page	Section	Current Wording	Proposed Wording
		<p>(Appendix C) prepared by Ratio Consultants for a detailed empirical assessment of the future car parking demand for the site</p> <p>Ratio estimate a rate of:</p> <ul style="list-style-type: none"> 5.0/100 sqm supermarket 3.0/100 sqm for shops 3.0/100 sqm for offices A peak demand for 0.35 spaces per seat for a tavern. 	<p>Ratio estimate a rate of:</p> <ul style="list-style-type: none"> 5.0/100 sqm supermarket 3.0/100 sqm for shops 3.0/100 sqm for offices A peak demand for 0.35 spaces per seat for a tavern. <p>A rate of 0.35 spaces (evening) and 0.2 spaces per seat (lunch) is recommended for Restaurants.</p> <p>A peak rate of 0.45 spaces per room is recommended for Residential Hotel.</p> <p>A peak rate of 0.2 spaces per room is recommended for Student Accommodation.</p> <ul style="list-style-type: none"> A review of applicable car parking rates and temporal demand and supply in the town centre will be undertaken at the planning permit stage for each site.
79	1.8, 1.9, 1.10, 1.11, 1.12		Reformatting change only.
81	5.3.3 Larundel Urban Village Third dot point	Allows opportunity for student housing to be located within the existing buildings within Precinct 2C	Allows opportunity for student housing to be located within the existing buildings within Precinct 2C or Precinct 1B (e.g. 1B-16)
	Fifth dot point	Provides for opportunity for non- residential uses to be located within existing heritage buildings (e.g. the community centre and the possibility of Linaker's Cottage to be used as a tavern).	Provides for opportunity for non-residential uses to be located within existing heritage buildings (e.g. the community centre and the possibility of Linaker's Cottage to be used as a tavern -child care centre).
82	Section 5.3.4 Building Heights Paragraph 3	Consideration has been given to both the NECSP and the Framework Plan. However, future indicative building heights within	Consideration has been given to both the NECSP and the Framework Plan, as well as to the Plenty Road Corridor Urban Design Framework and incorporation of

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Page	Section	Current Wording	Proposed Wording
		Polaris are also informed by, and are responsive to the physical attributes of the site (such as topography), the proximity of proposed buildings and existing buildings and roads on site and in adjoining areas (such as Lancaster Gate) and the scale, symmetry and height of existing buildings on site.	DDO17 into the Darebin Planning Scheme in June 2017. However, future indicative building heights are also informed by, and are responsive to, the physical attributes of the site (such as topography), the proximity of proposed and future buildings, existing and future buildings context and roads on site and in adjoining areas (such as Lancaster Gate) and the scale, symmetry and height of existing buildings on site.
	Throughout	Numerical values spelt out only, e.g. "six"	Numerical values spelt out and specified by numerals e.g. "six (6)"
	Paragraph 5	The PDP provides the opportunity for a range of building heights on the site. A conceptual indication of these heights is provided in Figure 86. The variety of heights, between one and three storeys (above ground), interspaced across the site, enhances the form of the development. Three storey developments have been constructed in the adjoining Lancaster Gate development site which also varies the building height guidelines identified in The NECSP.	The PDP provides the opportunity for a range of building heights on the site. A conceptual indication of these heights is provided in Figure 86. The variety of heights, Although generally anticipated to be between one and three storeys (above ground), there is capacity for greater overall building height at the south- west of the site, along Plenty Road and Main Drive (1B-01, 1B-02 and 1B-16), interspaced across the site, enhances the form of the development. Three storey developments have been constructed in the adjoining Lancaster Gate development site which also varies the building height guidelines identified in The NECSP.
	Paragraph 7	The indicative building heights shown in Figure 86 have been developed with the objective to respect the significance of the existing heritage buildings. These buildings are generally two storey in height, however are equivalent or comparable in height to a modern three or four storey building due to their high ceiling to floor dimensions. Steep pitched roofs, raised	The indicative building heights shown in Figure 86 have been developed with the objective to respect the significance of the existing heritage buildings. These buildings are generally two storey in height, however are equivalent or comparable in height to a modern three or four storey building due to their high ceiling to floor dimensions. Steep pitched roofs, raised ground floor level above natural ground level and architectural symmetry and scale.

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Page	Section	Current Wording	Proposed Wording
		ground floor level above natural ground level and architectural symmetry and scale. No four storey building above ground is proposed as part of the PDP.	No four-storey building above ground is proposed as part of the PDP.
	After Paragraph 10	Precinct 1A buildings should generally be of two (2) storeys above ground level. In Precinct 2C the building height should generally be of two (2) storeys above ground level so as to provide an appropriate transition to the nearby existing residential areas.	<p>There is an opportunity for greater building height up to seven (7) storeys for Building 1B-01 and five (5) storeys for 1B-02. Both of these sites front Plenty Road, which has an indicative upper height of 6 storeys proximate to the Polaris Estate site under the Plenty Road Corridor Urban Design Framework. A 6 storey height also applies to Lot 2D under the Village Precinct Development Plan. Both sites are also identified as potential landmarks and focal points for the Polaris Town Centre, creating a sense of arrival and place for the activity centre. The sites are also located where the topographical values are lowest.</p> <p>There is also opportunity for greater building height, up to 7 storeys, for Building 1B-16, given the estate's slope (north to the south and west) and the lowest topographic values being located along Main Drive. Moreover, the built form context of the land to the south of Main Drive is evolving with the Plenty Road Corridor Urban Design Framework identifying the existing service station and townhouses as a future mixed use - commercial area and La Trobe University proposing its own town centre development.</p>
	Figure 86	Indicative Conceptual Building Heights above ground (i.e. storeys)	<p>Replacement of Figure 86.</p> <p>Modification of the building envelopes for buildings 1B-01, 1B-02 and 1B-16.</p>
83	Section 5.3.5	Front setbacks from Plenty	Ground level Front setbacks from

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Page	Section	Current Wording	Proposed Wording
	Front Setback from Plenty Road Paragraph 3	Road for buildings located within Precinct 1B will vary between a minimum of 3 metres and a maximum of 11 metres	Plenty Road for buildings located within Precinct 1B vary between 0 metres and approximately 5.5 metres.
	Section 5.4 The Lancaster Gate Development Plan (2001, amended 2007) Paragraph 4	In order to achieve a good design outcome and to promote the precincts status as a Neighbourhood Activity Centre front setback of 3 to 11 metres from Plenty Road is proposed. Refer to Figure 49b on page 40. A reduced front setback at the 'corner' locations within Precinct 1B to create a sense of place and a sense of arrival to the new Main Street of the Polaris Neighbourhood Activity Centre.	In order to achieve a good design outcome and to promote the precincts status as a Neighbourhood Activity Centre, a ground level front setback of between 0 metres and approximately 5.5 metres from Plenty Road is proposed. Refer to Figure 49b on page 40. A reduced front setback at the 'corner' locations within Precinct 1B creates a sense of place and a sense of arrival to the new Main Street of the Polaris Neighbourhood Activity Centre.
86	Section 6.2 Yield	Precinct 1B – Accommodation A total of 39 dwellings are envisaged within Precinct 1B as follows: Home Office Apartments 19 Shop-top Apartments 20 The home office apartments and shop top apartments will be located adjacent to Main Street	Precinct 1B – Accommodation The following provides an approximate break down of potential A total of 39 dwellings are envisaged within Precinct 1B as follows: Home Office Apartments 19 Shop-top Apartments 20 Student Accommodation 200

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Conclusion

We trust that the above accords with our discussion to date and now allows Council to conclude its assessment of the application and in turn, respectfully request it decide the application at its earliest available opportunity. Furthermore, we confirm that our client seeks to a lodge planning permit application for Lot S3 and S9 that generally accords with this amended PDP in early 2019.

Should you have any queries in relation to the above, please do not hesitate to contact the undersigned on 03 8648 3500.

Yours sincerely,



James Goulding

Director

jgoulding@sjbplanning.com.au

Cc Mr Andrew Wyman
Deal Corporation Pty Ltd

Polaris 3083 Development Plan

1.1 Polaris 3083

The site is located at 1056-1140 and 1142 Plenty Road, Bundoora and forms part of the Lancaster Estate (formerly the Larundel Psychiatric Hospital site). The current approved land use and development strategic master planning document for the site is the *Lancaster Gate Development Plan (2001)*. An addendum was approved by the responsible authority on 20 July 2007 to amend the *Lancaster Gate Development Plan (2001)*. This addendum relocated the location of the multi-purpose community centre from within the Residential Precinct of the Lancaster Gate Estate (which is now indicated for Stage 5 residential subdivision), to one of the former Larundel Psychiatric Hospital buildings (located to the west of the oval, within the Mixed Use Precinct) to be the location of the proposed community centre.

This master plan contains two precincts; a mixed use precinct and a village precinct. Consistent with this approach, the *Polaris 2008 Development Plan (2008)* refines and provides additional detail and guidelines to the future land use and development envisaged to occur on the site.

A significant number of specialist consultants have prepared reports relating to specific aspects of the site during the past two decades which has also been supplemented by recent specialist input. Thus, a comprehensive information base is available regarding the site. This wealth of research and information has assisted with the preparation of the master plan, and has informed the holistic and integrated design response that has been adopted.

This is evident in the mix and location of land uses, vegetation retention and removal, building layout and configuration, road design and dwelling diversity. The design is a balanced response to the physical attributes, and opportunities and constraints of the site. In all, this master plan should be considered in its entirety whereby each precinct and sub-precinct interconnect and relate with each other and with the heritage of the site.

The original *Lancaster Gate Development Plan (2001, amended 2007)* encompasses an area of approximately 25 hectares. The *Polaris 3038 Development Plan (2008)* applies to the remaining undeveloped land in the western half of the site which totals approximately 10.59 hectares to be known hereafter as Polaris 3083 and referred to as 'PDP' or 'Polaris' in this document. Refer to Figure 1 for details.

The eastern half of the original Lancaster Gate Estate has been developed with residential development generally in accordance with the *Lancaster Gate Development Plan (2001, amended 2007)*.

Deal Corporation was introduced to this site by VicUrban through their Expression of Interest process, followed by an extensive and rigorous bid assessment process. VicUrban awarded the project to Deal Corporation after assessing the overall 'Master Plan' included herein.

Polaris, which is owned by Deal Corporation (LG) Pty Ltd and managed by Deal Corporation, will benefit from Deal Corporation's twenty years of experience in property development. Their combined experience in residential and commercial developments throughout Melbourne will benefit the Polaris 3083 Mixed Use project as it comes to fruition.

We are certain that this project will be one we are all proud to be associated with.

1.2 Scope, Purpose and Objectives of the Polaris 3083 Development Plan

The scope of the Polaris 3083 Development Plan (PDP) is to prepare a strategic plan that will guide the future land use and development of the remainder of the Lancaster Gate Estate.

The purpose of the PDP is to provide broad land use and development guidelines for the remaining undeveloped portion of Lancaster Gate. The proposed PDP ensures a holistic and integrated approach is adopted for the development of the remainder of the Lancaster Gate Estate.

The PDP maximises the opportunities of the site and resolves constraints to achieve an elevated and high net community benefit for the immediate area and for the Darebin municipality.

The PDP will not replace the current Schedule 1 to the Development Plan Overlay or the planning permit requirements of the relevant zone or the Development Plan Overlay of the Darebin Planning Scheme.

The objective of the PDP is to create a vibrant and high-quality environment for its future community, surrounding residents and the wider community. The PDP employs best practice urban design, planning, architecture and landscape architecture to achieve a mixed use 'live/work/shop/play' environment that is sensitively and sensibly arranged and integrated on the subject land with respect to its own intrinsic assets and characteristics, and also in response to the established patterns of adjacent existing urban fabric.

The key aims and objectives are:

- Urban design excellence
- Community and affordability initiatives
- Environmentally sustainable development

Except as shown in the amended development plan (lodged July 2012 and December 2018) the layout of Precinct 1A and Precinct 1B is to be generally in accordance with Figure 49b (page 40) of the PDP.

This amendment to the Lancaster Gate Development Plan is required under Schedule 1 to the Development Plan Overlay pursuant to Clause 43.04 of the Darebin Planning Scheme. Clause 43.04-3 states that the Development Plan may be amended to the satisfaction of the responsible authority. The new Development Plan must meet the requirements of Schedule 1 to the Development Plan Overlay.

1.3 Overview of the Polaris 3083 Development Plan

The PDP submission contains a total of six (6) chapters.

Chapter 1 provides a general introduction regarding the preparation, scope, purpose and objectives of the PDP.

Chapter 2 introduces the site in relation to its regional, municipal, local, historical and planning context. Characteristics and features of the site are outlined to provide the reader with a familiarity of the site. An overview of the planning scheme policy contained within the Darebin Planning Scheme and statutory planning context is provided.

Chapter 3 analyses the opportunities and constraints of the site relating to circulation and access, land uses, built form and height, vegetation and the public realm, main road exposure, heritage roads, landmarks and focal points, topography and orientation.

Chapter 4 outlines the vision and principles that underlie the PDP and outlines urban design guidelines for the future use and development of Polaris. Features of Precinct 1 and Precinct 2 (and their sub precincts) that comprise the PDP are introduced through text, conceptual images, plans and indicative benchmark images.

Chapter 5 provides a response to the requirements of Schedule 1 to the Development Plan Overlay, a response to the *Larundel Urban Village and Development Considerations (Revised 2001)*, to the City of Darebin's *North East Corridor Strategic Plan (1998)*, and reference to the *Lancaster Gate Development Plan (2001, amended 2007)*.

Chapter 6 provides an overview of the potential staging of the development and residential yield. It also outlines the process for implementation and the development approach process, development contribution and open space contribution and conditional approval of the PDP.

The Appendices include copies of all consultants' reports which have contributed to the PDP, including an overall site plan, integrated transport plan, heritage assessment, arborist report, engineering services report and a retail economic assessment. Certificate of Title details are also included in the Appendices.



DEAL CORPORATION

Polaris 3083 Development Plan

4.3 Overview of Development Plan

The PDP (Figure 49a) is described in terms of two precincts, Precinct 1 and Precinct 2. The original *Lancaster Gate Development Plan (2001, amended 2007)* envisages Polaris to be an urban village which contains integrated residential and commercial development that aims to meet the local communities retail and service needs, as well as those of a broader community, while creating an innovative mix of residential and development opportunities.

The PDP achieves this vision.

Both of these precincts contain two sub-precincts. Precinct 1 contains sub-precincts A and B, and Precinct 2 contains sub-precincts C and D.

The design response focuses on keeping existing high retention value trees where practical, while creating a layout that encourages permeability for both vehicles and pedestrians, that maximises the views of the existing buildings on the site, and that creates a viable Neighbourhood Activity Centre on the corner of Plenty Road and Main Drive.

The building footprints indicated on the PDP are indicative only and will be refined during detailed design of future development at the planning permit stage.

4.3.1 Polaris Overall Master Plan

The Overall Master Plan for Polaris is located in Figure 49b. This plan provides an illustrative overview of proposed land uses and development opportunities within the various precincts of the Polaris site area.

Note: The number and location of car parking spaces as shown on Figure 49b Polaris Overall Master Plan, is indicative only at this stage and is subject to change.



Figure 49a Polaris Development Plan
Not to scale

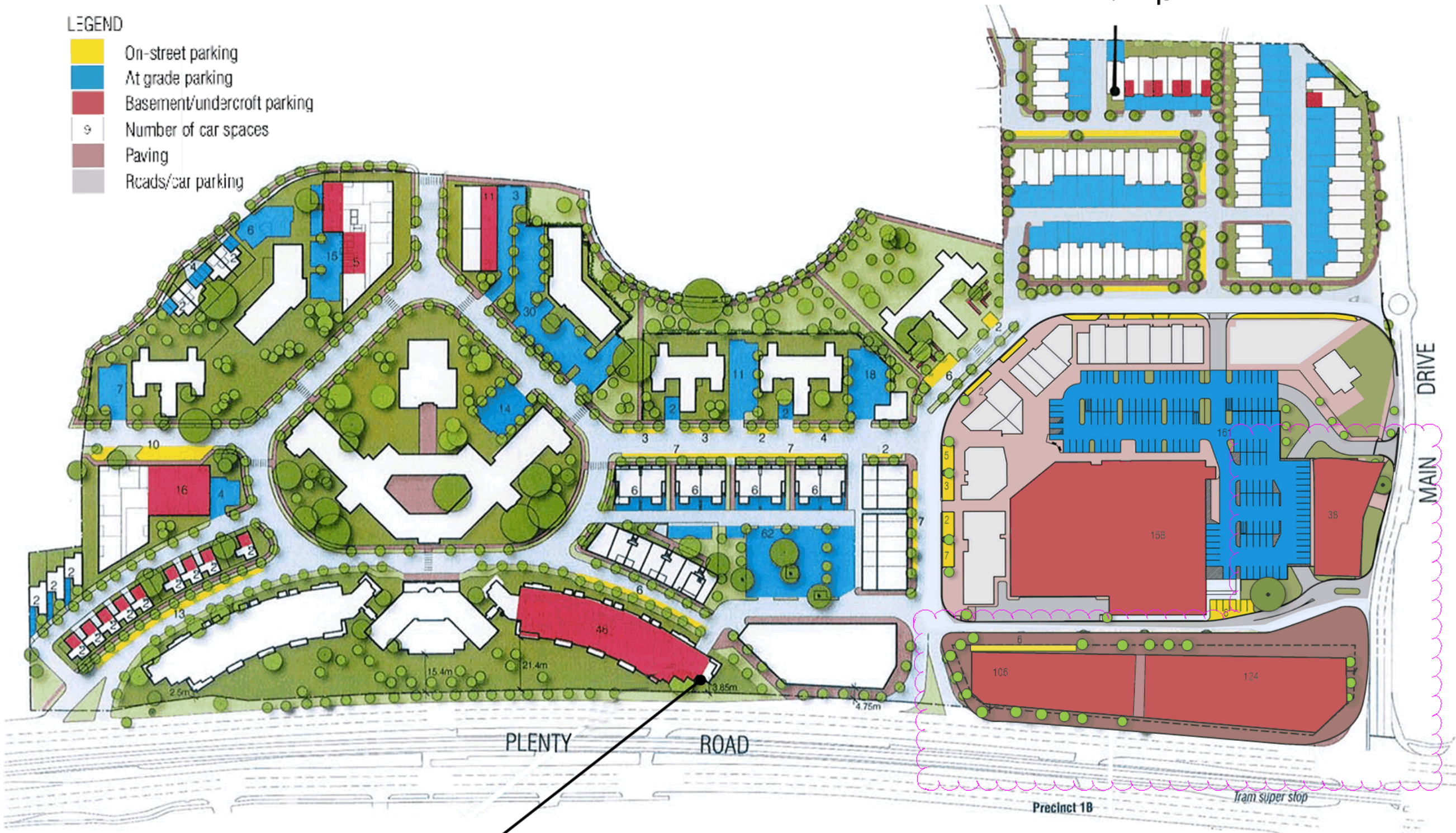
Polaris 3083 Development Plan

Precinct 2C
Accommodation land uses (for example dwellings, medium density dwellings, student housing, aged care)

Precinct 1A
Accommodation land uses (for example dwellings, medium density dwellings)

LEGEND

- On-street parking
- At grade parking
- Basement/undercroft parking
- 9 Number of car spaces
- Paving
- Roads/car parking



Not to scale

Figure 49b Polaris Overall Master Plan

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Precinct 2D
Accommodation land use (for example aged care, retirement village or residential building).

Mixed use neighbourhood activity centre providing retail, commercial, office, supermarket, food and drink premises (take away food premises, restaurant, cafes), residential (shop-top housing, student accommodation, residential hotel) and civic community centre, plaza land uses



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4.4 Precinct 1 Continued

4.4.2 Precinct 1B

Precinct 1B is a mixed use precinct which encompasses the Polaris Neighbourhood Activity Centre, a new main street, shop-top housing, food and drink premises, retail, commercial and office use and development and a civic plaza. These uses will be supported by non-retail uses such as the new community centre also to be located within Precinct 1B. The existing heritage buildings within this precinct are to be retained and either repaired or reconfigured to accommodate a suitable use.

Neighbourhood Activity Centre

The 'Mixed Use Precinct' identified under the Lancaster Gate Development Plan (2001, amended 2007) is referred to as Precinct 1B under the PDP. Precinct 1B is to be a vibrant mixed use precinct containing the new Polaris Neighbourhood Activity Centre (NAC). The NAC will serve the needs of the existing and future residential community of Polaris and surrounding residential communities.

The estimated Area Schedule for Precinct 1B is indicated in the following Table. These floor areas are estimates only and are provided to indicate the possible intensity of development that may occur within the Polaris NAC.

LAND USE	EST. GROSS FLOOR AREA (GFA)	EST. NET LEASABLE AREA (NLA)
Specialty Shops	4,300	3,700
Supermarket	3,700	3,700
Showroom	3,200	3,200
Large Format Specialty Shop	1,400	1,400
Residential Hotel	12,660	8,000
Tavern	850	850
Office	9,600	5,000
TOTAL	35,710	25,850

Figure 53 Estimated Area Schedule for Precinct 1B of the Polaris Development Plan

A retail economic and commercial assessment has been undertaken by Charter Keck Cramer (July 2008) as part of the investigations undertaken for the preparation of the PDP. This report assesses the proposed NAC against the objectives and policy directions contained within Darebin City Council's Retail Activity 'Centres Strategy' (Essential Economics, March 2005). The 'Retail Economic Assessment' (Charter Keck Cramer, July 2008) for the proposed Lancaster Neighbourhood Activity Centre supports the area of retail and commercial (ie office) floorspace proposed the Polaris Development Plan.

Linaker's Cottage

Linaker's Cottage is located adjacent to the southern boundary of Precinct 1B and may possibly be developed with a tavern. Bryce Raworth has prepared a Heritage Report (2007) which assesses the proposed PDP against the heritage significance of the site. Linaker's Cottage is of primary heritage significance and is proposed to be retained as part of the PDP. The significance of this building should be respected by retaining the entry porch and the general bungalow character of the building as viewed from Main Drive. However, it is possible to make substantial alterations and additions to the east and west elevations of the building on the proviso that there is some indent or point of transition indicated between the addition and the extant outline of the cottage as shown in Figure 54 below (Bryce Raworth, Proposed Development Plan for Lancaster – Heritage Issues, 2007).

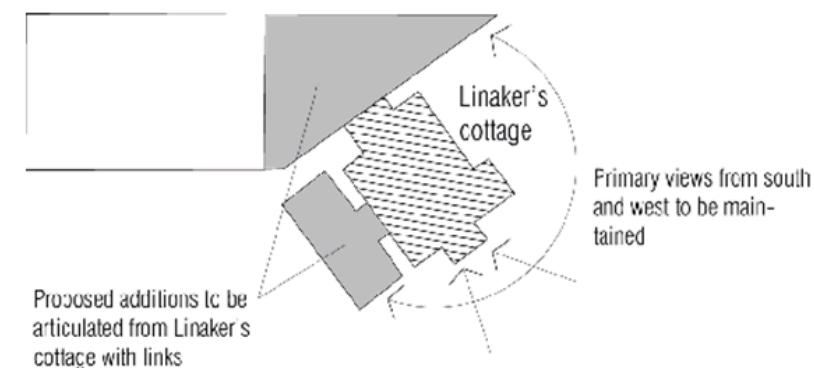


Figure 54 Indicative Future Additions to Linaker's Cottage (Source: HASSELL, 2008)

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4.4.2 Precinct 1B Continued



Figure 55 Location of Figure 56, 57 and 57a

Plenty Road Frontage

It is envisaged that the Plenty Road frontage of Precinct 1B may be developed with mixed use commercial/retail use and development. The interface of Precinct 1B with Plenty Road will be appropriately treated to create a distinctive character for the new Polaris NAC. Minimal setbacks of Precinct 1B will be encouraged due to the adjacent main arterial road and to achieve an active frontage. Sensitive uses, such as residential but excluding a residential hotel, are not considered to be appropriate along this frontage and are discouraged.

The Plenty Road frontage has the potential to support a preferred maximum building height of seven (7) storeys, having regard to local context considerations, including the site's relationship to surrounding development.

The functionality of buildings located within Precinct 1B, which forms part of the Polaris NAC, and the layout of the Precinct, have both significantly influenced the front setback of buildings to Plenty Road.

A varied ground level setback of between 0 metres and approximately 5.5 metres from the title boundary along Plenty Road is proposed. The varied setback will create a sense of place and a sense of arrival to the Polaris Town Centre, particularly at the corner intersection of Main Street and Main Drive. The title boundary is setback 4 metres from the Plenty Road kerb.

A strong built edge that defines the urban block and that punctuates the corners at landmark opportunities is a proper and valid urban design response, that promotes better activation of the street, passive surveillance, increased commercial exposure and more efficient use of land (ie avoidance of 'no man's land').

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The Plenty Road setback frontage (along Precincts 1B and 2D) has been designed with the view to creating a setback rhythm of buildings coming forward to mark urban corners and buildings setback in between to further accentuate these landmarks. The Plenty Road setback of Precinct 1B varies between 0 metres and approximately 5.5 metres from the title boundary. Refer to Figure 49b for details. This will prevent a hard, continuous edge along Plenty Road. Buildings sit proudly forward (ie in Precinct 1B) and others are recessed (ie in Precinct 2D) with appropriate landscaping treatment to accentuate their attractiveness along this frontage.

Native street trees are proposed for the Plenty Road site frontage wherever practical. This treatment is a continuation of the existing Plenty Road treatment and complements the native planting environment created by Bundoora Park. It is intended that street trees be planted within the footpath on Plenty Road and wherever possible incorporated into rain gardens that harvest surface run-off from the footpath. Trees to the car park on the east side of the commercial buildings will be native species. Refer to the Landscape Plan (Figure 80) for a list of indicative species. Rain gardens are proposed in order to capture street run-off in this area.

The adjacent conceptual plan (Figure 56) and cross section (Figure 57 & 57a) provide a conceptual indication of the form, character, mass and materials of future development as do benchmark images D, E and F.

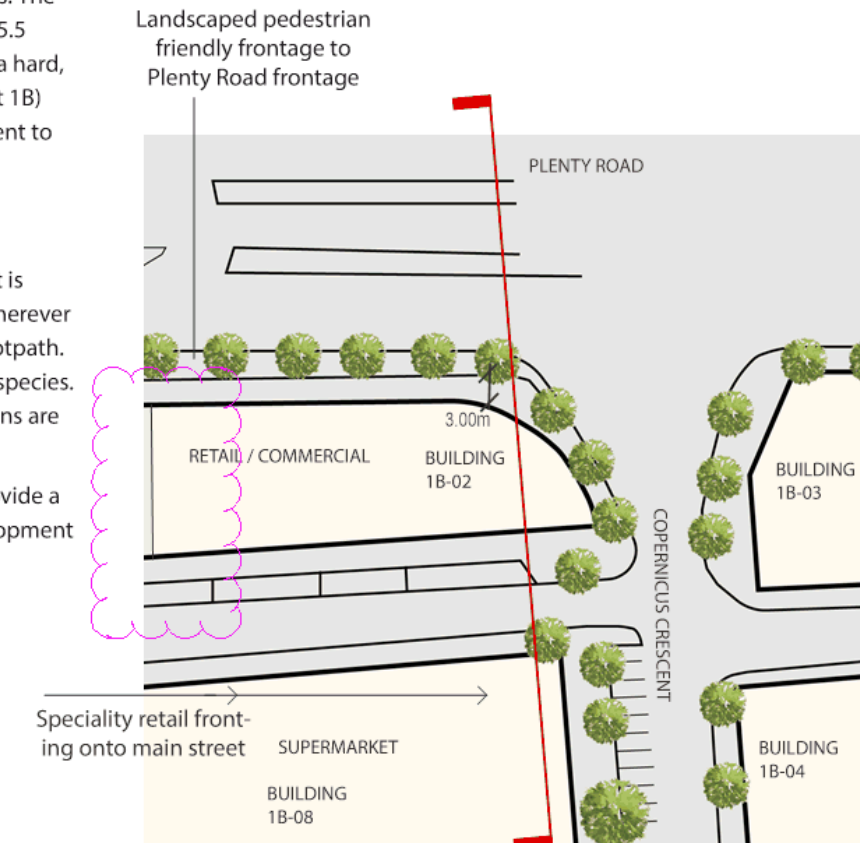


Figure 56 Indicative Plenty Road Interface



Benchmark Image D
Corner buildings should have greater height to define the corner. Buildings should be articulated and provide active edges



Benchmark Image E
Indicative landscaping treatment along the Plenty Road frontage of Precinct 1B

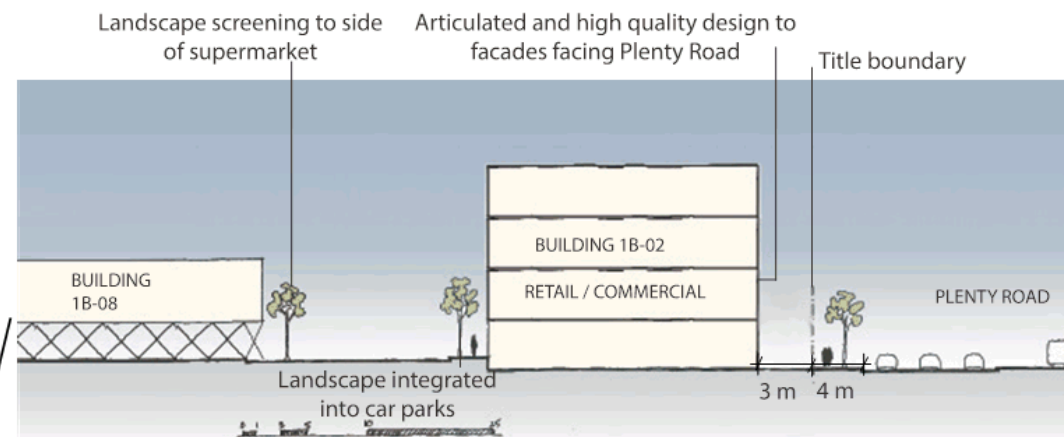


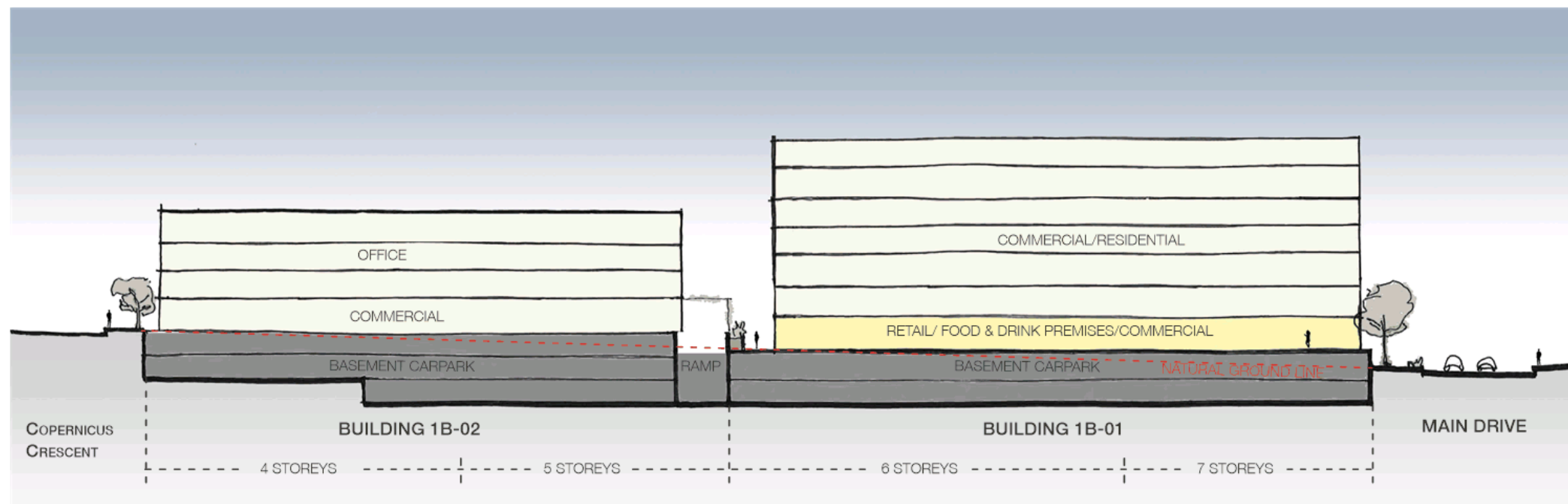
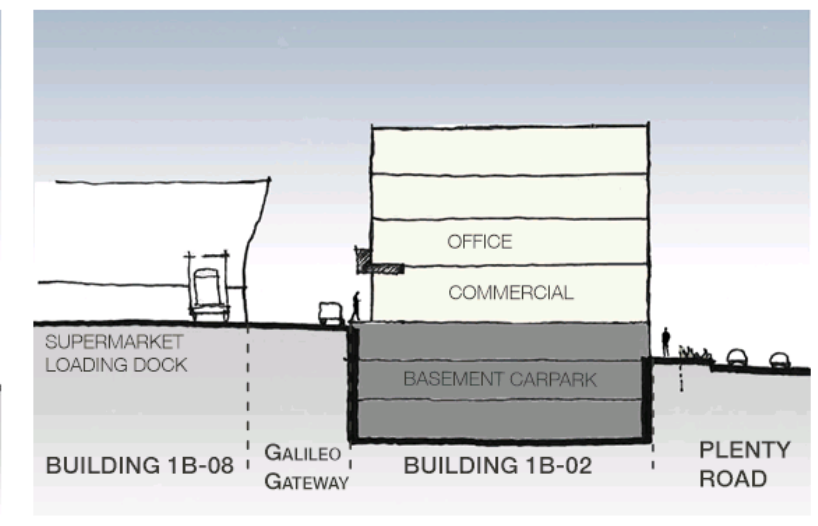
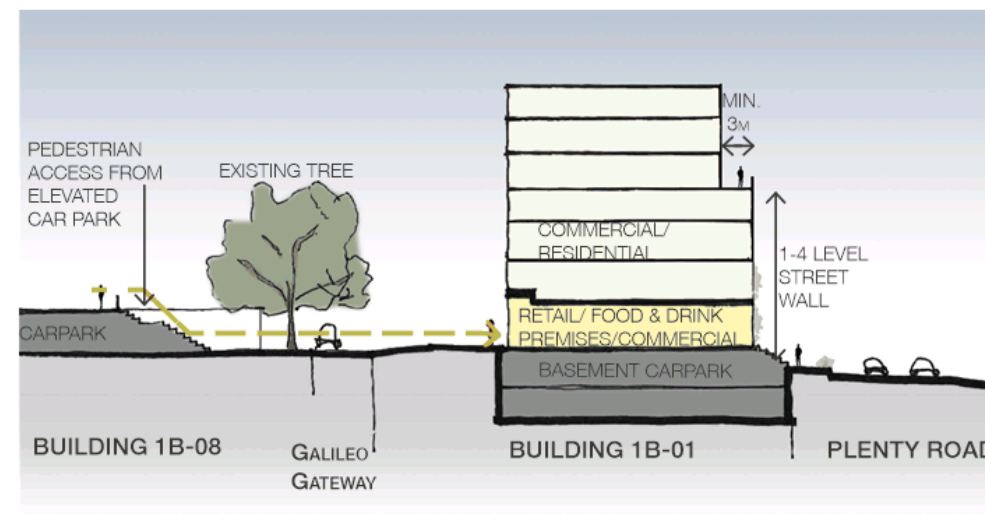
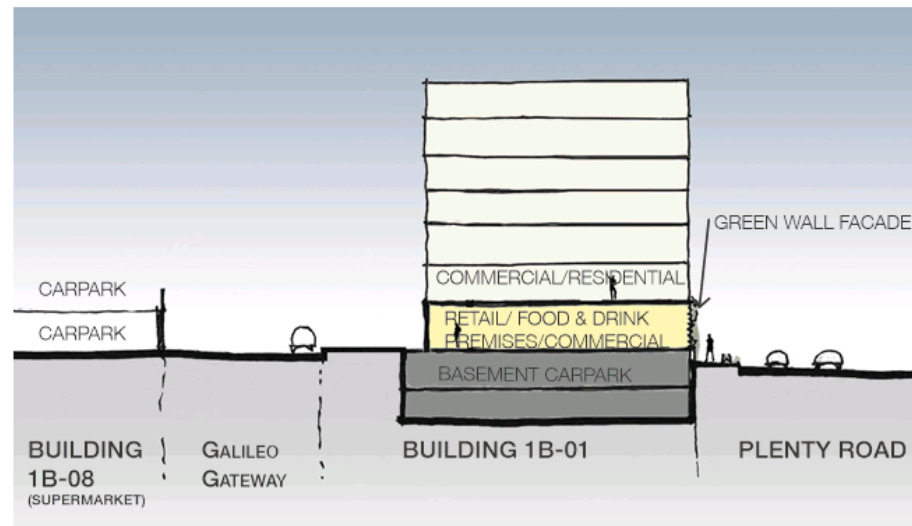
Figure 57 Indicative Section through buildings fronting onto Plenty Road

Appropriate screening to understorey of supermarket to avoid an open sided and open ended sub-basement car park below



Benchmark Image F
Level changes are well handled, simple and consistent ground plane treatment and provision of shade with trees and canopies create attraction and comfortable pedestrian amenity

Indicative Building Sections - Figure 57a



Polaris Precinct 1B

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4.4.2 Precinct 1B Continued



Figure 58 Location of Figure 59, 60 and 60a

Main Drive Frontage

The Main Drive frontage of Precinct 1B will have a continuous landscape treatment along the length of the boundary to create a sensitive interface with the adjoining use and development across Main Drive.

A building adjacent to Main Drive (located in the intervening area between Main Drive and the car park) (nominated as Building 1B-16 in Figure 49a, 59, 60 and 60a), may possibly be used for either commercial or residential uses. However, the use of this building is not determined at this stage, and will be proposed at the planning permit stage of the development. Opportunities to create an active frontage to the building will be encouraged and maximised where possible. This building will assist in minimising views of the car park from Main Drive. It has the potential to support a building height of up to seven (7) storeys, having regard to local context considerations, including the site's relationship to surrounding existing development. Appropriate landscape treatment and building circulation of the facade will assist in minimising the visual perception of a blank wall when viewed from Main Drive.

The rear of the supermarket will be treated in a visually interesting manner to positively contribute to the presentation of the Polaris Neighbourhood Activity Centre. Contemporary built form will be encouraged where appropriate within this precinct. The materials and finishes of the rear of the supermarket will be chosen to maximise the visual interest of this elevation.

There is opportunity for at-grade car parking to be supplemented with some deck, basement and/or semi-basement car parking within Precinct 1B, to minimise the expanse and visibility of at grade parking. An appropriate landscape response to the design of car parking areas in Precinct 1B will satisfactorily mitigate any potential adverse amenity impacts from adjoining roads and uses.

Good tree cover will be achieved in the supermarket car park. Kerb and channel around the tree planting areas will be avoided, and car park surface run-off will be captured and directed into rain gardens. In addition to the tree planting, massed shrubs and ground covers will be used to provide another layer of visual interest and pedestrian scale to the space.

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Trees with understorey planting will also be used to delineate the private vehicular entries to shop-top dwellings. Refer to benchmark images H and G for a conceptual indication of the form, character, mass and materials of development within Precinct 1B.



Figure 59 Indicative Part Plan of Precinct 1B



Benchmark Image G
Front facade to supermarket with external canopies for pedestrian comfort



Benchmark Image H
Signage to be integrated into the architecture

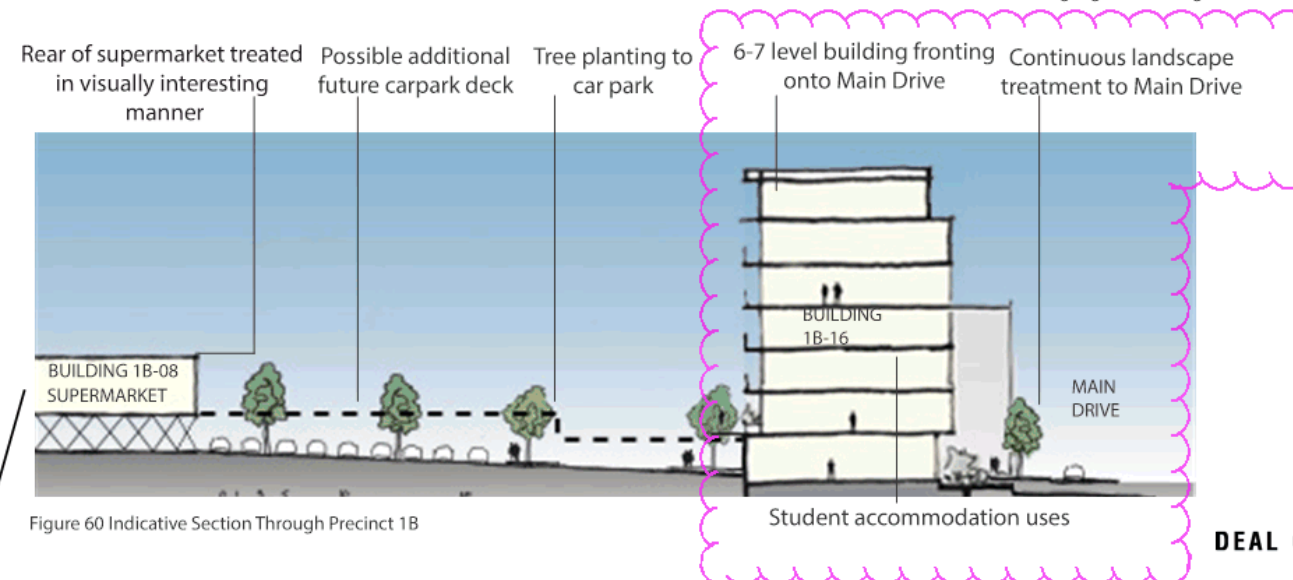
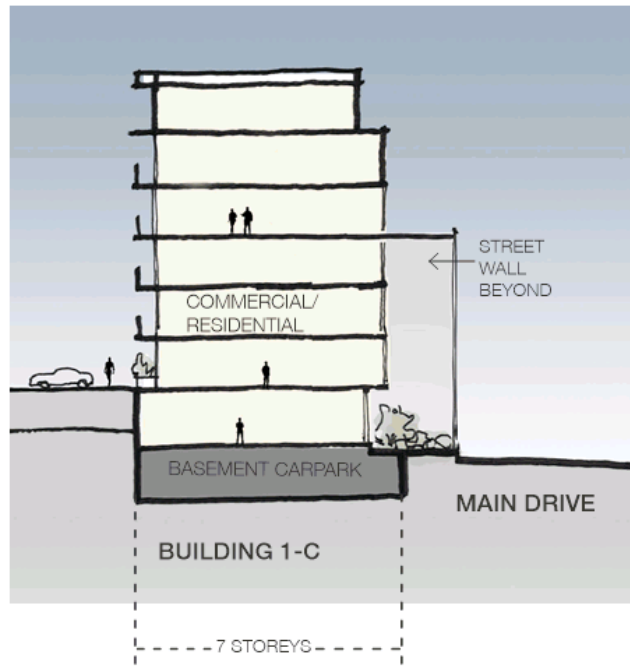


Figure 60 Indicative Section Through Precinct 1B

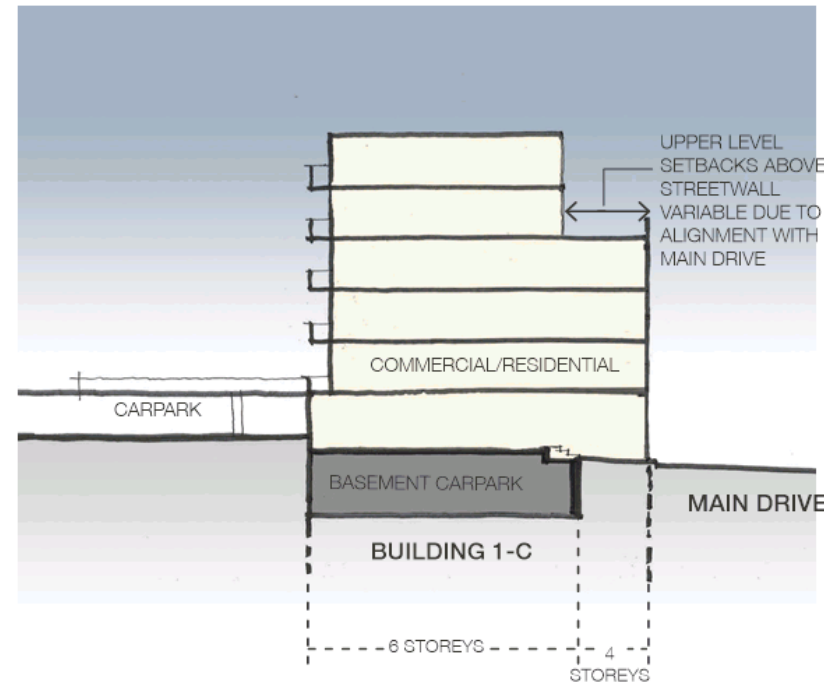
Appropriate screening to the understorey of the supermarket to avoid an open sided/open ended sub-basement car park below



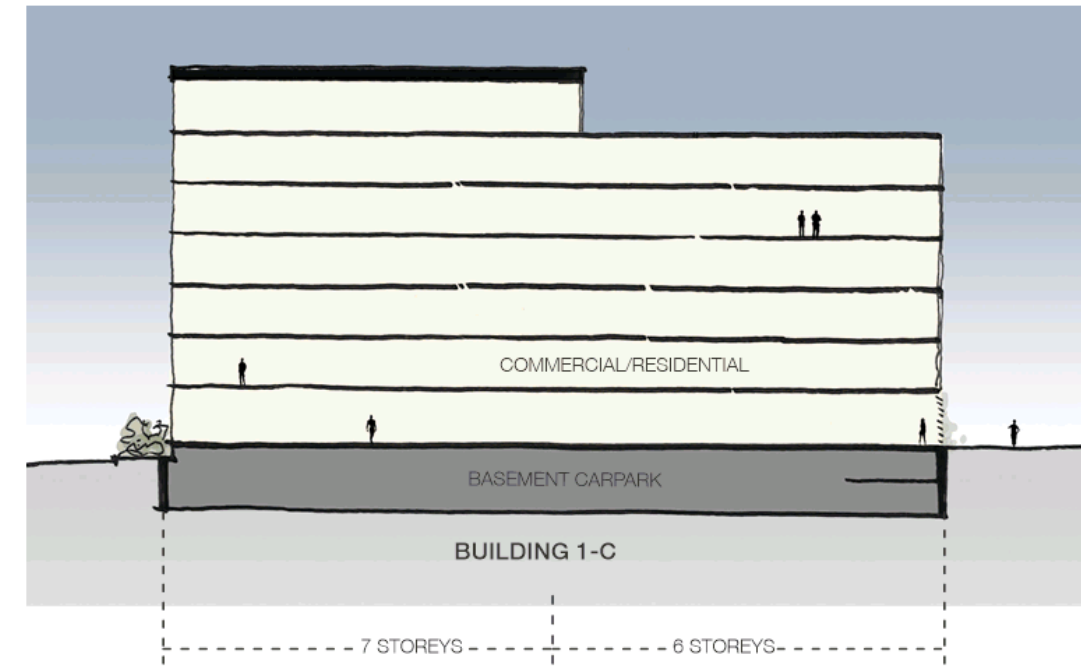
Indicative Building Sections - Figure 60a



SECTION F-F



SECTION G-G



SECTION H-H



Polaris Precint 1B

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4.7 Urban Design Guidelines

The purpose of the Urban Design Guidelines is to:

- Allow for innovative viable refurbishment and re-use of the heritage buildings and the construction of infill/new elements within the site, which respect the character of these heritage buildings
- Create a sustainable Neighbourhood Activity Centre to serve service the local and broader retail/commercial needs within a high quality facility, as well as creating a mix of medium and higher density residential developments

Future use and development on the site must consider the urban design guidelines contained with the *City of Darebin Larundel Urban Village Design and Development Considerations (March 2001, revised August 2001)*.

Consideration must also be given to the following Urban Design Guidelines as follows in the next sections:

4.7.1 Development Envelope

Front Setback

Minimum front setbacks are intended to enhance a coordinated streetscape across each precinct and will ensure adequate distances between roads and new buildings and on-street car parking.

All lots are required to have a minimum front setback from the front title boundary as follows:

Precinct 1A - Accommodation 1 - 3 metres from the boundary.

Precinct 1B - Neighbourhood Activity Centre

Generally 0 metre setback for retail and commercial buildings.

- Main Street frontage Generally 0 metre setback or retail and commercial buildings.

- Plenty Road frontage Generally varying between 0 metres and approximately 5.5 metres to the title boundary at ground level.

Precinct 2C - Accommodation Varies, but should be complementary with existing buildings.

Precinct 2D - Accommodation Varies, but should be complementary with existing buildings.

With the exception of vehicular access aisles established for the purpose of site entry, the front setback, where applicable shall be fully landscaped with paving, garden beds and/or grass as appropriate.

No building or storage shall be constructed within the setbacks above. New building setbacks from internal roads should respect the former hospital buildings on the site and the landscaped setting of the land, including provision area for the planting of canopy trees.

Side Setback

Buildings may be constructed to side boundaries. If buildings are not constructed on the boundary, the setback is to be a minimum distance of 3 metres, or as designated by Council ResCode (if residential building), or other relevant fire regulations.

Setbacks on corner lots must enable sufficient sightlines for traffic in accordance with the relevant Australian Standard (AS2980.1-1989).

Rear Setback

Buildings may be constructed to rear boundaries. If buildings are not constructed on the boundary, a minimum distance of 3 metres, or as designated by Council or other relevant fire regulations.

Building Heights

Generally, no building will exceed three storeys in height above ground level. Where it is necessary to exceed a building height of three storeys, for example due to the slope of the land, the proposal must be accompanied by appropriate urban design support, to the satisfaction of the responsible authority.

In accordance with Section 3.8 and 3.9, there is an opportunity for greater building height, up to seven (7) storeys for Buildings 1B-01 and five (5) storeys for Building 1B-02. Both buildings front Plenty Road, which has a 42 metre road reserve width and a designated upper height limit of six storeys proximate to the Polaris Estate under DDO17 and in the Plenty Road Corridor Urban Design Framework. A six (6) storey height applies to Lot 2D under the Village Precinct Development Plan.

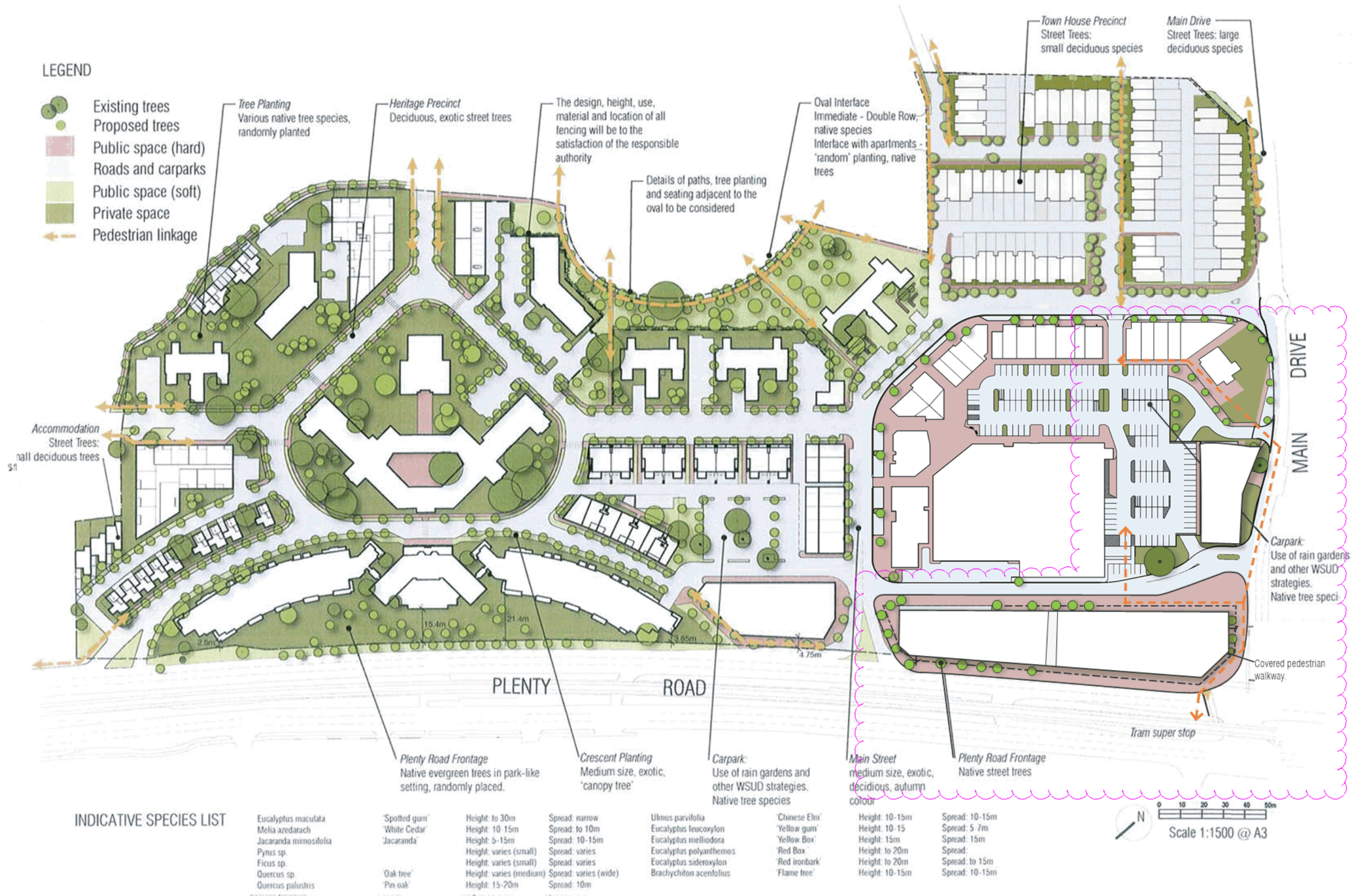
Both Buildings 1B-01 and 1B-02 are also identified as potential new landmarks and focal points for the Polaris Town Centre, creating a sense of arrival and place for the activity centre.

There is also opportunity for greater building height, up to seven (7) storeys for Building 1B-16, given the topography of the Polaris Estate which slopes down to the south and west.

The town centre's lowest topographical values are located along Main Drive and at the Main Drive /Plenty Road intersection.

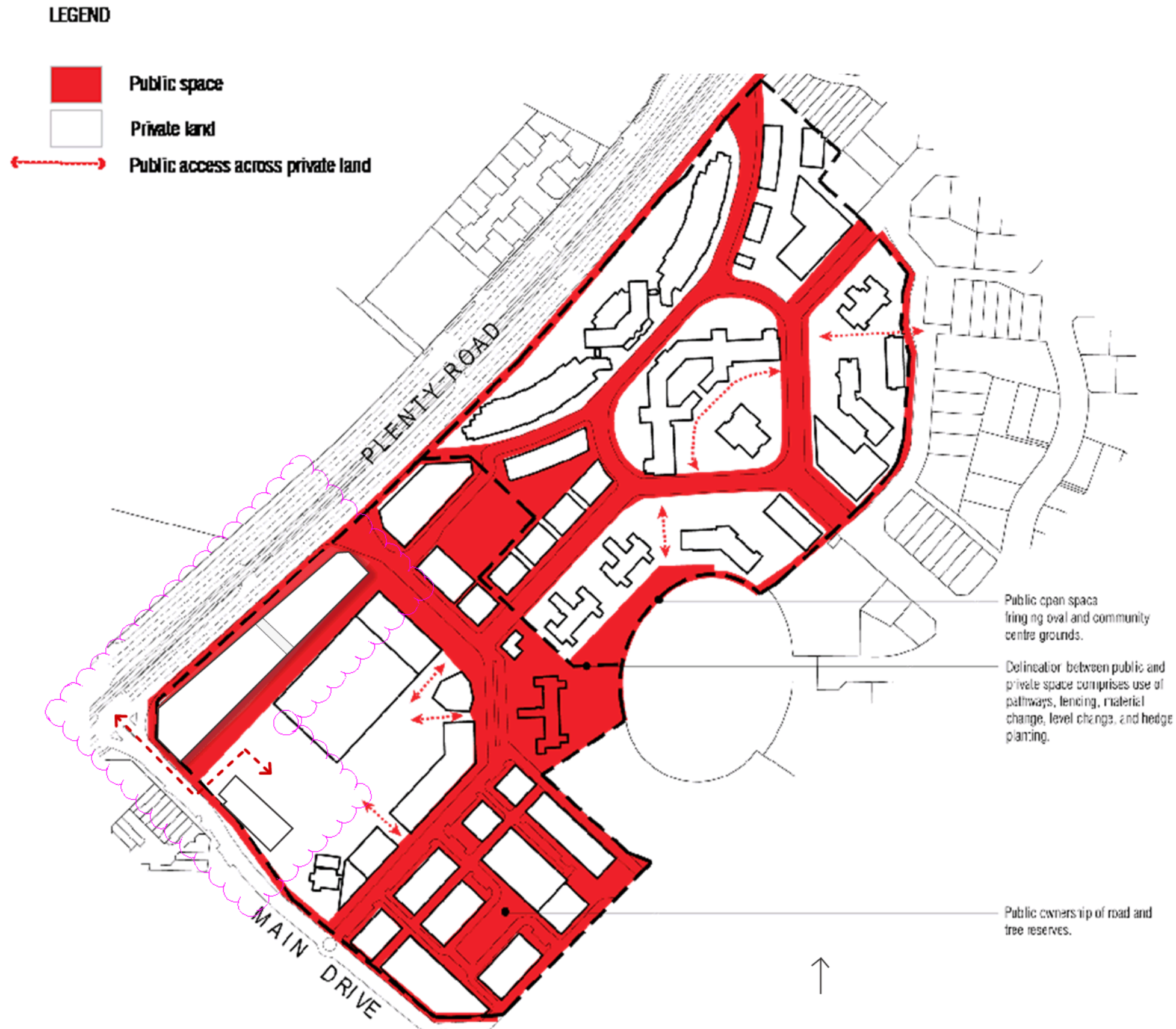
Unless otherwise specified, buildings will not generally exceed 11.5 metres in height, as measured from natural ground level to the satisfaction of the responsible authority. Where the building incorporates a pitched roof, the roof may exceed this height provided it does not contain any habitable space.

Where an overall building height of 11.5 metres is exceeded, the proposal must be accompanied by appropriate urban design support to the satisfaction of the responsible authority.



Not to scale

Figure 80 Landscape Concept Plan / Overall Master Plan (Indicative Only)



Not to scale

Figure 84 Public Private Concept Plan - Indicative Only

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5.2 Larundel Urban Village Design and Development Considerations March 2001 (revised August 2001) Continued

— Building height/location and minimizing potential for overshadowing and overlooking	Refer to Section 4.2 - Design Response for a detailed explanation regarding the proposed building heights. Potential impacts such as overshadowing and overlooking will be assessed at the planning permit stage of the development when details of the design are determined. However, buildings will be designed to minimize the potential for overshadowing and overlooking. Refer to Section 5.3.3 for heights of future development (Figure 86).
— Protection of significant view lines	The significant view lines on the site (i.e. existing heritage buildings and vegetation) are protected and enhanced as a result of the PDP. Refer to Section 4.2 - Design Response for a detailed explanation.
— Protection of existing trees	Retention of the existing trees on the site is encouraged where practical. The design response of the development plan includes the retention of existing trees on the site. Refer to Section 4.2, 4.8 and 4.9 for details.
— Regard to the adjacent urban character and heritage in the design and layout of all buildings, car park areas and landscapes	The heritage character of the site has been respected, maintained and enhanced as a result of the PDP. Refer to Appendix D.
▪ The oval to be integrated and used as a distinct node depending on the role function and sizing of the village centre	The oval has been integrated into the PDP (refer to the Landscape Master Plan, Figure 80 for details). Land adjacent to the oval will be public open space, and the community centre will be located adjacent to the oval.
▪ A sports pavilion to be provided to support the role of the oval as a local recreation facility	There is the possibility of using the community centre building as a sports centre/sports focal point. In conjunction with the oval, the community centre is able to serve as a local recreation facility, should the City of Darebin desire.
▪ Housing diversity particularly lower cost housing opportunities to be pursued, including evidence of discussions with relevant housing providers and agencies	A variety of residential land use types are envisaged to be developed with the site to meet housing diversity and affordable housing. Discussions have been held with La Trobe University in relation to the demand for student housing.

1. Project Sizing (Heritage and commercial/mixed use precinct)	
An indicative plan based on the vision and principles are indicated to provide guidance. It is based on the following project elements. These elements are indicative and do not preclude other land uses and mixes of uses, subject to Council approval. Further, site characteristics which must be taken into account when developing the precinct (ie protection of significant trees, landscape setbacks, open space provision, interface treatments etc), appropriate siting of new buildings, may result in changes to the following estimated project sizing.	The exact floor areas of each use are not known at this stage and the estimated floor areas are indicative only. These will be formalised at the planning permit stage for each precinct.
1.1 Residential	
▪ Student housing (x 400 beds)	Precinct 2C provides opportunity for the reuse of an existing heritage building for the residential type of student housing. There is also an opportunity for student accommodation in Precinct 1B (e.g. Building 1B-16). Approximately 200 student housing units are envisaged.
▪ Town houses (2/3 storey including SoHo) (x 50) – Main Street	Precinct 1A provides the opportunity for medium density housing in the form of townhouses. Approximately 53 dwellings in the form of 2-3 storey townhouses are envisaged for Precinct 1A.
▪ Apartments above/integrated with retail (x 100) – encourage 3-storey walk-up	Precinct 1B provides opportunity for shop-top housing above retail or office use. Approximately 39 dwellings are proposed within Precinct 1B.
▪ Medium rise (2 blocks x 16 units per block ie 32 units). 11.5m height limit consistent with existing buildings	The Development Plan provides opportunity for medium density housing (ie approximately 69 dwellings within Precinct 1A and approximately 191 dwellings within Precinct 2C). There is opportunity to reuse the existing heritage buildings on site with residential uses.
▪ Apartments in existing heritage buildings/non-student housing – 30 units	It is envisaged that the heritage buildings have the opportunity to be reused with residential development. Refer to Section 4.5.1 for detail.

Polaris 3083 Development Plan

5.2 Larundel Urban Village Design and Development Considerations March 2001 (revised August 2001) Continued

<ul style="list-style-type: none"> Other residential in residual areas of mixed use zone as permitting in land areas 	The Development Plan provides for a variety of residential housing types on the site. Refer to Section 6.2 for detail of the dwelling yield and dwelling type breakdown.
1.2 Retail	
<ul style="list-style-type: none"> 5,300 sqm/plus car parking 	The gross lettable area of retail floor space is approximately 11,790 square metres for the Polaris Neighbourhood Activity Centre within Precinct 1B. The Retail and Commercial Economic Assessment (July 2008) prepared by Charter Keck Cramer supports 11,790 square metres of gross lettable retail floor area on the site.
<ul style="list-style-type: none"> — Supermarket 3,700 sqm 	The floor area for the supermarket is approximately 3,700 square metres.
<ul style="list-style-type: none"> — Specialty 3,700 sqm (say 20 – 25 shops including newsagent, pharmacy, restaurants (6 x 120 sqm), bakery, medical, hairdresser, video, take-away etc) 	The total floor area of specialty shops is approximately 3,700 square metres. The Retail and Commercial Economic Assessment (July 2008) prepared by Charter Keck Cramer supports 3,030 square metres of specialty shops on the site.
<ul style="list-style-type: none"> — Total site requirement approximately 8,000 – 12,000 sqm 	The gross floor area of other retail shops and speciality shops totals 7,680 sqm. The PDP meets this guideline, as the total retail floor area proposed is 12,600 square metres, which is generally in accordance with the maximum of 12,000 square metres of this guideline.
1.3 Offices	
<ul style="list-style-type: none"> First floor and some shop front opportunities 	Precinct 1B provides opportunity for a variety of future office uses on the ground floor and the first floor of new buildings.
<ul style="list-style-type: none"> First floor – 2,000 sqm services offices 	Precinct 1B provides opportunity for a variety of first floor office uses.
<ul style="list-style-type: none"> Shop front including Telstra, employment etc (500 sqm) 	The development plan provides the opportunity for Telstra to be an occupant of the site if appropriate.
<ul style="list-style-type: none"> Total 2,500 sqm 	There is opportunity within Precinct 1B to accommodate office use. A gross floor area of approximately 9,600 sqm is proposed. Refer to Appendix F.
1.4 Entertainment/Short term accommodation/recreation	

<ul style="list-style-type: none"> Hotel including bistro(s), function room, take-away bottle shop (3,000 sq) 	There is the opportunity for a tavern (ie hotel use) to be located within Precinct 1B. There is also opportunity for a residential hotel (12,660 sqm) to be located within Precinct 1B.
<ul style="list-style-type: none"> No additional gaming machines will be allowed within the City of Darebin 	No gaming machines are proposed as part of the PDP.
<ul style="list-style-type: none"> Including (say) up to 200 rooms/serviced apartments 	The PDP provides opportunity for a range of residential land use types which may include serviced apartments within Precinct 2.
<ul style="list-style-type: none"> Site area requirement 8,000 sqm 	There is adequate land available on site for recreation, entertainment and short term accommodation purposes.
<ul style="list-style-type: none"> Gymnasium – small scale say 300 sqm 	There is opportunity for a small gymnasium to be provided on site, perhaps ancillary to a residential or as part of another commercial use.
1.5 Open spaces	
<ul style="list-style-type: none"> Place of respite including seating, lighting, shelter, sculpture and a small range of facilities to accommodate uses of the town centre. The space should be easily identifiable, secure and integrate with town centre uses. 	Public open spaces are extensively provided through the site. The civic pavilion to be located within Precinct 1B is easily identifiable and integrates seamlessly with the adjacent uses. Public art will be located on the site, and the open space areas will be well lit, and seating and shelters will be provided.
1.6 Community facilities	
<ul style="list-style-type: none"> Best located in village area to support community needs, eg private childcare facility — Allow 250sqm and site area of 500 sqm — Provision of a sports pavilion in an appropriate location adjacent the existing oval will be required 	The community centre is located within Precinct 1B adjacent to the oval.

Polaris 3083 Development Plan

5.2 Larundel Urban Village Design and Development Considerations March 2001 (revised August 2001) Continued

<p>The Development Plan shall identify a site for a future pavilion adjacent to and in support of public use of the oval. The size and location of the site and the provision of access to the future pavilion shall be to the Council's satisfaction. The opportunity of utilizing an existing building for the facility (such as the kiosk) shall be assessed in the first instance in the preparation of the plan.</p>	<p>Following the site analysis and design response, it is considered that the community centre and public open space adjacent to the oval provides opportunities for Darebin City Council to develop a pavilion if desired.</p>	<p>Creation of an active frontage to Plenty Road is important, to maintain a visual link to the proposed activities from the road. This interface could be managed through applying design solutions such as landscaping and 'Main Street' design. Facilities such as car parking can be designed not to dominate the road frontage. Careful design of the car parking and access to this part of the precinct will ensure that impact on future residents is minimised, and that a sympathetic interface is created with Plenty Road. All buildings located with frontage to Plenty Road must have windowed frontages, appropriate signage and landscape setbacks. Building design should be contemporary with regard to materials and finishes.</p>	<p>Refer to Section 4 of this submission for a detailed design response of the PDP. The PDP generally complies with these principles. The Precinct 1B Plenty Road frontage creates a frontage that is appropriate for a Neighbourhood Activity Centre and ensures that impacts on future residents are minimised and that a sympathetic interface is created with Plenty Road. Refer to Section 4.4.2 – Precinct 1B, for further detail. Refer to the Landscape Master Plan in Section 4.8 for further indicative details regarding the landscaping along the Plenty Road frontage. Building design will be contemporary with regard to materials and finishes as per the indication benchmark images in Section 4.0.</p>
<p>The site so identified shall be purchased by the Council at the time of completion of the relevant stage of development, subject to agreement on the price and terms of sale. The construction and or fit out of the facility may by agreement be undertaken by the developer to the Council's specifications. The construction and/or fitout of the facility shall be at the Council's expense.</p>	<p>Further discussions with Council are required in relation to this.</p>	<p>All new buildings should include sustainability principles in their design, including achieving a 5 star energy efficiency rating.</p>	<p>Refer to Section 4.13 for Environmentally Sustainable Development Principles that will guide the future design and development of each precinct.</p>
<p>1.7 Car parking</p>		<p>Uses on the boundary between the Village Precinct and mixed use precinct should be sensitive to the nature of the Village Precinct, particularly the heritage component. The Village Precinct encourages high quality building design and architecture, and this will also apply to development within the Mixed Use Precinct. Building heights and setbacks must take into account the nature of nearby buildings within the Village Precinct.</p>	<p>Uses envisaged for each precinct incorporate sensitive interfaces where appropriate (i.e. – with the surrounding residential estates to the north and east) and 'urban rhythm' interfaces where appropriate (i.e. with Plenty Road, and to a lesser extent, Main Drive). Refer to Section 4 of this submission for the design response and further detail. Refer to Section 5.3.3 for indicative building heights of each building. Consideration has been given to the significance of the existing heritage buildings on the site and the site's topography, with the aim of increasing, maintaining and enhancing views of the existing heritage buildings.</p>
<p>Total requirement dependent on project role, function and sizing – estimate 300 – 400 public spaces plus appropriate on site (private) spaces.</p> <ul style="list-style-type: none"> ▪ Supermarket 4.5/100 sqm (public) ▪ Specialty shop 3.0/100 sqm (public) ▪ Office 3.0/100 sqm (70% on site) ▪ Hotel 50 (public), 15 (private) 	<p>Refer to the <i>Integrated Transport Plan, 2008</i> (Appendix C) prepared by Ratio Consultants for a detailed empirical assessment of the future car parking demand for the site.</p> <ul style="list-style-type: none"> ▪ Ratio estimate a rate of 5.0/100 sqm for a supermarket ▪ Ratio estimate a rate of 3.0/100 sqm for shops ▪ Ratio estimate a rate of 3.0/100 sqm for offices ▪ Ratio estimate a peak parking demand of 0.35 spaces per seat for a tavern. 	<p>Uses on the boundary between the Village Precinct and Mixed Use Precinct and La Trobe University site should be sensitive to the nature of the university site particularly the heritage and significant vegetation and landscape component. High quality building design and architecture, is required adjacent this interface which will ensure minimal conflict of land uses and appropriate building development which is not of an excessive height (i.e. not above 2 storey). Location of car park areas should be appropriately located so as not to adversely impact on the university site's landscape environs.</p>	<p>Precinct 1A has an interface with La Trobe University. Refer to Section 4.6 for details regarding this interface. The future residential use and development of Precinct 1A will not have a detrimental impact on the use of the adjacent land with La Trobe University. Future detailed design will ensure the development adjacent to this interface will be treated appropriately. High quality built from building design and architecture will characterise future development of this precinct. Car parking and road layouts will be integrated into the future development of Precinct 1A. Refer to Section 4 for the design response and future detail regarding Precinct 1A and La Trobe University.</p>
<ul style="list-style-type: none"> • A rate of 0.35 spaces (evening) and 0.2 spaces per seat (lunch) is recommended for Restaurants. • A peak rate of 0.45 spaces per room is recommended for Residential Hotel. • A peak rate of 0.2 spaces per room is recommended for Student Accommodation. <p>A review of applicable car parking rates and temporal demand and supply in the town centre will be undertaken at the planning permit stage for each site.</p>			
<p>1.8 Design considerations</p>			



Polaris 3083 Development Plan

5.2 Larundel Urban Village Design and Development Considerations March 2001 (revised August 2001) Continued

<p>The area surrounding the oval, which has access to open space as well as local services and facilities, is a prime location for higher density residential development, which could include various forms of medium density housing, or alternatively student housing, as may occur within the Village Precinct. This development must not restrict view lines from the Mixed-Use Precinct across the oval to the Lancaster Estate. The oval's sports pavilion will need to be integrated with other development occurring in this location.</p>	<p>Refer to Section 4 of this submission for the design response in relation to the future use and development of land adjacent to the oval. The existing, heritage buildings adjacent to the oval are proposed to be retained. This maximises the views of the heritage buildings on the site from the eastern side of the oval and results in a uniform presentation of the heritage buildings from the surrounding residential development to the east. The community centre will serve the same purpose as was envisaged with the sports pavilion which was envisaged to be located adjacent to the oval.</p>
<p>The commercial/mixed-use precinct, while likely to contain a varied range of uses, will become an integrated precinct through the provision of careful design to ensure interface between all different uses is well designed and managed.</p>	<p>Precinct 1B is designed in response to the surrounding land uses and development, including the characteristics of Plenty Road. Refer to Section 4 of this submission for the design response, and further detail regarding the interface of Precinct 1B with the adjacent precincts of the PDP. A variety of land uses are proposed and are integrated through Precinct 1B.</p>
<p>1.9 Development and infrastructure contributions</p>	
<p>Development and infrastructure contributions are payable but depend on the nature of the proposed development.</p> <p>The developer will be responsible for:</p> <ul style="list-style-type: none"> ▪ Off site transport works (road, pedestrian, cycle) generated as a result of the development ▪ Off site landscape works on nominated roads, generated as a result of the development ▪ Community infrastructure contribution per dwelling/ retail and commercial floor area for the provision of a community facility within the Lancaster Gate development ▪ Construction of a sports pavilion to Council satisfaction 	<p>The development contributions will be assessed and determined at the future planning permit application stage of each precinct.</p> <p>Further discussions with Council are required in relation to this item.</p> <p>Refer to Section 6.3 for detail.</p>
<p>1.10 Conservation management plan</p>	

<p>A Conservation Management Plan has been prepared to guide the detailed design of additional buildings within the Heritage Precinct.</p> <p>Any plans for the development of the Heritage Precinct will require the preparation of a detailed heritage design response addressing the requirements of this conservation management plan.</p>	<p>Consideration has been given to the Conservation Management Plan for the site. In addition to this refer to Appendix D for a detailed heritage assessment of the PDP by Bryce Raworth (2007).</p> <p>Future heritage assessments of any proposed buildings and works will be undertaken, as necessary, to form part of any future planning permit application.</p>
<p>1.11 Tree preservation</p>	
<p>A significant tree survey must be prepared and endorsed by Council.</p> <p>This survey must show all significant trees and an appropriate design response having regard to the location and ongoing retention of these trees as part of the proposed development.</p>	<p>Tree surveys have been prepared for the site. Refer to Appendix E for details.</p> <p>The survey prepared nominates significant trees. Refer to Section 4.8 for tree removal and Figure 82 tree retention plan.</p>
<p>Where possible significant trees should be retained within open space, road reservations, tree reservations, walk ways and landscape areas.</p>	<p>Refer to the Tree Retention Plan (Figure 82) in Section 4.8 of this submission for details.</p>
<p>Each tree must have an identified tree protection zone. No development will be permitted within the tree protection zone without Council consent.</p>	<p>Refer to Sections 4.8 and 4.9 and Appendix E for details. This item can be satisfactorily addressed as a condition of permit at the planning permit stage of each precinct.</p>
<p>Tree protection barriers will be erected prior to any development occurring on the site. A Council officer will inspect barriers once erected.</p>	<p>This item can be satisfactorily addressed as a condition of permit at the planning permit stage of each precinct.</p>
<p>An Environmental Management Plan (EMP) will need to be submitted and be to the satisfaction of Council for any planning permits issued for the site.</p>	<p>The necessity of preparing an EMP will be determined through the planning permit application process and can be required by a permit condition. Refer to Section 4.10, page 65 for further details.</p>
<p>1.12 Traffic/engineering/landscape</p>	
<p>As part of Council's consideration of any amendment to the Larundel Development Plan a revised landscape, engineering, traffic and landscape plan must be submitted to Council to form part of any amendment approval.</p>	<p>A revised landscape, engineering, drainage and traffic plan has been submitted as part of this submission to Darebin City Council as part of the proposed PDP.</p>

Polaris 3083 Development Plan

5.3.3 Larundel Urban Village

Section 7 of the NECSP provides guidance for the 'Larundel Urban Village.' The PDP is consistent with the fundamental land use and development guidelines contained within the NECSP as it:

- Includes a mixed-use development (predominately located next to Plenty Road), medium density housing and a variety of residential land use types and a new activity centre on the site
- Provides for shop-top residential or office accommodation, with a retail use located on the ground floor along Main Street within Precinct 1B
- Allows opportunity for student housing to be located within the existing buildings within Precinct 2C or Precinct 1B (e.g. 1B-16)
- Facilitates high quality urban design and architectural integrity with a contemporary character
- Provides for opportunity for non-residential uses to be located within existing heritage buildings (e.g. the community centre use and the possibility of Linaker's Cottage to be used as a child care centre)
- Encourages higher density development that could be located within Precinct 1A and Precinct 2. The height of this development will respect the height and scale of the adjacent development. The height and scale of development will be assessed at the future planning permit stage of each precinct
- Provides for new and existing buildings to overlook public open spaces, including recreational spaces, streets, pedestrian and bicycle paths. Access will be provided to all buildings from the internal street network
- Encourages new buildings to be developed with a high degree of energy efficiency in accordance with current legislative requirements
- Nominates for Precinct 1B, the Polaris Neighbourhood Activity Centre, to be the first stage developed on the site in accordance with the Staging Plan located within Section 6.1 of this submission. This is consistent with the objective of the North East Corridor Plan which is for the early stages of development to create an attractive gateway to the site from Plenty Road
- Creates a permeable, legible, safe and attractive street network. The existing roads will be reused to allow the retention of trees alongside the site where possible to connect the new community with the history of the site
- Minimises expansive views of car parking and of the loading bays which serve commercial uses

- Allows for consideration to be given to the retention of exotic trees unless, as specified in the North East Corridor Strategy Plan, these trees place a significant restriction on development
- Retains the oval and the land surrounding the oval will be suitably landscaped and treated to provide an effective recreation space
- Adopts a flexible approach to the future use of the heritage buildings, in order to ensure their long term preservation
- Incorporates adequate stormwater drainage measures and the developer will provide appropriate improvements to the physical infrastructure
- Provides for a new community centre located within an existing heritage building adjacent to the civic plaza, neighbourhood activity centre and oval.

It is noted that Linaker's Cottage is not included in the NECSP. However, the PDP retains and incorporates this existing building.

The NECSP Framework Plan for Lancaster Gate nominates trees with a moderate or high conservation status to be retained and incorporated into developments. The Framework Plan within the NECSP does not identify any trees to be retained on the site.

The mixed-use 'work, shop, live, play' nature of the development necessitates a variety of building heights; from single level shops, to triple storey accommodation.

Buildings should not generally exceed three (3) storeys in height (11.5 metres) above ground level. Precinct 1A buildings should generally be of two (2) storeys above ground level. In Precinct 2C the building height should generally be of two (2) storeys above ground level so as to provide an appropriate transition to the nearby existing residential areas.

Refer to Section 5.3.4 for further details pertaining to Building Heights.

Polaris 3083 Development Plan

5.3.4 Building Heights

The guidelines of the NECSP relating to building heights advises that buildings should be no higher than three (3) storeys along Plenty Road and two (2) storeys elsewhere as indicated in the Framework Plan (p.31).

The Larundel Urban Village Framework Plan is based on a past design and development vision encompassing a wider area including the Lancaster Gate site and part of the La Trobe University site. This vision, as well as state planning policy has changed over time, including that for Polaris. The NECSP and Larundel Urban Village Framework Plan are meant to provide guidance only and are not intended to apply prescriptive control to future development within this area.

Consideration has been given to both the NECSP and the Framework Plan, as well as to the Plenty Road Corridor Urban Design Framework and incorporation of DDO17 into the Darebin Planning Scheme in June 2017. DDO17 specifies a six (6) storey upper range building height for properties located along Plenty Road and proximate to the Polaris Estate.

Future indicative building heights within Polaris are also informed by, and are responsive to the physical attributes of the site (such as topography), the proximity of proposed buildings, existing **and future building context** and roads on site and in adjoining areas (such as Lancaster Gate) and the scale, symmetry and height of existing heritage buildings on the site.

Importantly, the building heights of PDP have been informed by the contemporary, holistic and integrated design adopted for the entire site, rather than the application of a piecemeal precinct by precinct development response.

The PDP provides the opportunity for a range of building heights on the site. A conceptual indication of these heights is provided in Figure 86. Although generally anticipated to be between one (1) and three (3) storeys (above ground), there is capacity for greater overall building height at the south-west of the site, along Plenty Road and Main Drive (1B-01, 1B-02 and 1B-16). Three (3) storey developments have been constructed in the adjoining Lancaster Gate development site which also varies the building height guidelines identified in the NECSP.

The significant level changes of the topography over the site contribute to this varied building height. Significant view lines of the existing heritage buildings will be protected, and the building heights will integrate with the built form and scale of built form on adjoining land.

The indicative building heights as shown in Figure 86 have been developed with the objective to respect the significance of the existing heritage buildings. These buildings are generally two (2) storeys in height, however are equivalent or comparable in height to a modern three (3) or four (4) storey building due to their high ceiling to floor dimensions, steep pitched roofs, raised ground floor level above natural ground level and architectural symmetry and scale.

PMP9719

Generally, no building will exceed three (3) storeys in height above ground level. Where it is necessary to exceed a building height of three (3) storeys, for example, due to the slope of the land, the proposal must be accompanied by appropriate urban design support, to the satisfaction of the responsible authority.

Generally, no building will exceed 11.5 metres in height as measured from natural ground level to the satisfaction of the responsible authority. Where the building incorporates a pitched roof, the roof may exceed this height provided it does not contain any habitable space. Where it is necessary to exceed a building height of 11.5 metres in overall floor height for example, due to the slope of the land, the proposal must be accompanied by appropriate urban design support to the satisfaction of the responsible authority.

Precinct 1A buildings should generally be of two (2) storeys about ground level. In Precinct 2C the building height should generally be two (2) storeys above ground level so as to provide an appropriate transition to the nearby existing residential areas.

There is an opportunity for greater building height, up to seven (7) storeys, for Building 1B-01 and five (5) storeys for 1B-02. Both of these sites front Plenty Road, which has an indicative upper height of six (6) storeys proximate to the Polaris Estate site under the Plenty Road Corridor Urban Design Framework and DDO17. A six (6) storey height also applies to Lot 2D under the Village Precinct Development Plan. Both sites are identified as potential landmarks and focal points for the Polaris Town Centre, creating a sense of arrival and place for the activity centre. The sites are also located where the topographical values are lowest.

There is also opportunity for greater building height, up to seven (7) storeys, for Building 1B-16, given the slope of the Polaris Estate site (north to south and west) and the lowest topographic values being located along Main Drive. **Moreover, the built form context of the land to the south of Main Drive is evolving with Plenty Road Corridor Urban Design Framework identifying the existing service station and townhouses as a future mixed use - commercial area and La Trobe University proposing its own town centre development.**

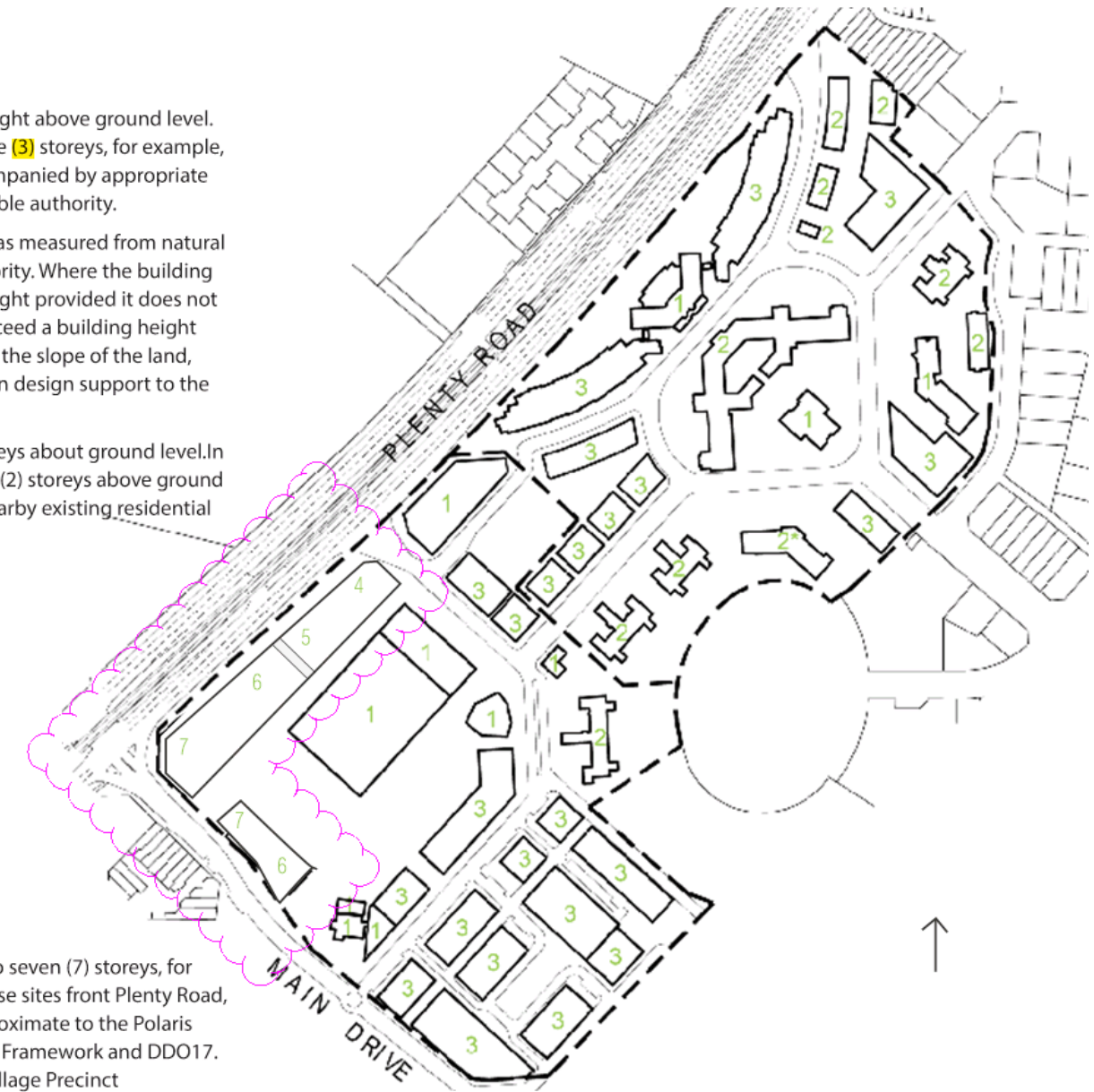


Figure 86 Indicative Conceptual Building Heights above ground (ie storeys)

Note (*)

Building 2C-04: Maximum two storey building height above ground is permissible provided the second storey is located within the roof space of the existing building.



DEAL CORPORATION

Polaris 3083 Development Plan

5.3.5 Front Setback from Plenty Road

Future buildings will be designed generally in accordance with the Urban Design Guidelines contained within Section 4.7 of this submission. The building heights shown in Figure 86 are indicative only and are subject to future detailed design and development which will be at planning permit stage, subject to the Responsible Authority's approval.

The NESCP (1998) does not nominate a specific front building setback to Plenty Road. However, the PDP is generally in accordance with the urban design guidelines of the NESCP (1998) as a high quality building frontage is proposed along Plenty Road, as varied front setbacks are proposed for all buildings fronting Plenty Road.

Ground level front setbacks from Plenty Road for buildings located within Precinct 1B vary between 0 metres and approximately 5.5 metres.

Front setbacks from Plenty Road for buildings located within Precinct 2D vary between a minimum of 2.5 metres to a maximum of 21.4 metres.

These front setbacks are indicative at this stage and are subject to further detailed design which will be included and assessed as part of future planning permit applications.

5.4 The Lancaster Gate Development Plan (2001, amended 2007)

It is the intention of the PDP to amend and replace parts of the *Lancaster Gate Development Plan (1998)* (LGDP) specific to the Polaris site. As such, development and design elements relating to Precinct 1B (formerly Mixed Use Precinct under the Lancaster Gate Development Plan), in relation to front setbacks from Plenty Road have been considered in context to the current holistic and integrated design approach of the PDP.

Under the LGDP it is nominated that buildings should be setback at least 10 metres from Plenty Road (p53). No specific front setback is nominated for buildings located within (proposed) Precinct 2D under the LGDP (formerly the Village Precinct under the LGDP).

The functionality of buildings located within Precinct 1B (ie the Polaris Neighbourhood Activity Centre), the layout and configuration of buildings and roads (both within the Precinct and across the entire estate), and the context of the precinct as part of the whole of the Lancaster development site significantly influences the front setback of buildings along Plenty Road.

In order to achieve a good design outcome and to promote the precinct's status as a Neighbourhood Activity Centre, a ground level front setback of between 0 and approximately 5.5 metres from Plenty Road is proposed. Refer to Figure 49b on page 40. A reduced front setback at the 'corner' locations within Precinct 1B creates a sense of place and a sense of arrival to the new Main Street of the Polaris Neighbourhood Activity Centre.

The very wide Plenty Road reserve fronting Precinct 1B demands some form of containment, as it is the 'front' of the Polaris Neighbourhood Activity Centre.

The Plenty Road setback frontage (along Precincts 1B and 2D) creates a setback rhythm of buildings coming forward to mark urban corners and buildings setback in between to further accentuate these landmarks. This setback response will ensure better activation of the street, promote passive surveillance, increase commercial opportunities for the Neighbourhood Activity Centre and ensure a more efficient use of land (ie a more sustainable outcome rather than the creation of a 'no man's land'), and a better and vibrant urban village character appropriate to this mixed use precinct.

Polaris 3083 Development Plan

6.2 Yield

It is envisaged that the future land use and development of the site will provide the following dwelling yields. These yields are indicative only and are subject to change. The final yield will be determined following detailed design of each building, and will be finalised at the planning permit stage of the development.

Precinct 1A - Accommodation

Approximately 69 dwellings, comprising both 2 and 3 storey townhouses with a variety of dwelling types (i.e. a variety of a number of bedrooms) is proposed within this precinct.

Precinct 1B - Accommodation

The following provides an indicative break down of potential dwellings envisaged within Precinct 1B:

- Home Office Apartments 19
- Shop-top apartments 20
- Student Accommodation 200

The home office apartments and shop-top apartments will be located adjacent to Main Street.

Precinct 2C - Accommodation

Student Accommodation: 125 beds

Dwellings/Apartments: A total of approximately 191 dwellings are to be provided within Precinct 2. This total will include a variety of dwelling types, including, 1, 2 and 3 bedroom dwellings.

A break down of the envisaged number of dwelling types is provided in the table below:

1 bedroom	56
2 bedroom	95
3 bedroom	40

Table 6.1.1a: Indicative Dwelling Type breakdown for Precinct 2C (Source: HASSELL, 2008)

Precinct 2D - Accommodation

Precinct 2D is comprised of a variety of room types within the future retirement village to be developed within the precinct.

	Studio	1 Bed	2 Bed	Total
GND	9	24	5	38
L1	10	27	6	43
L2	10	28	6	44
Managers	-	1	1	2
TOTAL UNITS	29	80	18	127
TOTAL BEDS	29	80	36	145

Table 6.1.1b: Indicative Room Type breakdown for Precinct 2D (Source: Gracious Living/Petrac, 2007)

Planning and Environment List Administrative Division

General form of application to the Planning and Environment List

Making an application

Information that must accompany your application

- Details of the Act and the section of the Act under which the application is made
- Copy of the decision by the responsible authority or other relevant authority (where relevant)
- Statement of Grounds, if a separate sheet is necessary
- The names and addresses of each joint applicant in decipherable form, and their signature, if this is a joint application

If the application relates to a dispute about a proposal to amend a plan or document, clearly identified copies of the original and proposed plan or document must be attached to the application. The proposed changes must be clearly identified on the proposed plan or document and a schedule listing the proposed changes must also be attached.

If your application is incomplete and you do not include all the material and information required, plus payment of the fee, your application may be rejected or struck out.

What will happen after you lodge your application?

After you lodge your application, the Tribunal may allocate a hearing date to hear the application or may allocate a date for a practice day hearing to consider the application and its future conduct. The Tribunal will issue an initiating order setting out the date for the hearing or the practice day hearing and giving directions about steps that you and other parties to the proceeding must comply with. The initiating order will set dates by which each step must be completed. A date will not be allocated until your application is complete.

If you fail to comply with the Tribunal's initiating order by the dates specified in the order, your application may be struck out without further notice. If you are unable to comply with any aspect of the initiating order, you must apply to the Tribunal in writing for an extension of time. You cannot change dates without an order by the Tribunal.

The Tribunal relies on the accuracy of the information you provide with your application to calculate the length of time required for the hearing. If you believe the time allocated is insufficient or too much, please advise the Tribunal in writing as soon as possible after receiving the initiating order.

**Planning and Environment List
Administrative Division**

VCAT Reference Number (Office Use Only) /

Name of Applicant(s)

Full Name

Is this a joint application? Please tick only one box

No Yes (If yes, refer to Planning and Environment List Guidelines for Making Joint Applications available on the VCAT website)

If this is a joint application, you must attach the names and addresses of each joint applicant in decipherable form, and their signature. Each joint applicant must be entitled to make the application. If you are applying for a fee waiver, all joint applicants must be eligible.

Address and Contact Details of Applicant(s)

All correspondence will be sent to the applicant(s) at this address or email, unless the 'Details of Representative' section is completed, in which case all correspondence will be sent to the representative address or email. Complete only one section. DO NOT COMPLETE BOTH SECTIONS. Address for service must be a Victorian address – VCAT Rules 2008 Order 4.08.

Address for service
Represented – see below.

Business phone	<input type="text"/>	After hours phone	<input type="text"/>
Mobile	<input type="text"/>	Fax number	<input type="text"/>
Email	<input type="text"/>		

Details of Representative

Name/Firm	<input type="text" value="Best Hooper Lawyer"/>
Reference Number/ Contact person	<input type="text" value="190151 / John Cicero/Eliza Minney"/>

Address for service

Business phone	<input type="text" value="03 9670 8951"/>	After hours phone	<input type="text"/>
Mobile	<input type="text"/>	Fax number	<input type="text" value="03 9670 2954"/>
Email	<input type="text" value="eliza.minney@besthooper.com.au"/>		

Statutory basis of application

Specify the Act and the section under which the application is made.

Act	<input type="text" value="Planning and Environment Act 1987 (Vic)"/>
Section	<input type="text" value="149(1)(a)"/>

Details of other parties

Who made the decision you want reviewed?

List the name of the responsible authority or other relevant decision-making authority responsible for the decision or who has failed to make a decision (when the application is to review failure)

Responsible authority

Darebin City Council

Relevant authority

Who is the order to be made against?

List the names and addresses of the persons or firms to be subject to the order. Attach a separate list if necessary.

Respondent 1 – name

Darebin City Council

Address

274 Gower Street, Preston, Victoria 3072

Respondent 2 – name

Address

Respondent 3 – name

Address

Are there any other persons who may have an interest or wish to be heard in respect of the application?

Attach a separate list if necessary.

Person 1 – name

Address

Person 2 – name

Address

Person 3 – name

Address

Information about the land to which this application applies

If there is no conventional address (street number, street and locality) insert a land description:

- Certificate of Title volume and folio;
- Lot and lodged plan number;
- Crown allotment; or
- Section number /Township/Parish.

Address or description of the land

1056 – 1140 Plenty Road, Bundoora

Details of the proposal

Application number, if known or relevant

Not known

Description of what is proposed

To amend the Polaris Development Plan, endorsed on 21 January 2013

What other factors are relevant to the application?

Set out details of any planning permit, licence, works approval, section 173 agreement or the like to which the application relates or which are related to the proposal, any related VCAT proceeding or decision, or any other relevant factor. Attach a separate list if necessary.

Polaris Development Plan, endorsed on 21 January 2013

Facts relied on (Statement of Grounds)

Set out a short statement of the grounds you rely on to support your application.
 Attach a separate sheet if necessary.

1. The proposed amendments to the Polaris Development Plan (endorsed on 21 January 2013) are appropriate and consistent with the guidelines for consideration of development plans contained in Schedule 1 to the Development Plan Overlay.
2. The proposed amendments represent a proper, orderly and acceptable planning outcome.

Attachments

Attach the following documents to this application. Tick and reference the attachments provided.
 Attach a separate schedule of attachments if necessary.

<input type="checkbox"/> Copy of the disputed decision (where relevant)	Ref. No.	
<input type="checkbox"/> Copies of related reports	Ref. No.	
<input checked="" type="checkbox"/> Copies of related correspondence	Ref. No.	2
<input type="checkbox"/> Statement of Grounds, if a separate sheet is necessary	Ref. No.	
<input type="checkbox"/> The names and addresses of each joint applicant in decipherable form, and their signature, if this is a joint application	Ref. No.	
Other attachments (if relevant)		
<input checked="" type="checkbox"/> Endorsed Polaris Development Plan	Ref. No.	1

Hearing time and complexity

Estimate the time it will take you to present your complete case at the hearing (submissions plus witnesses) and the number of expert witnesses you intend to call, if any.

Time to make submissions & present evidence: 1 day Hours Minutes

Number of witnesses: Expertise:

See VCAT's Practice Note PNVCAT2 – Expert Evidence for information about the obligations of expert witnesses and what must be included in the report of an expert witness.

Are you seeking a compulsory conference?

Yes No

A request for compulsory conference does not always mean that VCAT will schedule one. On the other hand, VCAT may schedule a compulsory conference even if parties do not request it.

Acknowledgement

I acknowledge that:

- To the best of my knowledge, all information provided in this application is true and correct and that no details relevant to the application have been left out.
- I approve the information that has been provided.
- It is an offence under section 136 of the Victorian Civil and Administrative Tribunal Act 1998 to knowingly give false or misleading information to VCAT.

Category of person completing this application:

Applicant in person Authorised Representative

Name of person completing this application (print)

Date

Checklist

Before you lodge this application, make sure that:

- You have completed all details and responded to all questions.
- All documents regarding fees (e.g. fee waiver documents, credit card form or cheque) are supplied with your application.
- You have attached and properly referenced all of the attachments listed in this form.

Fees

You must pay the relevant application fee at the same time you lodge this form. Fees may change each year. To find out about the current fee, visit the VCAT website www.vcat.vic.gov.au or call VCAT on 1300 01 8228.

VCAT may waive the requirement to pay a fee in some instances. Details are available on the VCAT website. You should apply for a waiver at the same time that you lodge this form.

Application Fee	\$1,213.80
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Fees can be paid via money order, cheque, cash or credit card.

Money orders and cheques are to be made payable to 'VCAT'. Cash payments will only be accepted if you are delivering this application in person to VCAT. Do not send cash in the mail. If faxing the application, you can only pay by credit card.

If you wish to pay the fee by credit card, please fill in the details on the last page.

About VCAT Fees

VCAT fees are charged according to three levels:

- Corporate fees for businesses and companies with a turnover of more than \$200,000 in the previous financial year, corporate entities and government agencies
- Standard fees for individuals, not-for-profit organisations, and small businesses and companies with a turnover of less than \$200,000 in the previous financial year. Companies must provide a statutory declaration to support this claim
- Concession fees for people who hold the Australian Government Health Care Card. You must provide a copy of your card with your application

To find out if you need to pay an application fee and how much it costs, visit the fees page at www.vcat.vic.gov.au

Fee Relief

We can reduce or not charge (waive) a VCAT fee in certain circumstances.

Some people are automatically entitled to a full fee waiver. You can also apply for fee relief if paying the fee would cause you financial hardship.

Visit vcat.gov.au for more information about fee relief

Are you applying for fee relief?

- No – complete **Fee payment** section

VICTORIAN CIVIL AND ADMINISTRATIVE TRIBUNAL

ADMINISTRATIVE DIVISION

PLANNING AND ENVIRONMENT LIST

VCAT REFERENCE NO. P295/2019

APPLICANT	Deal Corporation Pty Ltd
RESPONSIBLE AUTHORITY	Darebin City Council
SUBJECT LAND	1056-1140 Plenty Road BUNDOORA VIC 3083
WHERE HELD	Melbourne
BEFORE	Helen Gibson AM, Deputy President
HEARING TYPE	No hearing
DATE OF ORDER	5 March 2019

ORDER**Hearing details**

Item	Hearings	Date/Detail
A	Hearing date	22 August 2019
	Commencement time	10:00 AM
	Duration	2 Days
	Location	55 King St, Melbourne
	Major Cases List	No
	Short Cases List	No
B	Compulsory conference date	24 June 2019
	Commencement time	2:00 PM
	Duration	0.5 Day
	Location	55 King St, Melbourne

- 1 This proceeding is listed for a hearing and a compulsory conference in accordance with the details set out in **items A & B** above. Any changes to the hearing or compulsory conference details will be notified by further order of the Tribunal.

Other key dates

- 2 The following dates are the dates referred to in these orders.

Initiating order General Application – hearing plus compulsory conference 2017

Item	Action	By no later than	Order number
C	Applicant to serve application and specified documents on responsible authority/relevant authority	14-March-2019	3
D	Applicant to serve documents on respondents and others	14-March-2019	4
E	Responsible authority to file PNPE2 material	21-March-2019	7
F	Applicant to file statement of service with VCAT	28-March-2019	5
G	Closing date for respondents to lodge statement of grounds with VCAT and serve on applicant and responsible authority/relevant authority	04-April-2019	4 & 9

ACTIONS REQUIRED – APPLICANTS

Service of application

- 3 By no later than the date set out in **item C of order 2**, the applicant must serve on the responsible authority/relevant authority:
 - a copy of the application and all attachments;
 - other material filed with the Tribunal; and
 - a copy of this order.
- 4 By no later than the date set out in **item D of order 2**, the applicant must serve the following documents on all respondents and any other persons with an interest named in the application, plus the owner and occupier of the land if they are not the applicant or already named as respondents or persons with an interest::
 - the application, including all attachments and other material filed with VCAT;
 - a copy of this order;
 - a cover letter, which must:
 - explain that an application has been lodged and VCAT has directed service of the documents
 - specify the closing date, which is set out in **item G of order 2**, by which a statement of grounds must be received by VCAT and served on the responsible authority/relevant authority and applicant
 - explain that a statement of grounds may be lodged with VCAT as described in Appendix A

- specify the date and other details scheduled for hearing as set out in **items A & B of order 1**

Statement of service

- 5 By no later than the date set out in **item F of order 2**, the applicant must file with VCAT:
- a completed statement of service;
 - a list of names and addresses of all persons and authorities served; and
 - a sample of the cover letter sent with the documents served.

If a statement of service is not filed by the specified date, this application may be struck out without further notice. No reminder will be sent.

Hearing fees

- 6 If you are the applicant in this proceeding, you may be required to pay a daily hearing fee before the hearing commences. For more information, see Appendix A and the VCAT website.

ACTIONS REQUIRED – RESPONSIBLE AUTHORITY/RELEVANT AUTHORITY

Practice Note 2 material

- 7 By no later than the date set out in **item E of order 2**, the responsible authority/relevant authority must file with VCAT information about the application as specified in the current version of **VCAT Practice Note PNPE2 – Information from Decision Makers**, unless this material has already been filed in another related proceeding.
- 8 The responsible authority/relevant authority must comply with **VCAT Practice Note PNPE2 – Information from Decision Makers** and serve a copy of the completed table (excluding attachments) on the applicant.

ACTIONS REQUIRED – RESPONDENTS

Statement of grounds

- 9 If you are entitled to become a party to this proceeding and wish to contest this application, you must, within the time specified in **item G of order 2** (and also set out in the applicant's cover letter):
- lodge a copy of your statement of grounds with VCAT, indicating that a copy was served on the applicant and the responsible authority/relevant authority and the date(s) this was done. You must ensure your statement of grounds is received by VCAT by the specified date;
 - serve a copy of your statement of grounds on the person making this application (the applicant);
 - serve a copy of your statement of grounds on the responsible authority/relevant authority.

Information about how to lodge a statement of grounds with VCAT is contained in Appendix A of this order.

ACTIONS REQUIRED – ALL PARTIES

Communicating with VCAT

- 10 All communications to VCAT about this proceeding must quote the VCAT reference number included in this order (see page 1, top right corner).
- 11 A party who communicates in writing with VCAT must serve a copy of that communication on all other parties at the same time and notify VCAT this has been done. The communication must state that a copy was sent to the other parties and list the parties it was sent to. This applies to communications sent by letter, fax or email.

COMPULSORY CONFERENCE

Referral to compulsory conference

- 12 This application is referred to a compulsory conference in accordance with the details set out in **item B of order 1**. Pursuant to section 84 of the *Victorian Civil and Administrative Tribunal Act 1998* the following directions are given for the compulsory conference:
 - a A natural person who is a party to the proceeding must attend the compulsory conference either in person or by a representative who has authority to settle the proceeding on behalf of that party;
 - b Each body corporate which is a party (including a responsible authority, referral authority or statutory authority) must attend the compulsory conference by a representative who has authority to settle the proceeding on behalf of that party;
 - c If the application or permit application subject to the proceeding was made in the name of a person as agent or consultant for a principal, the principal must either attend the compulsory conference in person or by a representative who has authority to settle the proceeding on behalf of the principal;
 - d Each party should commence the compulsory conference with a short written statement briefly describing the most important issues in dispute from their perspective and what potential solutions they would consider.
- 13 Each party must complete an Opening Statement prior to the date of the compulsory conference and give it to the member on the day. A blank Opening Statement is included in Appendix B.

Helen Gibson AM
Deputy President



APPENDIX A

IMPORTANT INFORMATION FOR ALL PARTIES

Non-compliance with orders and directions

Please read the orders and following information carefully. Non-compliance with these orders may result in an application being struck out, or other sanctions under the *Victorian Civil and Administrative Tribunal 1998*, including an award of costs.

Major Cases List

If this proceeding has been included in the Major Cases List, **VCAT Practice Note PNPE 8 – Major Cases List (Planning)** governs the procedures for matters in this list.

Short Cases List

If this proceeding has been included in the Short Cases List, **VCAT Practice Note PNPE7 – Short Cases List** governs the procedures for matters in this list.

Statement of grounds

If you are a respondent, a person with an interest named in the application, or the owner or occupier of the subject land, you may be entitled to become a party to this proceeding if you file and serve a statement of grounds in accordance with this order, unless you indicate that you do not intend to participate in the hearing of the proceeding.

If you file a statement of grounds and indicate that you do not intend to participate in the hearing of the proceeding, you will not be a party to the proceeding but your statement of grounds will be considered at any contested hearing of the proceeding.

Fees apply if you intend to appear or present a submission at the hearing. If you do not pay the fee at the time of lodging your statement of grounds, you will not be a party and will not be entitled to take part at the hearing or any compulsory conference, even if you indicated that you wanted to participate. For information about fees and fee relief, visit www.vcat.vic.gov.au.

Your statement of grounds should provide sufficient information so that the issues you intend to raise can be clearly identified.

Lodge your statement of grounds online with VCAT

You may lodge your statement of grounds online with VCAT. Go to www.vcat.vic.gov.au. Alternatively, you may access a blank statement of grounds form on the VCAT website.

Withdrawals

If you are a respondent and wish to withdraw your objection at any time after you lodge your statement of grounds, you must inform VCAT, the applicant and the responsible authority/relevant authority in writing. You will then be withdrawn from VCAT's record and receive no further correspondence.

If you are the applicant, any application for withdrawal of the application must follow the procedures set out in **VCAT Practice Note PNVCAT1 – Common Procedures**.

Adjournments

Applications for adjournment of a hearing are not encouraged and you should not expect that an adjournment will be granted even if all parties consent.

VCAT may refuse an adjournment if it considers that the adjournment is:

- not in the public interest
- prejudicial to the interests of one or more parties or the expeditious determination of the proceeding
- contrary to efficient case management, or
- otherwise not justified.

If you wish to apply for an adjournment, choose the **Application for Consent to adjournment** form on the VCAT website or call us for a hard copy. Any application for adjournment must follow the procedures set out in **VCAT Practice Note PNVCAT1 – Common Procedures**. Refer to the VCAT website for more detail.

Providing your email address

VCAT will send copies of all orders, including the final decision, to a party's email address. VCAT will not send a hard copy in addition.

A party's email address will be available on VCAT's records for other parties to access. If you do not want other parties to have access to your email address, do not provide your email address. If VCAT does not have your email address, you will be sent a hard copy of any order, but you may receive it after other parties have received the order by email and after the order has been published.

VCAT does not accept any responsibility for emails not received due to changes in address, firewall or other security measures that may be attached to your email account.

Inspecting VCAT's file

Any party to the proceeding may inspect the VCAT file relating to the proceeding without a fee. Anyone else can inspect the file, but they must pay a fee. There is a charge for photocopying. To arrange an inspection time, complete the **File and Document Access Request Form** available on the VCAT website.

Hearing fees and waiver or reduction of hearing fees

If you are the applicant in this proceeding, you are required to pay a daily hearing fee for each day or part day of hearing. This includes accompanied site visits.

The amount of fee will depend on whether your case is deemed to be a complex case. You will be advised if your application is considered a complex case by the Registrar in advance and prior to the hearing. If your application is deemed a complex case you will be required to pay higher hearing fees. See VCAT's website for information on hearing fees for complex cases.

You are required to pay your fee online or at Counter Services on the ground floor at 55 King Street the day before the hearing. If you do not pay the fee, your hearing may be adjourned or struck out.

There are some exceptions to this requirement to pay hearing fees. **VCAT Practice Note – PNVCAT 6 Hearing Fees** sets out the procedures that apply to the charging of daily hearing fees in proceedings before the Tribunal, including who is liable to pay the daily hearing fee. This Practice Note and other information is available on VCAT's website.

For example, Day 1 hearing fees do not apply to a VicSmart application or a change in conditions application unless an objector application is also lodged, in which case the permit applicant must pay the Day 1 hearing fee. If an objector application is transferred into the Major Cases List, the permit applicant must pay any hearing fee.

The Principal Registrar has the power to waive or reduce a daily hearing fee in certain circumstances. An **Application for Fee Relief** form is available on VCAT's website. Any application to waive or reduce the daily hearing fee should be made as soon as possible in order to avoid any delay in the hearing.



APPENDIX B

COMPULSORY CONFERENCE INFORMATION SHEET

What is a compulsory conference?

A compulsory conference is a confidential meeting at which parties, with the assistance of a Tribunal member, can explore options to reach an agreed settlement on all or some of the matters at issue in a proceeding. If it is to be successful, parties must approach the compulsory conference with an open mind and be flexible in considering options that could resolve the case. More information about compulsory conferences is available on VCAT's website at www.vcat.vic.gov.au and in **VCAT Practice Note PNVCAT4 – Alternative Dispute Resolution (ADR)**.

Requirement to attend compulsory conference

If you are a party, you must attend the compulsory conference in person or by a representative who has authority to settle the proceeding on your behalf.

What happens if you do not appear?

If you do not appear or are not represented at the compulsory conference, the matter may be resolved in your absence and you may be struck out as a party. If all the parties present at the compulsory conference agree, the Tribunal may make a final order or make other orders affecting your interests. In particular, the Tribunal may make any of the following orders under the *Victorian Civil and Administrative Act 1998*:

- If you are the applicant to VCAT, your application may be dismissed or struck out.
- If you are not the applicant, you may be struck out as a party to the proceeding. This means that you can take no further part in the compulsory conference or hearing.
- You may be ordered to pay costs in certain circumstances.

Who may attend a compulsory conference?

Only parties to the proceeding may attend a compulsory conference. This means, for example, that people who were late in lodging their statement of grounds or who have indicated that they do not intend to participate in the hearing of the proceeding, and who are not therefore parties to the proceeding, cannot participate in the compulsory conference. It also means that a person who is struck out as a party because they do not attend the compulsory conference, is not entitled to participate in any further compulsory conference and their consent is not required to any orders made to settle the proceeding in whole or in part.

What happens if the compulsory conference is successful?

If the compulsory conference is successful and results in an agreement by the parties present at the compulsory conference, the Tribunal may make a final order to give effect to the agreement without a hearing being required.

What happens if the compulsory conference is not successful?

If the compulsory conference does not succeed and the parties present fail to reach agreement, the Tribunal will make an order listing the matter for hearing. The hearing will proceed on the date specified in the Tribunal's initiating order or other dates or details as to time or duration may be specified.

What happens if the compulsory conference is partially successful?

If the parties present reach agreement about some issues but not others, the hearing will proceed. If the parties present agree, the Tribunal may make an order that limits the issues to be considered at the hearing or excludes certain issues from consideration. The Tribunal may make further orders that restrict the ability of parties to contest at the hearing any matters resolved at the compulsory conference.

What should you bring to the compulsory conference?

Please complete the Opening Statement at Compulsory Conference form, which is enclosed, and bring it with you to the compulsory conference. Do not file it with the Tribunal prior to the compulsory conference, as it is confidential. The applicant should bring an extra copy of any relevant plans including elevations.



**PLANNING AND ENVIRONMENT LIST
OPENING STATEMENT AT A COMPULSORY CONFERENCE**

VCAT reference number	
Applicant	
Responsible/Relevant authority	
Other parties	

Each party must complete this opening statement prior to the date of the compulsory conference and give it to the member on the day. The statement must not be filed with VCAT prior to the compulsory conference because it is confidential. Its purpose is to clarify the respective positions of each of the parties to the application by identifying the issues that the parties consider most important to them and possible solutions.

Name of party signing form	
-----------------------------------	--

Most important issues in dispute from your perspective (including any legal matters) Attach additional pages if required

What potential solutions would you consider? Attach additional pages if required

Signature: _____ **Date:** _____



PLANNING AND ENVIRONMENT LIST

STATEMENT OF SERVICE

To be completed by or for the Applicant

Subject Land	1056-1140 Plenty Road BUNDOORA VIC 3083	VCAT Ref: P295/2019
---------------------	--	----------------------------

I,(Print full name)
of(Print name of firm, if relevant)
.....(Print address)
.....

STATE to the Victorian Civil and Administrative Tribunal (VCAT) that:

1. On (***date of service***) I served a copy of the application and all attachments and other material filed with the Tribunal and a copy of the initiating order on the responsible authority/relevant authority.

2. On (***date of service***) I served the following documents on each of the persons specified below by post*/by email*/in person*. (****delete whichever does not apply***)

Documents served:

- a cover letter;
- a copy of the application (excluding attachments) and any other relevant documents required to be served by VCAT’s initiating order;
- a copy of the VCAT initiating order;

Persons served: (*tick as appropriate*)

- all respondents named in the application
- other persons with an interest named in the application
- owner of the land, if not the applicant or respondent
- occupier of the land, if not the applicant or respondent
- any other person or authority

3. I attach copies of the following documents.

Documents attached: (*tick as appropriate*)

- list of names and addresses of all persons or authorities served
- copy of sample cover letter sent with documents served

I understand that knowingly giving false or misleading information to VCAT may result in imprisonment or fine (section 136 of the *Victorian Civil and Administrative Tribunal Act 1998*).

SignatureDate.....

**5.2 DEVELOPMENT PLAN POD/1/2007/F
1056-1140 AND 1142 PLENTY ROAD, BUNDOORA
(POLARIS - FORMER LARUNDEL PSYCHIATRIC
HOSPITAL)**

Author: Principal Planner

Reviewed By: General Manager City Sustainability and Strategy

Applicant	Owner	Consultant
SJB Planning	Deal Corporation P/L (Dealcorp)	SJB Planning Clarke Hopkins Clarke Architects

SUMMARY

- It is proposed to amend the Village Precinct Development Plan (**VPDP**) approved under the provisions of the Development Plan Overlay – Schedule 1.
- This decision relates only to a small part of the Polaris Estate – Lot 9 – Stage 3. There is an existing planning permit which has been approved for a 4 storey apartment building.
- The developer now wishes to instead build 2 storey townhouses rather than the 4 storey apartment building. To allow this, an amendment is required to the Development Plan and subsequently, an amendment to the Planning Permit will also be required. An application for an amendment to the Planning Permit has been lodged separately and would be considered under delegation should this Development Plan amendment be approved.
- The site is in the Mixed Use Zone (**MUZ**) and covered by (either wholly or in part) by the Development Plan Overlay – Schedule 1 (**DPO1**) and the Heritage Overlay (HO107, HO108 and HO111).
- A series of Section 173 Agreements apply across the land.
- The proposal is generally consistent with the provisions of the DPO1.
- It is recommended that the application be supported.

CONSULTATION:

- Notice of the proposed amendment was not given as the proposal reduces the height of the proposed building and the previous 4 storey proposal was not subject to any submissions. See details below.
- This application was not referred to internal units for review.
- This application was not required to be referred to external authorities.

Recommendation

That amendment POD/1/2007/D be supported and the amended Village Precinct Development Plan be approved.

INTRODUCTION AND BACKGROUNDSite History – Lancaster Gate

After the Hospital closed in 1999, Places Victoria (then known as the Urban and Regional Land Corporation (URLC)), purchased the Lancaster Estate for redevelopment.

In November 2001, Council approved the Lancaster Gate Development Plan. This plan was prepared by the URLC (now Places Victoria) and divided the Lancaster Estate into three parts; a Residential Precinct (approx. 15.38h hectares), the Village Precinct which included the former hospital buildings and the Mixed Use Precinct (approx. 10.61 hectares).

The Residential Precinct was subsequently subdivided, developed into individual dwelling lots and sold off.

The residual Village Precinct and Mixed-Use Precinct (now Town Centre) were sold by Places Victoria (then VicUrban) via expression of interest in 2006. These precincts now form 'Polaris'.

Polaris

The Polaris 3083 Development Plan (**PDP**) was approved in its current state on 23 September 2009, and subsequently amended 8 October 2012. Development under the PDP commenced in 2010 comprising the mixed use town centre.

This is a general, over-arching document which sets out principles on how the site should be developed including mix of uses, building height, general layout and form, tree retention and removal, car parking and other matters.

The VPDP provides more specific guidance for the redevelopment of the residential lots in the north-east section of the site. It generally sits alongside, and is to be read in conjunction with the PDP.

Various permits have been issued for the Village Precinct under the VPDP, including:

- Planning Permit D/759/2014 for Lot 7;
- Planning Permit D/264/2014 for Lot 10;
- Planning Permit D/400/2016 for Lot 9;
- Planning Permit D/800/2017 for Lot 2D; and
- Planning Permit D/532/2018 for Lot 8.

ISSUES AND DISCUSSION**Subject site and surrounding area**

- The subject site is the Polaris Site (Lancaster Gate) comprising the former Larundel Psychiatric Hospital in Bundoora.

- The site is currently being redeveloped and comprises two (2) sub-precincts – the Town Centre and the Village Precinct.
- The Village Precinct (to which this amendment is relevant) relates to the residential area on the north-east side of the site, generally comprising Lots 7, 8, 9, 10 and 2D (refer Figure 1).
- Lot 9 comprises 7,562 square metres and benefits from frontages to Gonella Crescent, Quasar Court, Nova Circuit and Sanctuary Drive.
- Lot 9 is intended to be developed in three (3) stages as set out in Planning Permit D/400/2016:
 - Stage 1 will comprise the central 2 storey townhouses;
 - Stage 2 will comprise the heritage apartments;
 - Stage 3 presently comprises a 4 storey apartment (and proposed to be 2 storey town as part of this application).

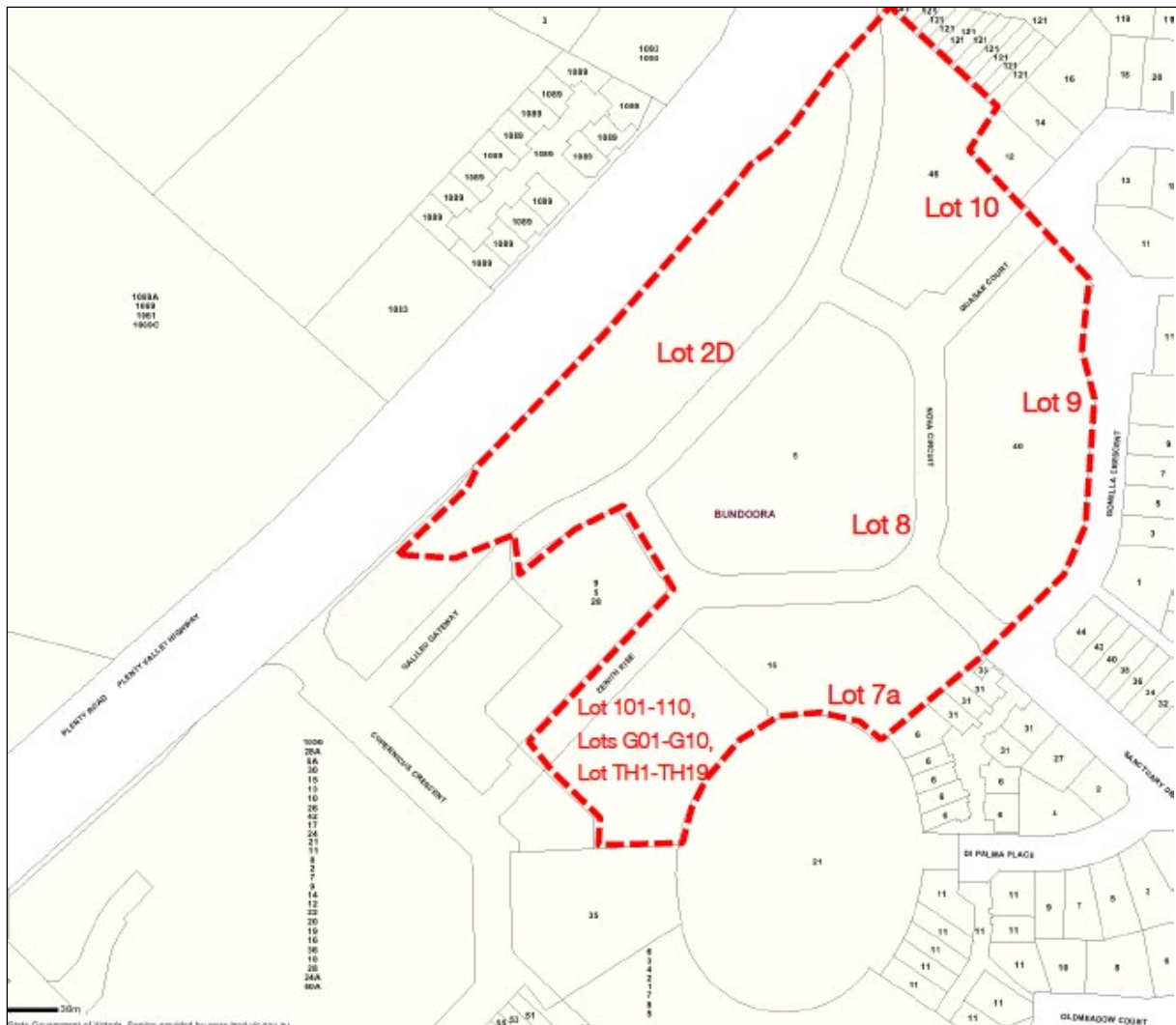


Figure 1 – Polaris Village Precinct

Proposal

It is proposed to amend the approved VPDP, primarily with respect of Lot 9 as follows:

- Lot 9: Change of the proposed building typology (at the Stage 3 / southern end) from a 4 storey apartment building to 2 storey townhouses;
- Lot 9: Nomination of setbacks from Gonella Crescent, Sanctuary Drive and Nova Circuit not previously nominated in the VPDP (albeit marginally reduced from those approved under Planning Permit D/400/2016);
- Lot 9: Consequential changes to the vehicle access arrangements within the site, including removal of the apartment building basement;
- Other consequential changes to the Village Precinct Development Plan to reflect changes introduced under Amendment VC148.

The proposed seeks to amend the VPDP to accord with the Section 72 amendment application presently afoot with Council.

Notice

- Notice of the amended VPDP was not given as the proposed amendments seek to reduce the scale and intensity of the redevelopment of Lot 9 (i.e. from 4 storeys to 2 storeys). This is primarily due to the proposal being significantly reduced in density and likely to have less impact than current approved plan.

PLANNING ASSESSMENT

The principle requirement of the DPO1 is that the Development Plan should address the elements of the North East Corridor Strategic Plan relevant to the site. It is important to note that this plan has been largely superseded by the work undertaken through amendment C137 for the Plenty Road Corridor, particularly in relation to height controls.

Sub-clause 1.0 of the DPO1 sets out the requirements for what should be included in the development plan, which have been provided for in the amended VPDP submitted by the applicant.

Sub-clause 2.0 sets out the guidelines for the consideration of the development plan, including:

The responsible authority will discourage industrial uses from locating on the land.

The responsible authority should consider whether the development plan or permit application is consistent with the North East Corridor Strategic Plan.

Any applicable heritage study and any applicable conservation policy should be considered. Whether the location, bulk, height and appearance of any proposed buildings or works will be in keeping with the character of the area.

Whether any proposed landscaping or removal of vegetation will be in keeping with the character and appearance of adjacent buildings, the streetscape or the area.

The layout and appearance of areas set aside for car parking, access and egress, loading and unloading and the location of any proposed car parking.

Whether subdivision will result in development which is not in keeping with the character and appearance of adjacent buildings, the streetscape or the area will adversely affect the significance, character or appearance of the heritage place.

This North East Corridor Strategic Plan (**NECSP**) is the current key reference point in the Scheme for the development of this site. The strategic vision of the plan that, amongst other things, the Larundel Urban Village be redeveloped to accommodate medium density

residential development with mixed use development fronting Plenty Road (ancillary retail premises outside the local centre).

Varied forms of medium density housing are contemplated, including townhouses and apartments, in order to maintain a level of activity and safety in the area at night. The Larundel Urban Village is also identified as the most appropriate area for student housing – potentially in the heritage buildings. Like uses should face each other across every street.

With regards to vegetation the development should seek to retain as many trees as possible. Exotic trees of importance to the area should also be protected unless they place a significant restriction on development. All development proposals must be based on an accurate survey of existing native trees and provide justification for the removal of any native trees. Where possible, the development should be planned to incorporate trees identified for protection within the public domain. Any trees identified for protection remaining on private lots should be protected by appropriate building siting controls.

With regards to heritage, heritage buildings should be retained and reused unless it is proved to be financially unfeasible. Historic interpretation should be promoted through the display of information about the site and buildings' history.

Under the current application to amend the development plan, the considerations are far more discrete, namely:

- The change in land use/typology;
- Built form changes; and
- Consequential changes as a result of the above.

Land Use and Building Typology

The approved VPDP present proposes a 4 storey apartment building within the Stage 3 (southern portion) of Lot 9. This building has been approved under Planning Permit D/400/2016.

The Applicant has lodged a concurrent Section 72 amendment application to develop the Stage 3 area for 2 storey townhouses in lieu of the approved 4 storey apartment building. This change has been motivated by a shift in market conditions that favours demand for townhouses. Save for this amendment proposal to the VPDP, the Section 72 amendment presently afoot would need to be refused if for other reason than it is not 'generally in accordance' with the VPDP.

The land use remains residential (dwellings) which is consistent and appropriate for the village precinct.

The NECSP entertains a range of housing types across the precinct provided they contribute to the delivery of medium density housing. The proposed townhouses evidently fit within this vision. A diversity of housing types will be delivered within Lot 9 – namely the heritage apartments and townhouse typologies. More importantly however it is the precinct as a whole which is charged with achieving this objective. An appropriate mix of housing typologies within the broader PDP area continues to be maintained as part of this amendment.

Built Form Matters

The proposed amendments to the VPDP include changes the built form outcome envisaged on Lot 9.

Fundamentally, the proposed built form in the southern extent of Lot 9 will reduce from 4 storeys to 2 storeys. This will ultimately result in less impact to the public realm and surrounding area.

The amended VPDP now nominates setbacks from the various street frontages as follows:

- Gonella Crescent – 1.6-2.3 metres;
- Sanctuary Drive – 2 metres; and
- Nova Circuit – 2 metres.

There are no setbacks for Lot 9 nominated in the approved VPDP. Therefore it cannot be said whether the setbacks now provided are fundamentally different to the current VPDP, albeit they are generally a reduction on what has been approved for the 4 storey apartment under Planning Permit D/400/2016.

In any event, the setbacks proposed are considered an appropriate treatment for 2 storey townhouses and will be consistent with the setbacks provided (and recently approved by Council) on the adjacent Lot 8.

Consequential Matters

The proposed amendments to the typology and built form results in consequential changes to the traffic and access arrangements. The vehicle crossover from Nova Circuit will be retained in its current location. Given the typology change, a series of rear loaded garages will be provided to the townhouses in lieu of a common basement. As this will be sleeved behind the activated street frontages, this is an acceptable variation.

No change is proposed to the tree removal/retention outcome already approved under Planning Permit D/400/2016.

Other Amendments

The amended VPDP contains a small number of minor 'housekeeping' amendments. Primarily, this is updated car parking rates to reflect recent changes by the State to Clause 52.06. These changes are appropriate and ensure that the VPDP remains consistent with the provisions of the Scheme.

REFERRAL SUMMARY

Nil

PLANNING SCHEME SUMMARY

Darebin Planning Scheme clauses under which a permit is required

- Pursuant to Clause 43.04, a development plan must be prepared to the satisfaction of the Responsible Authority.

Schedule 1 applies to the land and sets out requirements for the development plan (sub-clause 1.0) and guidelines for consideration (sub-clause 2.0).

Applicable provisions of the Darebin Planning Scheme

Section of Scheme	Relevant Clauses
PPF	11.01, 11.02, 11.03, 15.01, 15.02, 15.03, 16.01, 17.01, 17.02, 17.04, 18.01, 18.02, 19.01, 19.02, 19.03, 21.01, 21.02, 21.03, 21.04, 21.05
Zone	32.04
Overlay	43.01, 43.04, 45.06
Particular provisions	52.06
General provisions	65

POLICY IMPLICATIONS**Environmental Sustainability**

All new dwellings are required to achieve a minimum six (6) star energy rating under the relevant building controls.

Social Inclusion and Diversity

Nil

Other

Nil

FINANCIAL AND RESOURCE IMPLICATIONS

There are no financial or resource implications as a result of the determination of this application.

FUTURE ACTIONS

Nil

RELATED DOCUMENTS

- *Planning and Environment Act 1987* (as amended)
- Darebin Planning Scheme
- Polaris 3083 Development Plan (as amended)

Attachments

- Village Precinct Development Plan (March 2019) (**Appendix A**)
- Village Precinct Plan (March 2019) (**Appendix B**)
- Lot 9 Plan (March 2019) (**Appendix C**)

DISCLOSURE OF INTEREST

Section 80C of the *Local Government Act 1989* requires members of Council staff and persons engaged under contract to provide advice to Council to disclose any direct or indirect interest in a matter to which the advice relates.

The Officer reviewing this report, having made enquiries with relevant members of staff, reports that no disclosable interests have been raised in relation to this report.

SJB Planning



Village Precinct Development Plan

1056-1140 Plenty Road, Bundoora

October 2013
(Amended March 2019)

RECEIVED ON
18 MAR 2019
CITY OF DAREBIN
STATUTORY PLANNING

RECEIVED ON
18 MAR 2019
CITY OF DAREBIN
STATUTORY PLANNING

Statement of Environmental Effects

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Village Precinct Development Plan

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Village Precinct Development Plan

1.0 Introduction

This development plan amendment affects part of the land located at 1056-1140 Plenty Road in Bundoora. This land comprises part of the former Larundel Psychiatric Hospital site and part of the 25.99ha Lancaster Estate site.

After the Hospital closed in 1999, Places Victoria (then known as the Urban and Regional Land Corporation (URLC)), purchased the Lancaster Estate for redevelopment.

In 2001, Council approved the Lancaster Gate Development Plan. This plan was prepared by the URLC and divided the Lancaster Estate into three parts; a Residential Precinct (approx. 15.38h hectares), the Village Precinct which included the former hospital buildings and the Mixed Use Precinct (approx. 10.61 hectares).

The Residential Precinct was subsequently subdivided, developed into individual dwelling lots and sold off. In 2007, an addendum was added to the Lancaster Gate Development Plan to relocate the proposed multi-purpose community centre from the Residential Precinct to the Mixed Use Precinct.

In 2006, Places Victoria (then VicUrban) entered into an Expression of Interest campaign to sell the 10.61 hectare land parcel comprising the Village Precinct and Mixed Use Precincts. As part of the transaction, a master plan concept was prepared for the residual eastern portion of the Lancaster Estate.

This master plan took the form of the Polaris 3083 Development Plan (PDP), which was approved by Council in September 2009. Although branded differently, the PDP forms an amendment to the original Lancaster Gate Development Plan and relates to the redevelopment of the Village Precinct and Mixed Use Precincts. The PDP provides for a Town Centre in the Mixed Use Precinct in the north-west of the site as well as for retail, apartments and townhouse development.

Development in accordance with the PDP commenced in 2010 with a planning permit for the construction of the Town Centre including a major line supermarket, retail premises and apartments, as well as the construction of major roads and infrastructure across the entire PDP area. Since 2010, planning permits have also been issued for subdivision and the construction of townhouse and apartment developments immediately south and east of the Town Centre. The balance of the PDP area, mainly the original Village Precinct containing the heritage buildings, however remains vacant.

1.1 Purpose of the Development Plan

This document is known as the Village Precinct Development Plan (VPDP). It is a further amendment to the endorsed Lancaster Gate Development Plan and builds on the approved PDP.

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Except where specified under the plan, the VPDP must be read in conjunction with the PDP document 1, 2 and 3 of 3 approved on September 2009 and pages 1 to 14 approved on 17 January 2013, and as subsequently amended.

This document seeks to guide the redevelopment of Village Precinct and considers site context, built form and land use, heritage, traffic and car parking, arboriculture, open space, landscaping, and servicing and infrastructure matters. It has specific regard to the extant condition of the heritage buildings, the feasibility of restoration and adaptive reuse and Council's current strategic planning for the Plenty Road Corridor.

This document will be the primary reference document for the consideration of future planning permit applications within the Village Precinct. Any permits issued for development within the precinct must be generally in accordance with the approved development plan.



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Village Precinct Development Plan

2.0 Site Details and Context

2.1 The Village Precinct

The Village Precinct includes Lots 7, 8, 9, 10 and 2D. These lots are formally described as follows:

- Lot 7A (16 Zenith Rise) on PS734593,
- Lots 101-110, Lots G01-G10, Lot TH1-TH19 (16 Zenith Rise) on PS734593
- Lot 8 (5 Nova Circuit) on Plan PS638777A
- Lot 9 (40 Nova Circuit) on Plan PS638777A
- Lot 10 (46 Nova Circuit) on Plan PS638777A
- Lot 2D on Plan PS640935V

Copies of the above certificates of title and plans of subdivision are attached as **Appendix A**.



Figure 1: Village Precinct Site Plan

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2.2 Location

The Village Precinct is located in the suburb of Bundoora and City of Darebin approx. 15 kilometres north-east of Melbourne CBD.

The Village Precinct adjoins the Polaris Town Centre to the west and an existing residential neighbourhood comprising single and multiple dwelling residential development to the east (being the original Residential Precinct of the Lancaster Gate Estate). It abuts Plenty Road to the north-west and former oval (now known as Lancaster Green) to the south-east. Bundoora Park is located on the western side of Plenty Road which is a major regional open space and recreation resource. La Trobe University's Centre for High Learning is located to the south of the site and La Trobe University Bundoora campus is located to the south-east of Main Drive. The Village Precinct also adjoins the Community Centre located at the south-east of the Town Centre and abutting Lancaster Green.

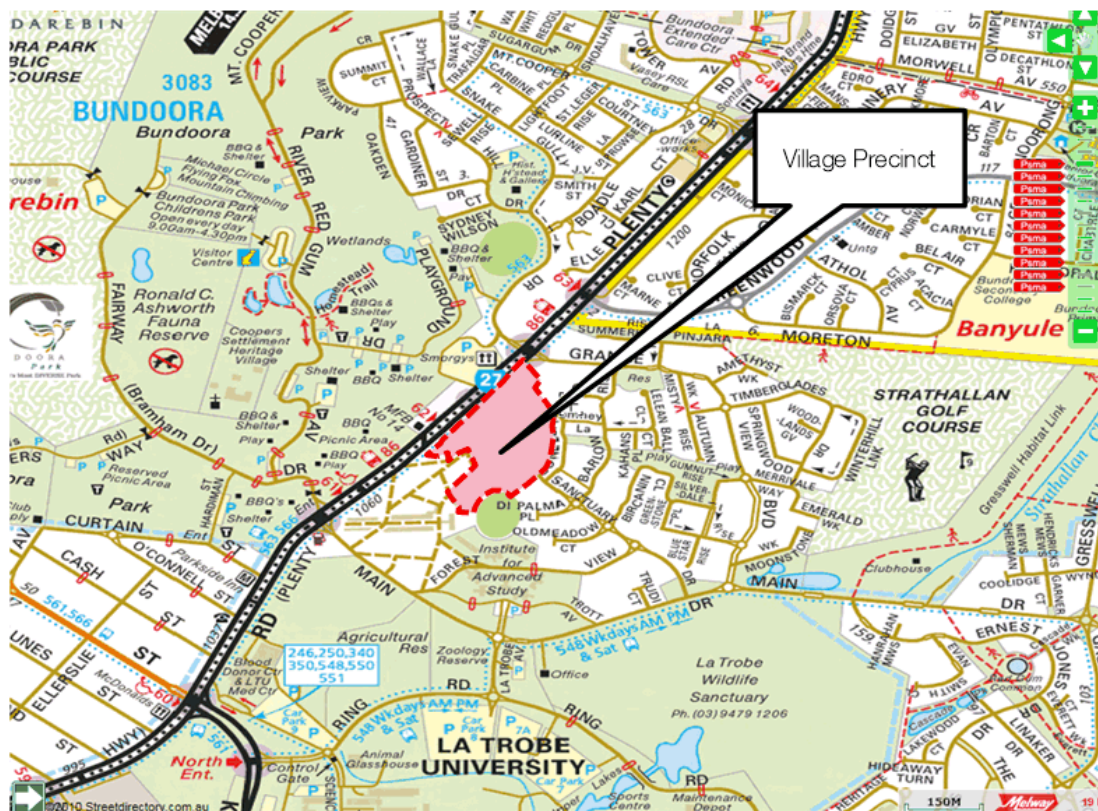


Figure 2: Location Plan

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2.3 Existing Built Form

The master plan for the Larundel Psychiatric Hospital was prepared by Percy Everett, then the Chief Architect of the Victorian Public Works Department. There are nine (9) original heritage buildings which comprise the former Larundel Psychiatric Hospital. These include Buildings 1 to 9, being the main hospital building and wards and support buildings.

The former hospital comprises a complex of single and double-storey buildings, mostly of red brick construction. They are designed in a Tudor Revival style imparting a domestic quality to the otherwise formal institutional nature of the complex.



Figure 3: Former Larundel Hospital Buildings

The buildings are principally accessed from the curved drive, off Plenty Road and symmetrically arranged around the axis oriented north-west / south-east. The areas of the site to the north and south of the principal buildings are arranged less formally. An oval (known as Lancaster Green), to the south of the Village Precinct, was added in 1953 and is located behind the male receiving wards.

Village Precinct Development Plan

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The layout of the existing site reflects Everett’s original 1938-42 master plan, with the addition of new buildings in the 1950s outside of the Village Precinct. The existing road layout is of contributory heritage significance, and the relationship between the remaining buildings and roadways demonstrates the original site planning. This ensures the site continues to be interpreted as buildings within a master planned landscape, albeit now on a reduced scale. The planning of the site retains sufficient distance between the buildings such that they were largely viewed ‘in the round’, and as discreetly linked elements in a park-like setting.

The arrangement of buildings in the Everett master plan reflects the functional and aesthetic principles of the site and this should inform the layout of the future development.

All of the former hospital buildings form part of the Village Precinct except for Building 9 (Former Larundel Building B) which has been restored and converted into the Community Centre.

The condition of the former hospital buildings has significantly deteriorated since the hospital closed. The buildings have suffered from opportunistic vandalism (including graffiti), weather exposure and fire damage. Building 1 was substantially destroyed by fire in 2005, leaving only brick walls and the chimneys intact.

A complete assessment of the buildings’ condition was undertaken by Lovell Chen Architects & Heritage Consultants in 2012 as part of a review of the 2000 Conservation Management Plan (CMP) prepared for the former hospital buildings. A summary of assessment and heritage values is provided below for each building in the Village Precinct. A copy of the 2012 CMP Review prepared by Lovell Chen Heritage Architects and Heritage Consultants is attached as **Appendix B**.

Building	Summary of Heritage Value
Building 1 Administrative Building	Considerable destruction to fabric due to fire and vandalism, reduced level of heritage significance: contributory significance
Building 2 Hospital Building	Significant vandalism and environmental degradation, retains primary significance due to complex plan and visual prominence. Interiors have undergone alteration and modification and are of little or no significance. The elevated walkway to the east and open garden areas to either side should be retained.
Building 3 Dining and Messroom Building	The portions of the service building have been demolished and east elevation modified. The building is affected by vandalism (spray painting and graffiti), windows and doors are screened with security sheeting and, the interior is affected by water penetration. The building has contributory significance. The interior is of little or no significance.

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Building	Summary of Heritage Value
Building 4 Male/Female Receiving Wards	Affected by opportunistic vandalism, part-secured by wire fencing and windows, doors are screened with security sheeting. Interior affected by fire, fixtures and fittings vandalised. Plan form and parts of interior are largely intact. Primary significance, interior little or no significance.
Building 5 Male/Female Receiving Wards	Affected by opportunistic vandalism, part-secured by wire fencing and windows, doors are screened with security sheeting. Roof tiling and building interior damaged. Floor plan is similarly intact. Primary significance, interior little or no significance.
Building 6 Sub-Receiving Wards	Affected by vandalism and the impacts of environmental damage from the loss of roof fabric and joinery. Graffiti and spray-painting to building exterior. Original plan form intact along with partly intact fixtures and elements of original fabric albeit deteriorated. Primary significance.
Building 7 Sub-Receiving Wards	Affected by vandalism and the impacts of environmental damage from the loss of roof fabric and joinery. Graffiti and spray-painting to building exterior. Original plan form intact, but subject to fire damage which has charred internal spaces and stripped paint and plaster from the walls. Primary significance.
Building 8 Former Larundel Building A	Affected by vandalism and impact of environmental degradation. Primary significance and recommended for retention.

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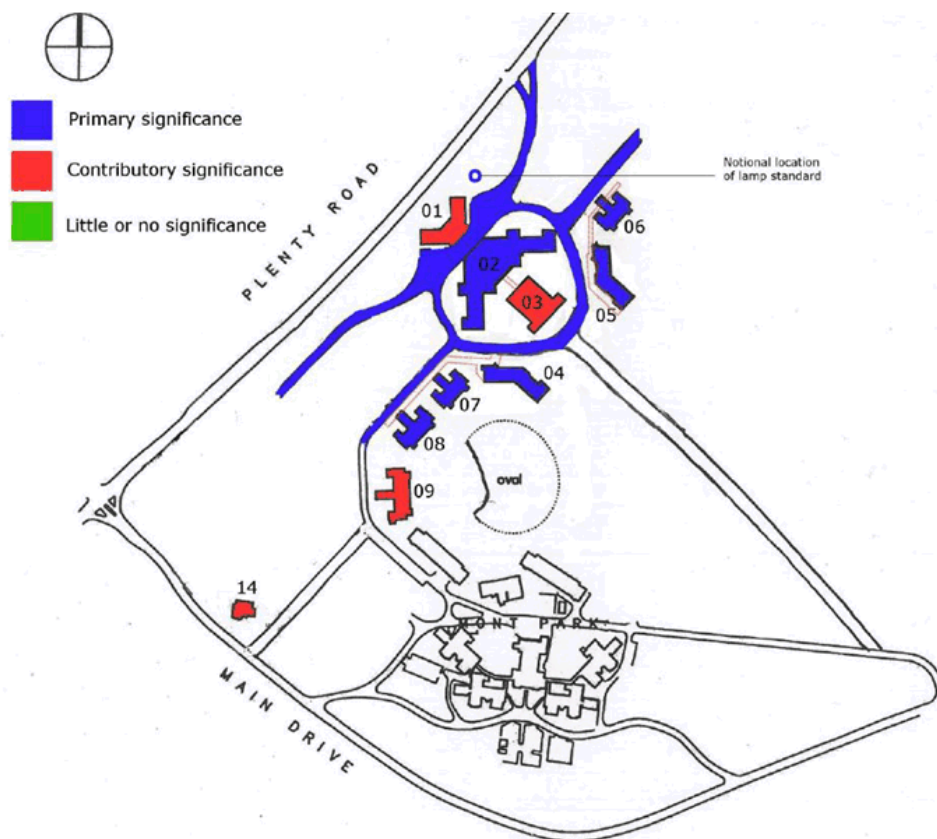


Figure 4: Plan Summary of Heritage Significance

2.4 Topography

The land in the Village Precinct falls gently from the north to the south and west. There is a level change of 3m from north-east to the north-west along Plenty Road, and approx. 6.33m to the south-east at Sanctuary Drive. Given the site’s topography, the finished floor levels of the existing buildings sit above the natural ground level. Survey plans prepared by Bosco Jonson are attached as **Appendix C**.

2.5 Road Network, Circulation and Access

The internal road layout in the Village Precinct is fully constructed and provides vehicle and pedestrian access to the surrounding road network and Polaris Town Centre. The internal roads follow the original road design and are aligned in an axial arrangement to complement the symmetrical layout of the former hospital buildings.

New development in the Village Precinct will access key destination points such as Polaris Town Centre and main thoroughfares such as Plenty Road and Main Drive via Nova Circuit, Galileo Gateway, Zenith Rise and Copernicus Crescent. New development is unlikely to exit the Village Precinct into the surrounding residential area.

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Village Precinct Development Plan

The existing road connections between the Village Precinct and Sanctuary Drive and Gonella Crescent will provide access for the surrounding residential area into the Polaris Town Centre, and to Main Drive and Plenty Road.

There is limited capacity to provide on-street car parking within the internal roads.

2.6 Interfaces

The Village Precinct has external interfaces to Plenty Road, the Community Centre (Building 9), Lancaster Green, the Polaris Town Centre and approved future residential apartment building on Lot 6. It also maintains sensitive interfaces to existing residential development along Sanctuary Drive, Gonella Crescent and Grange Boulevard. Internally, future development within the Village Precinct will also maintain sensitive interfaces with the former hospital buildings.

2.7 Key views and corridors

Key view lines are challenged by the precinct’s size and variation in natural ground levels. Currently, there are minimal views of the former hospital buildings from Plenty Road due to existing vegetation. Although the symmetrical organisation of the buildings can be best appreciated from an aerial or plan perspective, there are important landmark views within the site including the visual relationship between key heritage buildings on the north-south axis (Buildings, 6, 2, 7 and 8). Reverse views also exist from Lancaster Green into the Village Precinct.

2.8 Landscape

The Village Precinct has a highly modified landscape given past development and the introduction and planting of non-indigenous and exotic vegetation. Notwithstanding, the existing landscape character of the Village Precinct has a semi-rural park-like quality which contributes to the original pastoral setting.

The landscape setting was originally designed by Hugh Linaker who was appointed Superintendent of Parks and Gardens at Mont Park between 1912 and 1937. While no longer viewed within a generous pastoral setting and with an altered roadway arrangement, the site landscape contributes to the understanding of the aesthetic value of the former Larundel Hospital site.

The landscape comprises a mix of native and exotic tree and shrub species. An arboriculture assessment carried out by Stephen Fitzgerald Arboriculture in 2007 and attached to the PDP as Appendix E provides an assessment of tree retention values.

A tree retention plan included as Figure 82 of the PDP identifies a variety of trees with high, medium and low retention values. It is noted that arboricultural conditions may have altered since 2007 and that several trees including Trees 165, 168, 361 and 368 have been subsequently removed in accordance with Planning Permit D909/2010.

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3.0 Planning Scheme Policy and Statutory Requirements

3.1.1 Land Use Zone

The Village Precinct is located in the Mixed Use Zone (MUZ) which seeks to:

- To implement the State Planning Policy Framework and the Local Planning Policy Framework, including the Municipal Strategic Statement and local planning policies.
- To provide for a range of residential, commercial, industrial and other uses which complement the mixed-use function of the locality.
- To provide for housing at higher densities.
- To encourage development that responds to the existing or preferred neighbourhood character of the area.
- To facilitate the use, development and redevelopment of land in accordance with the objectives specified in a schedule to this zone.
- A planning permit is not required to use the land for the purpose of a dwelling.

A planning permit is not required to use the land for the purpose of a dwelling.

A permit is required for buildings and works associated with land uses such as accommodation and other section 2 uses (other than a single dwelling on a lot greater than 300sqm and one dependent person's unit on a lot). A development must meet the requirements of Clause 55. This does not apply to a development of five or more storeys, excluding a basement.

A permit is required to subdivide land in the MUZ.

Advertising signage requirements are provided at Clause 52.05 and the MUZ is considered to be Category 3.

3.1.2 Overlays

Development Plan Overlay

Pursuant to Clause 43.04 of the Darebin Planning Scheme, the Village Precinct is affected by the Development Plan Overlay Schedule 1 relating to the Former Larundel Psychiatric Hospital.

The purpose of the DPO is:

- To implement the State Planning Policy Framework and the Local Planning Policy Framework, including the Municipal Strategic Statement and local planning policies.
- To identify areas which require the form and conditions of future use and development to be shown on a development plan before a permit can be granted to use or develop the land.



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Village Precinct Development Plan

- To exempt an application from notice and review if it is generally in accordance with a development plan.

A permit must not be granted to use or subdivide land, construct a building or construct or carry out works until a development plan has been prepared to the satisfaction of the responsible authority.

A permit granted must:

- Be generally in accordance with the development plan.
- Include any conditions or requirements specified in a schedule to this overlay.

The development plan must describe:

- The land to which the plan applies.
- The proposed use and development of each part of the land.
- Any other requirements specified for the plan in a schedule to this overlay.

The development plan may be amended to the satisfaction of the responsible authority.

Pursuant to DPO1, the Development Plan should address the elements of the North East Corridor Strategic Plan relevant to the site. It should also provide various plans including but not limited to the layout of the existing and proposed buildings and works, the location of passive and open space, treatment of internal roads and road connections and proposed uses.

Pursuant to DPO1 the following guidelines are to be considered when assessing a proposed development plan or planning permit:

- That the proposal discourages industrial uses.
- Whether the development plan or permit application is consistent with the North East Corridor Strategic Plan.
- Consideration of any applicable heritage study and any applicable conservation policy
- Whether the location, bulk, height and appearance of any proposed buildings or works is in keeping with the character of the area.
- Whether any proposed landscaping or removal of vegetation will be in keeping with the character and appearance of adjacent buildings, the streetscape or the area.
- Consideration of the layout and appearance of areas set aside for car parking, access and egress, loading and unloading and the location of any proposed car parking.
- Whether subdivision will result in development which is not in keeping with the character and appearance of adjacent buildings, the streetscape or the area will adversely affect the significance, character or appearance of the heritage place.

An application under any provision of this scheme which is generally in accordance with the development plan is exempt from the notice requirements of Section 52(1)(a), (b) and (d), the decision requirements of Section 64(1), (2) and (3) and the review rights of Section 82(1) of the Act.

Village Precinct Development Plan

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Heritage Overlay Schedule 111

Heritage Overlay Schedule 111 relating to Central Administration, Hospital and Dining/Kitchen Blocks and the Mental Patients Block (including the Male & Female Receiving Wards, and Sub-Receiving Wards affects the PDP. An incorporated plan 'Heritage Overlay Plan, Larundel' applies under the overlay as do external paint controls and prohibited uses.

3.1.3 Existing Development Plans

Lancaster Gate Development Plan

In 2001, Council approved the Lancaster Gate Development Plan (LGDP). This plan was prepared by the URLC and divided the Lancaster Estate into three parts; a Residential Precinct (approx. 15.38h hectares), the Village Precinct which included the former hospital buildings and the Mixed Use Precinct (approx. 10.61 hectares combined). The plan was later amended in 2007 to relocate the multi-purpose community facility to Building 9 in the current Polaris Town Centre.

The Village Precinct is identified for possible student housing, medium density town house development, apartment housing, education, community, open space, recreation and office development and ancillary development.

A further amendment to the development plan is required under the LGDP in respect of the future use and development of the Village and Mixed Use Precincts.

Polaris Development Plan

In 2009, Council approved the Polaris 3083 Development Plan (PDP) as an amendment to the original LGDP and relating specifically to the redevelopment of the original Village Precinct and Mixed Use Precincts.

The plan provides for a Town Centre generally located at the south-west of the site and in the original Mixed Use Precinct. It also provides for retail, apartments and townhouse development.

Whilst the plan did contemplate future development of the former hospital buildings, insufficient analysis was undertaken to test the viability of the proposed restoration and redevelopment works under the PDP. The PDP was prepared on the basis of a 2000 Conservation Management Plan, which included an assessment of the condition of the heritage buildings. Since 2000, the condition of the former hospital buildings has significantly deteriorated, suffering from opportunistic vandalism including graffiti, weather exposure and fire damage.

Furthermore, the design of the some of the former hospital buildings doesn't readily facilitate commercial and/or residential redevelopment due to the single storey built form and character, and constrained aesthetic prominence given the buildings' position relative to site contours. Consequently, amendments to the PDP are required to deliver a practical restoration and redevelopment solution for the Village Precinct.

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Village Precinct Development Plan

3.1.4 Other Planning Considerations

North East Corridor Strategic Plan

The North East Corridor Strategic Plan (NECSP) was adopted by Council in 1998. The plan covered the Lancaster Gate Estate and generally envisaged medium density housing in the eastern half of the estate, medium density or mixed use in the western half of the estate and a maximum residential building height of 2 storeys. Mixed use development of a maximum of 3 storeys was anticipated along Plenty Road and around former hospital Building 2, which was to become a local centre. All heritage buildings were to be retained and a bus route was planned along Sanctuary Drive and Galileo Gateway. No trees in the Village Precinct were identified for retention.

The NECSP contemplates the possibility of future demolition of some former hospital buildings if retention and reuse is proved financially infeasible. The recent deterioration of the buildings and costs involved in restoration and adaptive reuse significantly affects the feasibility of retaining the buildings as originally contemplated.

Moreover, several changes to State planning policy have occurred since the NECSP was approved, in addition to the reclassification of the site as the Lancaster Gate Neighbourhood Activity Centre. The Polaris Town Centre, approved under the PDP, has been relocated to the west along Main Drive and maximum building height has been increased generally to 3 storeys across the site.

The NECSP is not an incorporated document pursuant to Clause 81 of the Darebin Planning Scheme. It is intended to act as a reference document to DPO1, however given its age and recent permit and development plan changes affecting the Village Precinct, it provides limited guidance in terms of strategic planning outcomes and background information.

Plenty Road Corridor Framework and Scheme Amendment C137

Council has undertaken significant strategic planning for the Plenty Road Corridor, incorporating new built form controls under the Plenty Road Corridor Urban Design Framework. The Framework formed the basis of Scheme Amendment C137 which introduced a new Design and Development Schedule (DDO17) into the Darebin Planning Scheme. Whilst the PDP is identified as a strategic site, with active frontages to Plenty Road and Main Drive and future pedestrian links to the Future La Trobe University Town Centre site to the south, it is not located within the DDO17 boundary.

DDO17 allows for a maximum 6 storey building height proximate to the site at 1093-1095 Plenty Road and on the land west of Plenty Road between Bundoora Park and Bradshaw Street.



4.0 Development Plan Requirements

4.1.1 Proposed Master Plan

The proposed master plan for the Village Precinct will guide the future development of the site, balancing the retention and restoration of former hospital buildings with new townhouse, apartment and mixed use development.

The master plan, attached on the following page together with detailed individual lot plans, has been designed to respond appropriately to the future character of the Plenty Road Corridor, the site's sensitive interfaces, existing heritage values and the feasibility of restoration and adaptive re-use works.

The master plan proposes the following elements:

- Retention and restoration of Buildings 2 (external western facade and returns along Galileo Gateway), 6, 7, 8;
- Inclusion of Building 8 and 9 into Heritage Overlay 111;
- Demolition of Buildings 1, 2 (limited to the eastern floorplate and façade behind the retained external façade), 3, 4 and 5 and temporary planting of building footprint with grass prior to further development;
- Creation of 2000sqm of Public Open Space generally in the footprint of Building 1 (to be removed) and along a minimum frontage of 40m to Plenty Road;
- Construction of new apartment buildings and/or townhouses on Lots 10, 9 and 7;
- Construction of townhouses generally in the footprint of Building 3;
- Construction of mixed use buildings abutting Plenty Road on Lot 2D.

A mix of housing typologies are proposed including 2 and 3 storey townhouses and partial 3, 4 and 5 storey apartment buildings. A substantial number of dwellings are proposed to be located the restored former hospital buildings.

The scale and density of development expands upon the approved PDP in order to provide a viable and practical restoration and redevelopment strategy for the Village Precinct.

4.1.2 Proposed uses

The Village Precinct will accommodate a variety of uses including dwellings, aged care accommodation and commercial uses such as office and retail uses. The former hospital buildings are most suited to residential uses whilst future buildings along Plenty Road may comprise future mixed use or commercial buildings. Future uses will be consistent with the purposes and expectations of the Mixed Use Zone.



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Village Precinct Development Plan

4.1.3 Heritage Values

The VPDP focuses on achieving positive heritage outcomes for the former Larundel Hospital Buildings that are consistent with the development strategies established in the CMP as reviewed in 2012.

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DRAWING LEGEND

- APARTMENT
 - 3 storey
 - 4 storey
 - 5 storey
- TOWNHOUSES
- COMMERCIAL
- HERITAGE
- BY OTHERS
- APPROVED/ BUILT

ALL DEVELOPMENT TO BE GENERALLY IN ACCORDANCE WITH VILLAGE PRECINCT DEVELOPMENT PLAN

- NOTE: INDICATIVE BUILDING ENVELOPES REPRESENT A MAXIMUM BUILDING HEIGHT AND ARE PROVIDED TO ALLOW AN APPROPRIATION OF THE MAXIMUM SCALE OF DEVELOPMENT OVER THE SITE ONLY. THEY DO NOT IMPLY A RIGHT TO BUILD WHOLLY WITHIN THE BUILDING ENVELOPE AND ALLOW SOME FLEXIBILITY REGARDING THE SLOPING NATURE OF THE LAND AND THE POSSIBLE NEED TO VARY PLANS IN THE FUTURE WITHOUT AMENDING THE DEVELOPMENT PLAN. ALL FUTURE DEVELOPMENT WILL BE SUBJECT TO PLANNING AND PERMIT APPLICATIONS. THE FORM, SETBACK AND HEIGHT OF FUTURE BUILDINGS MUST BE DESIGNED HAVING REGARD TO THE SITE CONTEXT HAVING FULL REGARD TO:
- TOPOGRAPHY OF THE LAND.
 - THE SCALE, LOCATION AND VIEWS OF ADJACENT HERITAGE BUILDINGS.
 - THE AMENITY OF THE PUBLIC OPEN SPACE INCLUDING MASS AND BULK IMPACTS.
 - MASS AND BULK IMPACTS UPON ADJACENT DWELLINGS INCLUDING PRIVATE OPEN SPACE.
 - OVERSHADOWING IMPACTS UPON ADJACENT DWELLINGS INCLUDING PRIVATE OPEN SPACE.
 - THE NEED TO PROTECT AND RETAIN VEGETATION ON THE SITE.
 - PLENTY ROAD

- NOTE 1: ALL FUTURE DEVELOPMENT IS SUBJECT TO A PLANNING PERMIT APPLICATION.
2. BUILDING ENVELOPES ARE INDICATIVE ONLY. NEW BUILDINGS ADJACENT TO RETAINED HERITAGE BUILDINGS TO BE OF AN EQUIVALENT SCALE OR LESS.
 3. BUILDING HEIGHT PROVIDES AN INDICATION OF MAXIMUM BUILDING HEIGHT.
 4. STOREYS ON THE DEVELOPMENT PLAN INCLUDE BASEMENTS, AS DEFINED UNDER CLAUSE 72 OF THE DAREBIN PLANNING SCHEME.

LOT 10: APARTMENT BUILDING FRONTING GALILEO CIRCUIT REDUCED IN HEIGHT TO 3, 4 AND 5 STOREYS FROM 4 AND 5 STOREYS HAVING REGARD TO THE TOPOGRAPHY OF THE LAND AS SHOWN ON FIGURE 6 INDICATIVE WEST ELEVATIONS.

LOT 9: APARTMENT BUILDING FRONTING NOVA CIRCUIT REDUCED IN HEIGHT TO 2 STOREYS FROM 3 STOREY & 4 STOREYS AND CONVERTED TO TOWNHOUSES.

LOT 8: UPPER LEVELS MAY OVERHAND THE NOVA CIRCUIT GROUND LEVEL. SETBACK AND MUST MAINTAIN A MINIMUM 1m SETBACK FROM THE BACK OF FOOTPATH.

AS PART OF ANY PLANNING PERMIT ISSUED FOR BUILDING AND WORKS INCLUDING DEMOLITION ON LOT 8 & 9 SITE AGREEMENT MUST BE REGISTERED ON TITLE PRIOR TO THE COMMENCEMENT OF DEMOLITION, UNLESS OTHERWISE AGREED IN WRITING WITH THE RESPONSIBLE AUTHORITY

LOT 7: ANY DESIGN WILL NEED TO PROVIDE AN APPROPRIATELY LANDSCAPED SOFT EDGE TRANSITION FROM THE HARD EDGE OF THE 2 AND 3 STOREY BUILT FORM IN THE PUBLIC OPEN SPACE SETBACK AND THE FRONT SETBACK TO THE ROAD. RESERVE WITH SUFFICIENT LAND TO PROVIDE FOR APPROPRIATE CANOPY TREES ON THE PRIVATE LAND. CANOPY TREES WILL COMPRISE A MEDIUM CANOPY TREE IN THE FRONT SETBACK AND PUBLIC OPEN SPACE SETBACK OF TWO OF EVERY THREE OF THE TRIPLE STOREY DWELLINGS AND A SMALL CANOPY TREE IN THE FRONT SETBACK AND PUBLIC OPEN SPACE SETBACK OF TWO OF EVERY THREE OF THE DOUBLE STOREY DWELLINGS. A QUALITATIVE ASSESSMENT WILL NEED TO BE PROVIDED BY AN ARBORIST AND /OR LANDSCAPE ARCHITECT DEMONSTRATING THAT THE DESIGN ALLOWS AN APPROPRIATE SOFT EDGE TRANSITION. SETBACKS MAY NEED TO BE INCREASED TO PROVIDE FOR THE TREES. CANOPY TREES MUST HAVE THE FOLLOWING MINIMUM WIDTHS AT MATURITY: SMALL CANOPY (4 METRES), MEDIUM CANOPY (6 METRES).



1. INDICATIVE YIELD ONLY
2. INDICATIVE LAND USE TYPOLOGIES SHOWN
3. INDICATIVE SETBACKS SHOWN
4. FINAL YIELD, LAND USE TYPOLOGIES AND SETBACKS TO BE DETERMINED AT THE PLANNING PERMIT APPLICATION STAGE AND FOR EACH LOT OR PART THEREOF.

Indicative Village Precinct Development Plan



8th March 2019
1225/MP02T

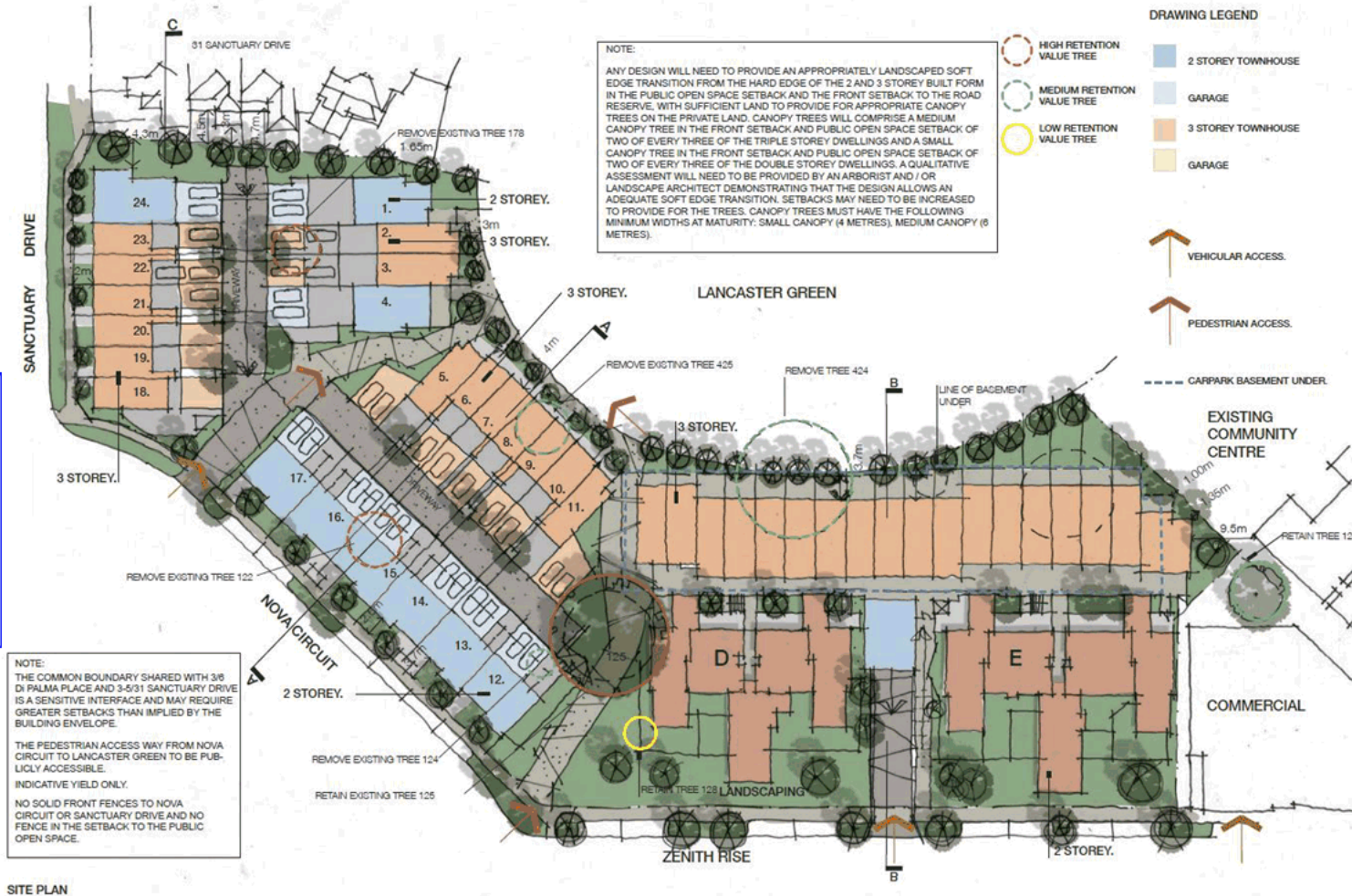
Village Precinct Development Plan

Village Precinct Development Plan

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SITE PLAN

Clarke Hopkins exist
 118 Sandwell Street
 Dandenong VIC Australia 3175
 Telephone (03) 9413 4300
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 Email info@clarkehopkins.com.au
www.clarkehopkins.com.au

Block 20 - Lot 7 CONCEPT 05
 Polaris 3083 Bundoora

Scale 1:200 @ A1
 1:500 @ A3

17th November 2015
 1304/CP01h

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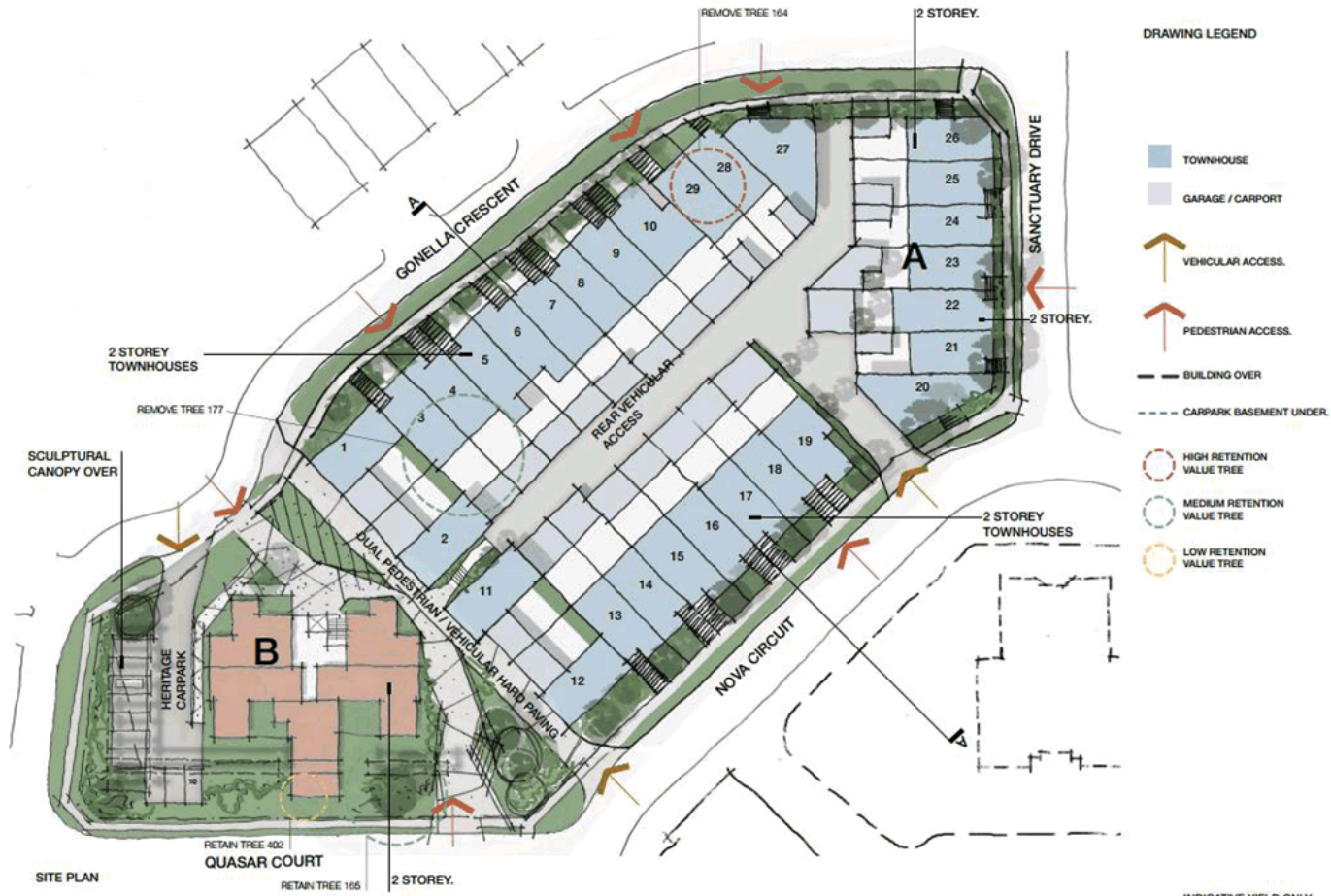
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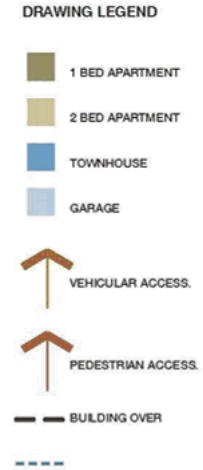
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	ClarkeHopkins Clark 175 Spadewell Street Melbourne VIC 3000 Telephone (03) 9419 4300 Fax (03) 9419 4304 Email clark@clarkhopkins.com.au www.clarkhopkins.com.au	Block 2C - Lot 9 CONCEPT 02 Polaris 3083 Bundoora	 Scale: 1:200 @ A1 1:500 @ A3 0 25	13th Feb 2019 1279/CP01e
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THE COMMON BOUNDARIES SHARED WITH 11-21/121 GRANGE BOULEVARD AND 12 GONELLA CRESCENT ARE A SENSITIVE INTERFACE AND MAY REQUIRE GREATER SETBACKS THAN IMPLIED BY THE BUILDING ENVELOPES.

REFER FIGURE 9 - APARTMENT BUILDING FRONTING GALILEO CIRCUIT REDUCED IN HEIGHT TO 3,4, AND 5 STOREYS (FROM 4 AND 5 STOREYS) HAVING REGARD TO THE TOPOGRAPHY OF THE LAND AS SHOWN ON FIGURE 12, INDICATIVE WEST ELEVATIONS PROVIDED.



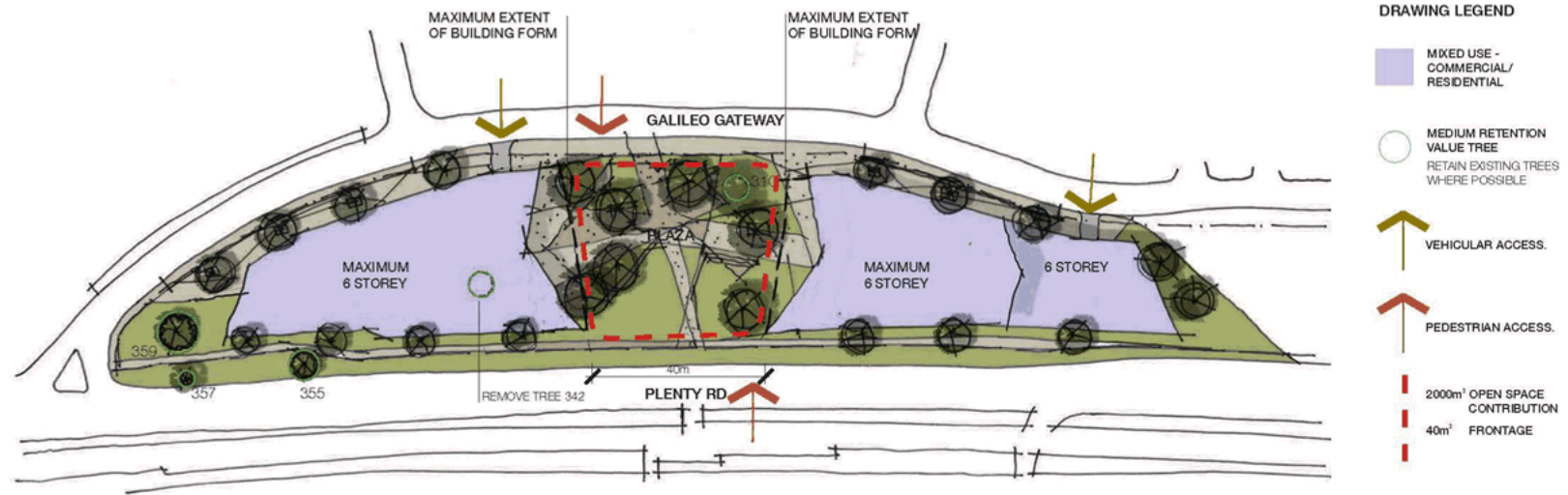
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INDICATIVE YIELD ONLY

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	Clarke Hopkins exist 115 Bessie Street Collingwood VIC 3068 Telephone 03 9419 4390 Facsimile 03 9419 4391 Email enquiries@clarkehopkins.com.au www.clarkehopkins.com.au	Block 2C - Lot 10 CONCEPT 01 Polaris 3083 - Estate Masterplan		Scale 1:250 @ A1 	28th Mar 2014 1280/CP 01
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NOTE: THE DEVELOPMENT TO BE GENERALLY IN ACCORDANCE WITH THE FOLLOWING PRINCIPLES:
 THE BUILDING MAY PROVIDE A PODIUM FORM (MAXIMUM FOUR STOREYS) TO THE PUBLIC REALM, WITH UPPER TOWER LEVELS GENERALLY SETBACK A MINIMUM OF 3m FROM THE PODIUM WALLS BELOW ADJACENT TO THE PUBLIC OPEN SPACE AND DIFFERENTIATED FROM PODIUM BY DESIGN AND / OR SETBACKS AS FRONTING PLENTY ROAD AND OTHER STREETS.

GENERALLY PROVIDE AT GRADE ACTIVE COMMERCIAL FRONTAGES TO PLENTY ROAD AND THE PUBLIC OPEN SPACES WITH CANOPY/ SHELTER OVER FOOTPATH/ TOWARDS COMMERCIAL ACTIVE FRONTAGES.

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Precinct 2d - Site Plan and Plenty Rd Elevation
 Polaris 3083 Bundoora



28th Mar 2014

12122/CP04c

Whilst the master plan includes the loss of some buildings, this loss is balanced by an overall consolidation of the site's heritage values. The master plan provides for adaptive reuse and restoration of primary significant buildings, a landscape that responds to the heritage values of the site and an infrastructure layout that is respectful of the site's heritage.

In particular, the demolition of Buildings 1, 2 (limited to the eastern floorplate and façade behind the retained external (western) façade), 3, 4 and 5 and provision of a larger scale of development within the site will improve the viability of heritage restoration works and ultimately facilitate the redevelopment of the Village Precinct. Partial demolition of Building 2 is to be limited to the rear administrative/specialist medical rooms, central bridge, atrium and internal walls, with the central foyer, male and female ward wings and the projecting gables retained.

Future development will preserve heritage values and achieve the objectives of CMP by:

- Retaining key visual relationships between heritage buildings, particularly the north-south axial relationship between Buildings 6, 2, 7, 8 and 9.
- Introducing new views of the former hospital buildings from Plenty Road through the provision of 2000sqm of open space.
- Undertaking high quality external conservation works to the retained heritage buildings.
- Reinforcing the landscaped character; retaining buildings within a landscaped setting that is interpretive of the historic open setting and improving the interface to Lancaster Green.
- The inclusion of Buildings 8 and 9 within the Heritage Overlay area.
- Ensuring that there is sufficient physical and visual separation between the heritage buildings and new development, particularly where new building form has a greater scale than existing heritage buildings.
- Introducing new built form to the rear of Buildings 7 and 8 that adopts a contemporary architectural language and is visually recessive.
- Reducing the extent of visually intrusive at-grade car parking by locating car parking out of view from the public realm, providing landscaping treatments and basement level car parking.
- Reusing heritage bricks, to be salvaged from demolished buildings, in the conservation and repair of retained heritage fabric where feasible and/or as part of landscaping treatments.
- The inclusion of a comprehensive graphic interpretation scheme as a component of the development would enhance the opportunities for wider understanding of the significant values of the site.

In addition, the following commitments are made to conserving heritage values on the site as part of the VPDP:

- The immediate commencement of further securing works to the buildings to be retained. Early weatherproofing and enclosure works to retained heritage buildings comprising the enclosure of window openings with painted plywood will reduce the potential for further deterioration of the heritage buildings.



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- General works to tidy up the site including the removal of overgrown scrub and the removal of covered walkways and other peripheral elements will be undertaken in order to improve the site’s physical appearance and internal security and surveillance.
- Provision of grassed areas to the footprint of the demolished buildings as an interim measure to manage site presentation.

The first works under the VPDP will commence on Lot 7. These works will consist of both conservation works and the adaptive reuse of Buildings 7 and 8, as well as new development to the rear of the lot. Any development on Lot 7 must include restoration works on the former hospital buildings, even in the event future works are staged. Furthermore, before any occupancy permit issues for any building constructed on Lot 7, an occupancy permit must first be obtained in respect of both Buildings 7 and 8.

In respect of the redevelopment of Lot 8, the following requirements apply:

- Any development of Lot 8 will include the restoration of Building 2;
- If the development of Lot 8 is to occur in stages, the restoration of Building 2 must occur as part of the first stage, or as otherwise agreed in writing with the Responsible Authority;
- Any restoration of Building 2 must be undertaken generally in accordance with the Conservation Management Plan Review prepared by Lovell Chen dated February 2013 and to the satisfaction of the Responsible Authority; and
- The restoration of Building 2 (as retained) must be fully restored to the satisfaction of the Responsible Authority and an occupancy permit must be obtained for Building 2 before any occupancy permit issued for any building constructed on Lot 8, or as otherwise agreed in writing with the Responsible Authority.

As part of the any planning permit issued for building and works including demolition on Lot 8, a Section 173 Agreement is to be registered on title prior to the commencement of demolition, unless otherwise agreed in writing with the Responsible Authority.

4.1.4 Built Form Guidelines

Height and Density

Building height varies across the precinct, having regard to site contours, the recommendations of the Plenty Road Urban Design Framework and sensitive interfaces associated with the amenity of existing dwellings and heritage values of the retained former hospital buildings. Development should be visually recessive relative to the retained heritage fabric. In lesser sensitive areas, buildings may adopt a greater scale compared to the retained former hospital buildings.

Building height varies between 2 and 6 storeys with the greatest heights concentrated along Plenty Road, transitioning down to 2-4 storeys at the residential and heritage interfaces.

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Two 6-storey mixed use buildings are proposed at the Precinct’s Plenty Road frontage, which is consistent with Council’s Plenty Road Land Use and Urban Design Framework and DDO17 as introduced by Amendment C137.

New apartment buildings range in height from 3 – 5 storeys. Taller buildings have been located in the lower parts of the landscape to maintain consistent viewlines, particularly to the heritage buildings, and to provide an appropriate transition to sensitive interfaces.

Like building height, density is concentrated along Plenty Road and at the gateways of the site, and is broken up by locating the 2 and 3 storey townhouses to the south of the former hospital buildings.

Indicative building heights are demonstrated in the sections below.

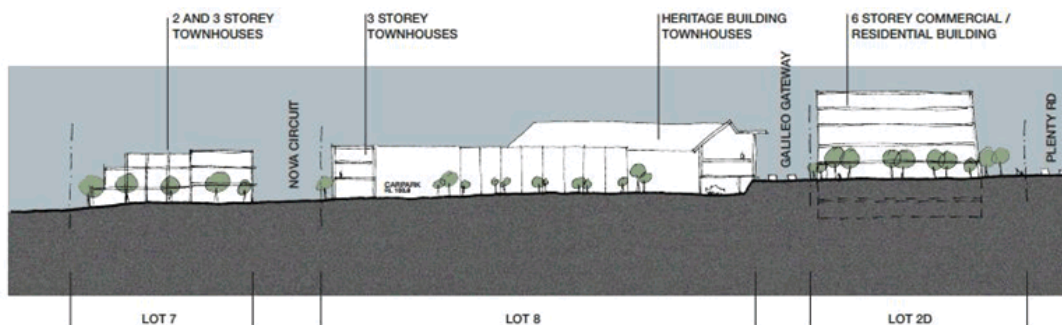


Figure 5: Indicative north-east to South-west Section: Plenty Road to Sanctuary Drive

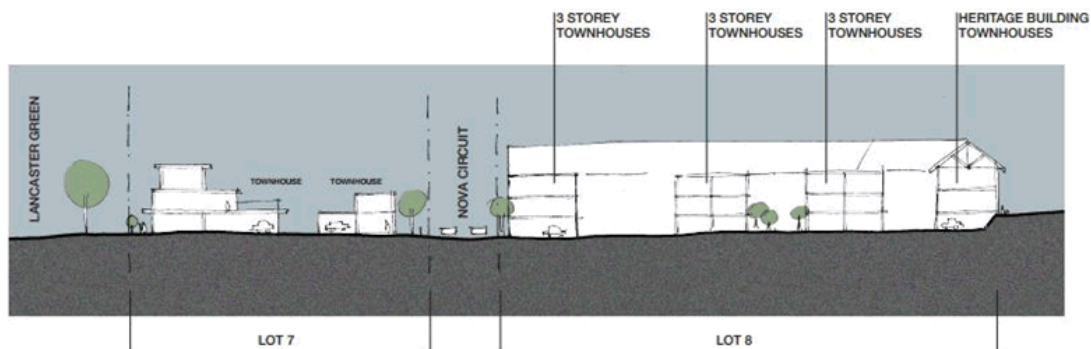


Figure 6: Indicative section through Lot 7 – Nova Circuit to Lancaster Green

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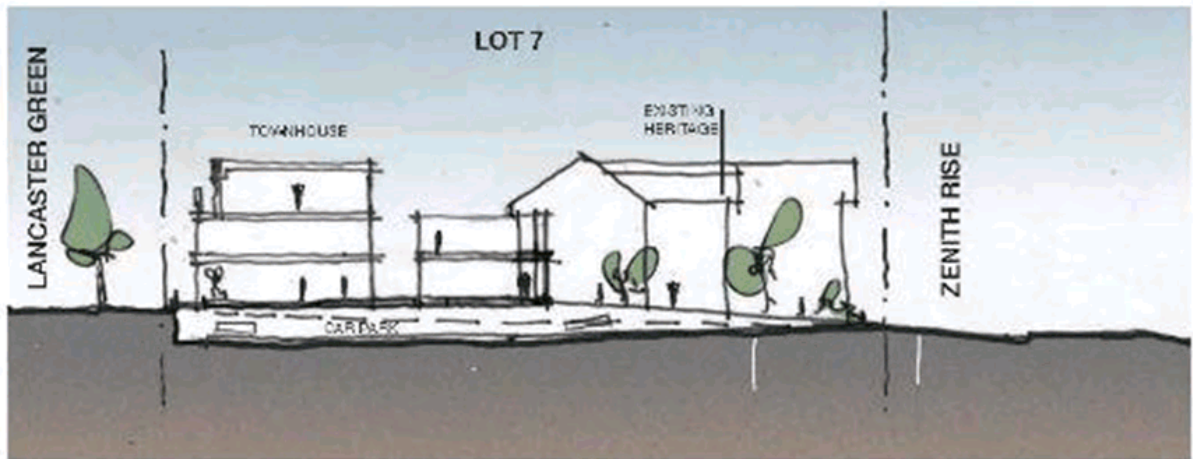


Figure 7: Indicative section through Lot 7 – Zenith Rise to Lancaster Green

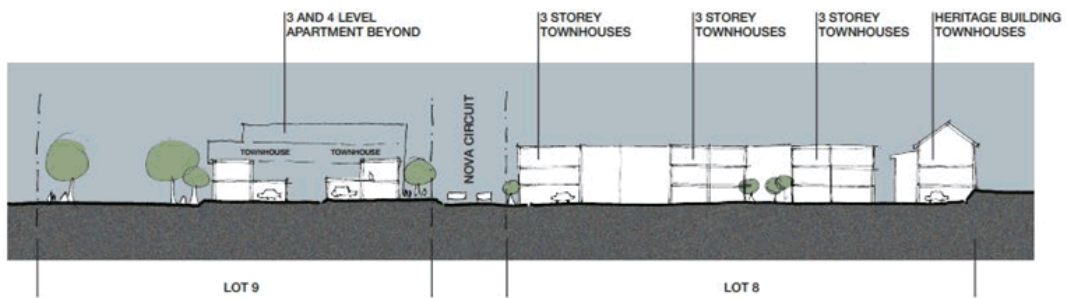


Figure 8: Indicative section through Lot 9 – Gonella Crescent to Lot 8

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All development is to be generally in accordance with the Village Precinct Development Plan (this document). The indicative building envelopes represent a maximum building height and are provided to allow an appreciation of the maximum scale of development over the site only. They do not imply a right to build wholly within the building envelope and therefore, allow some flexibility having regard to the sloping nature of the land and the possible need to vary plans in the future without amending the development plan.

All future development will be subject to planning permit applications. The form, setback and height of future buildings should be designed having regard to the following:

- Topography of the land;
- The scale, location and views of the adjacent heritage buildings;
- The amenity of the public open space including potential mass and bulk impacts;
- Potential mass and bulk impacts upon adjacent dwellings secluded private open space;
- Potential overshadowing impacts upon adjacent dwellings secluded private open space;
- The need to retain vegetation on the site; and
- Plenty Road.

Note: Storeys on the development plan exclude basements as defined under Section 72 of the Darebin Planning Scheme.

Setbacks

New development is to be sited carefully to respect sensitive interfaces to existing heritage buildings and residential dwellings along surrounding streets. The views and presentation of heritage buildings will be enhanced through the inclusion of appropriate setbacks between the existing and new development.

The sensitive interface to existing residential dwellings (shown as red dashed line below) include dwellings directly abutting or adjoining the Village Precinct, such as those located at 121 Grange Boulevard, 11, 12, 13, 14 and 16 Gonella Crescent, 11 Rumney Lane, 1, 3, 5, 7, 9 and 14 Gonella Crescent, 44 Sanctuary Drive, units at 31 Sanctuary Place and 6 Di Palma Place.

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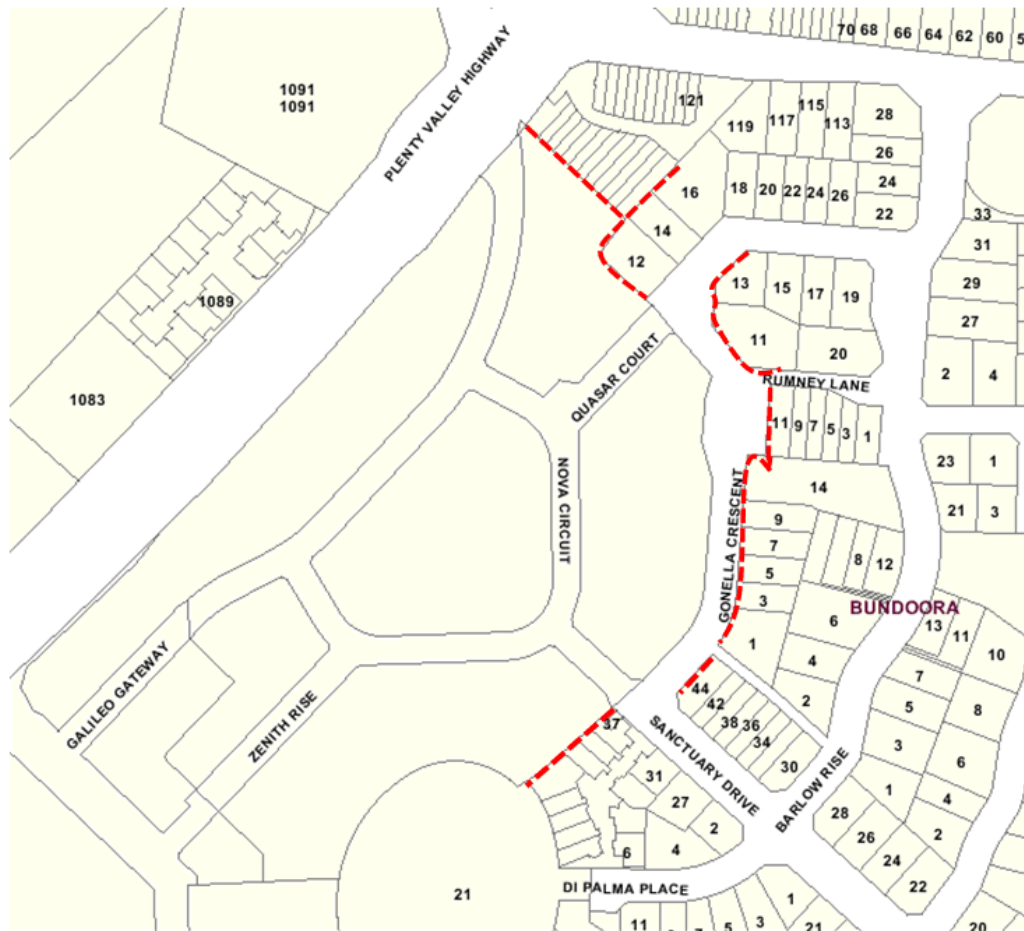


Figure 11: Sensitive residential interfaces

Setbacks to abutting dwellings should be designed to appropriately respond to existing residential amenity in keeping with relevant ResCode Standards B17 to B24 as provided at Clause 55 of the Darebin Planning Scheme. Setbacks to corner lots must enable sufficient sightlines for traffic in accordance with the relevant Australian Standard (AS2890.1-1989).

New development is also to be set back from the internal roads and along the important north-south axis to provide opportunity for an open landscape setting. Open front setbacks will help to preserve sightlines and provide landscaping opportunities. The siting of at-grade car parking generally out of view of the public realm will also reinforce the achievement of open, landscaped front setbacks.

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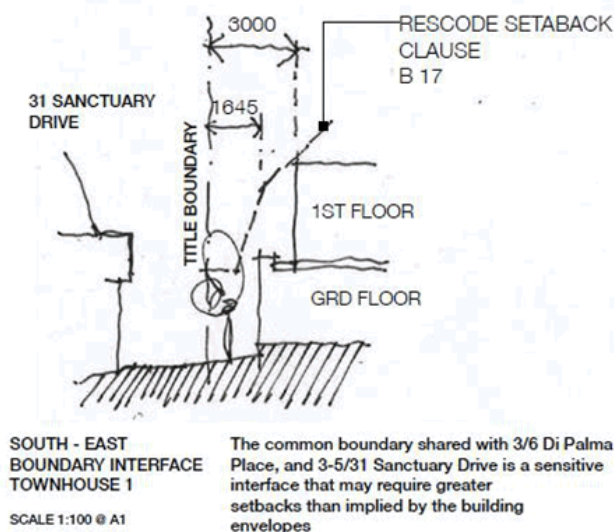


Figure 12: Indicative setback treatment to southern adjoining residences on Lot 7 (Standard B17)

Retained Former Hospital Buildings – Design and Architectural Approach

The architectural presentation and design of the buildings is a key element of the heritage values of the place. The Tudor Revival buildings incorporate Moderne elements that result in a highly detailed and cohesive design outcome.

The conservation and restoration of former hospital buildings aims to retain and conserve the site’s historic meanings and architectural presentation. This enables the continued interpretation and understanding of the heritage values associated with the former Hospital and through surviving evidence of the original master plan.

Conservation and restoration works should focus on returning high quality and properly conserved external elevations to the retained buildings. Conservation works should utilise traditional materials and construction methods. Reconstruction and repair works will be undertaken with like materials and detailing.

Adaptive reuse works should ensure that the exterior presentation of the heritage buildings is largely unaffected. Any alteration to the exteriors of buildings for new openings and the like should minimise the visual and physical impacts of new and introduced elements to the heritage buildings, so as to not undermine the architectural presentation of the buildings. New and introduced elements should be undertaken on lesser elevations with consideration to minimising their visual impact.

In particular, the rear of the buildings has the potential to contain additions, pavilions, infrastructure and car parking but must maintain sufficient visual and physical separation. The position and scale of new structures should be informed by the potential for visibility from the public domain and aim to minimise their visual presence.

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There is considerable flexibility in terms of the treatment and alteration of internal spaces.

New Development – Design and Architectural Considerations

Future site development should aim to maintain and enhance the architectural and aesthetic values of the former Larundel psychiatric hospital, through considered design and inclusion of new structures and landscape elements.

New built form should demonstrate excellence of design and attention to construction quality. Building facades should represent a simple and contemporary style. Bulky roof forms or extensive blank facades in a single material or colour should be avoided. The architectural design of new buildings should utilise contemporary architectural language but reflect the scale and form of the existing heritage buildings.

Development should give primacy to the street address and development with two street frontages is to maintain articulated and façade treatments to both frontages. Activated edges should achieve a high level of pedestrian amenity through a combination of uses, entrances, glazing, and other design detail and architectural treatments.

Blank walls should be avoided to ensure an attractive and visually appealing presentation. The visual impact of large areas of exposed walls is to be reduced in scale by architectural treatments and or use of colour. Exposed blank walls are to be treated to the same standard as the rest of the building.

Plant and equipment will be concealed or screened and exterior elements are to be integrated into the building.

The master plan identifies the location of new development amongst the extant hospital buildings. New buildings should be responsive in scale and height to the extant heritage places and have been carefully sited in order to preserve visual links between the heritage buildings. New elements will strike a balance between achieving an appropriate design response that considers the heritage values of the site and fulfilling a new design brief.

New buildings and additions to heritage places should have regard to the form, scale and visibility of development. New buildings should aim to be visually recessive and deferential to the significant heritage buildings by:

- Adopting a lower scale when adjacent to the heritage buildings;
- Referencing the form and scale of historic buildings in developing an architectural response;
- Providing sufficient setbacks, separation from heritage buildings and of landscape treatments;
- Locating additions to the heritage buildings preferably to the rear, away from the public domain and separated by low level linking elements;
- New buildings being designed with articulated plan and three dimensional forms.



Materials and finishes

A varied palette of materials is to be used to ensure a visually rich environment. Materials and finishes are to be robust.

Conservation works should utilise traditional materials. Reconstruction and repair works will be undertaken with like materials and detailing.

The reuse of heritage bricks from former hospital buildings which are to be removed is encouraged in the conservation and repair of retained heritage fabric where feasible and/or as part of landscaping treatments.

New buildings and additions are encouraged to comprise a contemporary design and detail response. Material selection may refer to the extant materials palette but the use of modern materials is appropriate.

New built form proposed with an interface to Quasar Court on Lot 10 should also recognise the existing character and themes of materials, finishes and colours and create a transition between new and existing development.

4.1.5 Open Space

The new mixed use buildings proposed in Lot 2D along a 40m frontage to Plenty Road will be separated by the 2000sqm of Public Open Space, which not only will become a green, community space but will also provide views from Plenty Road in to the site and to the former hospital buildings.

The 2,000sqm of Public Open Space will be vested in or transferred to Council within 6 months following the issue of either a Statement of Compliance for the subdivision of Lot 2D or the issue of an occupancy permit for any building constructed on Lot 2D, whichever occurs earliest.

Prior to vesting or transferring the land, the landowner must:

- Prepare a landscape design for the POS Land to the satisfaction of Council which is designed to be generally in accordance with that standard usually required by Council for a similar sized Public Open Space; and
- Carry out improvements to the POS Land in accordance with the design approved by Council at the full cost of the Owner.

Following the vesting or transfer of the open space, no further Public Open Space contributions are payable in respect of the Village Precinct or original Mixed Use Precinct.

4.1.6 Landscape

Future landscaping of the Village Precinct is to provide an attractive setting that seeks to enhance the aesthetic value of the site through the maintenance of existing landscape elements and the introduction of landscape treatments that are informed by historic precedents.

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With respect to heritage considerations, there is a strong emphasis on the preservation of the site's landscape character and the understanding that the former hospital buildings were traditionally seen within an open landscape setting.

The provision of landscaped open front setbacks will reinforce the site's traditional landscape character and also ensure visually intrusive at-grade car parking is located out of view from the public realm.

Landscape responses should exclude residential front fences onto the internal roadway complex, except where required to comply with the Building Code of Australia. Boundary delineation should be achieved using a landscape resolution more in keeping with the landscape context of the site, such as low hedges. Insensitively designed fencing has the potential to diminish the presentation of the heritage place.

The general vegetation strategy includes the provision of a mix of exotic and native tree species to complement the existing site character, whilst retaining as many of the trees identified for retention in Figures 81 and 82 of the PDP as practical.

Landscaping should contribute to a high quality frontage and appropriate species selection should be used to visually soften and reduce building bulk and screen visually obtrusive areas. Environmental factors should be considered in relation to minimising surface runoff, species selection, materials and finishes and ongoing energy requirements. In selecting plant species, consideration should be given to local soil and weather conditions, sun and ongoing moisture requirements, shade and drought tolerance and screening potential. The species of all proposed plants selected must be to the satisfaction of the responsible authority.

Existing vegetation that is retained should be protected during construction. Tree Protection Zones (TPZ) must be generally established prior to construction around trees and/or groups of trees that are to be retained. The TPZ will need to be determined in consultation with an arborist and is dependent upon tree species and age for example, and also in relation to proposed development adjacent to identified trees.

New street tree planting will comprise both exotic and native species to reinforce the current character of vegetation on the site. Selection will be based on providing solar access to dwellings, habitat creation, trees that do not damage infrastructure and the need to provide shady streets for pedestrian amenity.

New planting on the land surrounding Lancaster Green will comprise new tree and lawn planting to provide an attractive and passive interface between Lancaster Green and the heritage buildings.

As noted above, the new area of Public Open Space proposed along Plenty Road is subject to a landscape design to the satisfaction of Council and is to be designed generally in accordance with the usual standards required by Council for a similar sized Public Open Space

A detailed landscape plan, including existing trees to be retained and removed, should be prepared by a qualified landscape architect with the permit application for each lot. The landscape plan should be generally provide:



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- Existing trees to be retained or removed.
- A planting schedule of proposed vegetation detailing the botanical name, common name, size at maturity and quantities of all plants.
- Clear graphics identifying trees (deciduous and evergreen), shrubs, groundcovers and climbers; and a scale, north point and appropriate legend.
- Street trees within the nature strip/s adjacent to the property.
- Details of all surfaces including lawns, mulched garden beds and hard paving (such as asphalt, concrete, brick or gravel).
- All constructed items including retaining walls, letter boxes, garbage bin receptacles, outdoor furniture, lighting, clothes lines etc.
- Edge treatment between grass (lawn) and garden beds.
- An outline any building/s including any basement, the location of entry doors, windows, gates and fences.
- The location of both existing and proposed overhead and underground services. Conflicts of such services with the existing and proposed planting must be avoided.

Arboricultural considerations

An arboricultural assessment was carried out by Stephen Fitzgerald Arboriculture in 2007, and is attached to the PDP as Appendix E. This assessment assigned an arboricultural value to existing trees (high, medium and low retention value) and identified trees in poor health or dangerous condition and those recommended for removal. The results of the arboriculture assessment are summarised in Figure 82 of the PDP.

An arborist report will be provided as part of all planning permit applications, detailing all existing vegetation and assessing the benefit of retaining all trees and providing critical root zones and retention measures.

Vegetation removal

The master plan seeks to retain as many of the existing trees identified for retention in Figures 81 and 82 of the PDP as possible.

However, in order to achieve the ultimate development vision for the Village Precinct, some existing trees, in addition to those proposed for removal in the 2007 arboriculture assessment, may require removal despite good health or significance. Any removal of existing trees is to be balanced with appropriate new replacement vegetation. The extent of vegetation proposed to be removed on site is not considered excessive having regard to future replacement planting and the Village Precinct master plan.

Tree retention policy

A planning permit must require that if any tree which is identified for retention in Figure 82 of the PDP, subject to a future review of arboricultural values, is removed or destroyed during and by construction works, a monetary amount shall be paid to the responsible authority unless the responsible authority agrees to revegetation “works in lieu” of an equivalent value.

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The monetary amount in respect of each tree is to be determined by an appropriately qualified person and is to be agreed upon between Council and the permit applicant.

The payment of contributions and offset plantings is to compensate for the loss of amenity associated with the removal of trees, with the calculation of the amenity value based upon the existing Arborist Report (Appendix E: Lancaster Precinct Arboricultural Report prepared by Stephen Fitzgerald Arboriculture June 2007) or future updated arboricultural advice, and in accordance with Council's Tree Retention Policy 2007.

If any monies are paid to the responsible authority under this provision, these monies should be used by the responsible authority for revegetation works on the land or in a location agreed by both parties if insufficient land area is available on site.

Trees planted in the new POS Land, subject to first having been approved for such planting by Council, shall be offset as a credit for any trees removed to the satisfaction of Council.

4.1.7 Traffic and parking

No change is proposed to the existing road network, comprising roads and connections to Plenty Road, the Town Centre and surrounding residential neighbourhood. The existing road network, as created under the PDP, provides for vehicle, pedestrian and bicycle connections. Additional pedestrian and vehicle connections are proposed under the master plan within individual lots and connecting with Lancaster Green, surrounding streets, basements or at-grade car parking areas.

The proposed internal road network retains the historic layout of the key access roads and provides an integrated network to cater for the needs of pedestrians, cyclists, cars, and service, emergency and delivery vehicles. The proposed vehicular access connections onto Plenty Road and Main Drive provide for safe, convenient and dispersed connections onto the adjacent arterial road network.

Design of at-grade car parking in the Village Precinct, minor roads 'mews' and pathways should consider the aesthetic values of site. New roads and paths should be of a lesser scale and employ materials that differentiate them from the reconstructed road layout. Car parking should have a limited interface with the roadway views of the hospital buildings. Large areas of hardstand for car parking, especially adjacent to the former hospital buildings should be avoided, or if it cannot be avoid, otherwise treated in a manner to minimise the visual impacts.

Car parking rates have been established under the PDP and in subsequent planning permits. A detailed traffic engineering assessment should be prepared by a qualified traffic engineer with the permit application for each lot. The assessment should include a car parking demand analysis and address Clause 52.06 of the Darebin Planning Scheme. Future development should have regard to parking rates approved under the PDP and in subsequent permits, including those below. Established parking rates may be varied for example in accordance with sustainable transport principles, empirical data or temporal parking demand analysis, to Council's satisfaction.

- 0.7 spaces per 1 bedroom dwelling
- 1 space per 2 bedroom dwelling



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- 2 spaces per 3 bedroom dwelling
- 0 spaces per dwelling for visitor parking may be provided on-street
- A peak rate of 0.3 spaces per student bed for student accommodation
- A peak rate of 0.3 spaces per aged care bed (inclusive of staff and visitor parking) and a peak rate of 0.5 spaces per unit for independent units for older persons, or 1 space per dwelling for townhouses.

For the avoidance of doubt, where there are differing rates of provision in the VPDP or Clause 52.06, the lower rate is to be applied.

It is intended that future redevelopment will provide self-sufficient car parking facilities via a combination of basement, at-grade and on-street car parking with consideration to any temporal parking characteristics. Areas surrounding car parks should be landscaped, and include canopy tree planting.

The design and configuration of car parking facilities should generally accord with the provisions of Clause 52.06 of the Darebin Planning Scheme or Australian Standards AS/NZS2890:1:2004.

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Transport Management Plan

A Transport Management Plan (TMP) has been prepared by Cardno for the VPDP.

As with the Integrated Transport Plan (ITP) prepared by Ratio Consultants in July 2009, the TMP is intended to act as 'base' reference document and is to be used as a guide only for the future development within the Polaris Village Precinct.

The TMP has been prepared to address the following:

- 1. Estimate of each transport mode's share of travel.*
- 2. A traffic analysis which has regard to the likely pattern and intensity of development and the expected number of trips generated.*
- 3. Works to existing and proposed intersections and proposed traffic management and control works on site and on adjoining roads where necessary to accommodate traffic increases and safe pedestrian movement.*

The TMP requires the proponent to undertake Traffic and Transport Assessments in support of each Planning Permit Application for development in the future. These assessments should consider specific detail of modal share, traffic generation and any future road works required to accommodate development generated traffic.



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4.1.8 Servicing and infrastructure

Subject to augmentation and upgrade works, the Village Precinct will be generally serviced by existing infrastructure.

The provision of all services (electricity, gas, water supply, sewerage drainage etc) to the Village Precinct must be undertaken to the satisfaction, and in accordance with the requirements/conditions of the relevant authority. All telecommunication and electrical supply is to be provided underground.

Consultation with all relevant services authorities during the design stages of each lot will ensure the coordination of utility services to minimise construction and maintenance costs and disruption to the public and landscaping.

Fire hydrants must be located in accordance with the requirements of the Metropolitan Fire Brigade.

Roads and drains must be designed and constructed in accordance with the City of Darebin Road Geometry and Design Guidelines to the satisfaction of the responsible authority.

No buildings or works are to be constructed over any easement or other restriction on the land or any sewers, drains, pipes, wires or cables under the control of a public authority or the responsible authority without the prior written consent of the responsible authority and any relevant authority

The existing access points in Plenty Road will be retained and upgraded where required subject to traffic engineering assessment in accordance with Council and VicRoads requirements.

Prior to the commencement of any development permitted by a planning permit, a detailed drainage plan must be prepared to the satisfaction of the responsible authority.

4.2 Implementation

4.2.1 Stages of development

Under the revised Development Plan, the retained former hospital buildings will be restored and adapted for reuse. Building and works are proposed to commence on Lot 7 and will include both restoring retained former hospital buildings and the construction of new apartments and townhouses.

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4.2.2 Future planning permit approvals

Redevelopment of the Village Precinct has commenced with this development plan. The next steps in the approval process are envisaged to be:

- The layout of all proposed buildings will be finalised and approved at the planning permit application stage for each lot.
- Future permit applications will have regard to the heritage values of the site as clarified in the recommendations of the 2012 CMP Review.
- Indicative uses have been identified at Section 4.1.2. The final uses for each lot will be finalised at the planning permit application stage.
- Other than the 2000sqm Public Open Space, the location of passive and open space will be finalised during the detailed design of each future lot at the planning permit stage.
- Specific design detail pertaining to vegetation and landscaping will be provided at the future planning permit stage for individual lots in keeping with the parameters provided at Section 4.1.6.
- Detail relating to the future subdivision of each lot will be provided at the subdivision permit stages
- Servicing infrastructure needs will be addressed at planning permit stage for each lot.
- An updated survey and detailed arboriculture assessment will be undertaken at the planning permit stage for each lot.
- The provision of a Traffic and Transport Assessment will be undertaken at the planning permit stage for each lot
- As part of the any planning permit issued for building and works including demolition on Lot 8, a Section 173 Agreement is to be registered on title prior to the commencement of demolition, unless otherwise agreed in writing with the Responsible Authority.

Note Appendices A to D are not included as they are addressed in full through the Officer's assessment report.



43/47

Village Precinct Development Plan

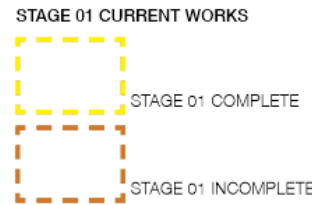
DRAWING LEGEND

- APARTMENT
 - 3 storey
 - 4 storey
 - 5 storey
- TOWNHOUSES
 -
- COMMERCIAL
 -
- HERITAGE
 -
- BY OTHERS
 -
- APPROVED/ BUILT
 -

ALL DEVELOPMENT TO BE GENERALLY IN ACCORDANCE WITH VILLAGE PRECINCT DEVELOPMENT PLAN

NOTE: INDICATIVE BUILDING ENVELOPES REPRESENT A MAXIMUM BUILDING HEIGHT AND ARE PROVIDED TO ALLOW AN APPRECIATION OF THE MAXIMUM SCALE OF DEVELOPMENT OVER THE SITE ONLY. THEY DO NOT IMPLY A RIGHT TO BUILD WHOLLY WITHIN THE BUILDING ENVELOPE AND ALLOW SOME FLEXIBILITY HAVING REGARD TO THE SLOPING NATURE OF THE LAND AND THE POSSIBLE NEED TO VARY PLANS IN THE FUTURE WITHOUT AMENDING THE DEVELOPMENT PLAN. ALL FUTURE DEVELOPMENT WILL BE SUBJECT TO PLANNING AND PERMIT APPLICATIONS. THE FORM, SETBACK AND HEIGHT OF FUTURE BUILDINGS MUST BE DESIGNED HAVING REGARD TO THE SITE CONTEXT HAVING FULL REGARD TO

NOTE: 1. ALL FUTURE DEVELOPMENT IS SUBJECT TO A PLANNING PERMIT APPLICATION.
 2. BUILDING ENVELOPES ARE INDICATIVE ONLY. NEW BUILDINGS ADJACENT TO RETAINED HERITAGE BUILDINGS TO BE OF AN EQUIVALENT SCALE OR LESS.
 3. BUILDING HEIGHT PROVIDES AN INDICATION OF MAXIMUM BUILDING HEIGHT.
 4. STOREYS ON THE DEVELOPMENT PLAN EXCLUDE BASEMENTS, AS DEFINED UNDER CLAUSE 72 OF THE DAREBIN PLANNING SCHEME.



- TOPOGRAPHY OF THE LAND.
- THE SCALE, LOCATION AND VIEWS OF ADJACENT HERITAGE BUILDINGS.
- THE AMENITY OF THE PUBLIC OPEN SPACE INCLUDING MASS AND BULK IMPACTS.
- MASS AND BULK IMPACTS UPON ADJACENT DWELLINGS (SHELDED PRIVATE OPEN SPACE).
- OVERSHADOWING IMPACTS UPON ADJACENT DWELLINGS (SHELDED PRIVATE OPEN SPACE).
- THE NEED TO PROTECT AND RETAIN VEGETATION ON THE SITE.
- PLENTY ROAD

LOT 7: ANY DESIGN WILL NEED TO PROVIDE AN APPROPRIATELY LANDSCAPED SOFT EDGE TRANSITION FROM THE HARD EDGE OF THE 2 AND 3 STOREY BUILT FORM IN THE PUBLIC OPEN SPACE SETBACK AND THE FRONT SETBACK TO THE ROAD RESERVE, WITH SUFFICIENT LAND TO PROVIDE FOR APPROPRIATE CANOPY TREES ON THE PRIVATE LAND. CANOPY TREES WILL COMPRISE A MEDIUM CANOPY TREE IN THE FRONT SETBACK AND PUBLIC OPEN SPACE SETBACK OF TWO OF EVERY THREE OF THE TRIPLE STOREY DWELLINGS AND A SMALL CANOPY TREE IN THE FRONT SETBACK AND PUBLIC OPEN SPACE SETBACK OF TWO OF EVERY THREE OF THE DOUBLE STOREY DWELLINGS. A QUALITATIVE ASSESSMENT WILL NEED TO BE PROVIDED BY AN ARBORIST AND / OR LANDSCAPE ARCHITECT DEMONSTRATING THAT THE DESIGN ALLOWS AN ADEQUATE SOFT EDGE TRANSITION. SETBACKS MAY NEED TO BE INCREASED TO PROVIDE FOR THE TREES. CANOPY TREES MUST HAVE THE FOLLOWING MINIMUM WIDTHS AT MATURITY: SMALL CANOPY (4 METRES), MEDIUM CANOPY (6 METRES).

LOT 10: APARTMENT BUILDING FRONTING GALILEO CIRCUIT REDUCED IN HEIGHT TO 3, 4 AND 5 STOREYS (FROM 4 AND 5 STOREYS) HAVING REGARD TO THE TOPOGRAPHY OF THE LAND AS SHOWN ON FIGURE 9 INDICATIVE WEST ELEVATIONS.

LOT 9: APARTMENT BUILDING FRONTING NOVA CIRCUIT REDUCED IN HEIGHT TO 2 STOREYS (FROM 3 STOREY & 4 STOREYS) AND CONVERTED TO TOWNHOUSES.

LOT 8: UPPER LEVELS MAY OVERHANG THE NOVA CIRCUIT GROUND LEVEL SETBACK AND MUST MAINTAIN A MINIMUM 1m SETBACK FROM THE BACK OF FOOTPATH.

AS PART OF ANY PLANNING PERMIT ISSUED FOR BUILDING AND WORKS INCLUDING DEMOLITION ON LOT 8, A C173 AGREEMENT MUST BE REGISTERED ON TITLE PRIOR TO THE COMMENCEMENT OF DEMOLITION, UNLESS OTHERWISE AGREED IN WRITING WITH THE RESPONSIBLE AUTHORITY.



1. INDICATIVE YIELD ONLY
2. INDICATIVE LAND-USE TYPOLOGIES SHOWN
3. INDICATIVE SETBACKS SHOWN
4. FINAL YIELD, LAND USE TYPOLOGIES AND SETBACKS TO BE DETERMINED AT THE PLANNING PERMIT APPLICATION STAGE AND FOR EACH LOT OR PART THEREOF.

RECEIVED ON
 18 MAR 2019
 CITY OF DAREBIN
 STATUTORY PLANNING

Indicative Village Precinct Development Plan



8th March 2019
 1225/MP02T

RECEIVED ON
 18 MAR 2019
 CITY OF DAREBIN
 STATUTORY PLANNING



DRAWING LEGEND

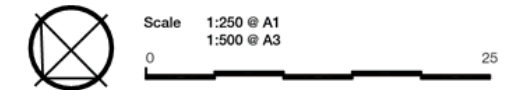
- TOWNHOUSE
- GARAGE / CARPORT
- VEHICULAR ACCESS.
- PEDESTRIAN ACCESS.
- BUILDING OVER
- CARPARK BASEMENT UNDER.
- HIGH RETENTION VALUE TREE
- MEDIUM RETENTION VALUE TREE
- LOW RETENTION VALUE TREE

SITE PLAN

INDICATIVE YIELD ONLY.

Clarke Hopkins Clarke
 115 Sackville Street
 Collingwood VIC Australia 3066
 Telephone (03) 9419 4340
 Facsimile (03) 9419 4345
 Email studio@chc.com.au
 www.chc.com.au

Block 2C - Lot 9 CONCEPT 02
 Polaris 3083 Bundoora



13th Feb 2019
 1279/CP01e

**5.3 STATUTORY PLANNING COMMITTEE REPORT
67-73 Mahoneys Road, Reservoir**

Author: Urban Planner

Reviewed By: General Manager City Sustainability and Strategy

Applicant	Owner	Consultant
Victoria Police	Department of Justice and Regulation	Maureen Jackson Planning

SUMMARY

- Construction of a double story building for use as a Police Station/Emergency Services Facility. It is envisaged that there will be 93 staff and ten (10) operational vehicles on site.
- This report assesses the application on its planning merit. Matters such as the preferred location of a police station are not relevant to council’s decision about a planning permit and cannot be considered by council in its role as Responsible Authority. As such, these matters are not discussed in this report.
- No detailed internal floor plan is included for confidentiality purposes, nevertheless the plans show that the ground floor will include a reception, foyer, public WC and the applicant has indicated the first floor will generally be used for office purposes.
- There are to be 48 car spaces provided in total on site, with construction of a 6.4 metre wide crossover to the north east corner of the site.
- The building will have a contemporary design with a maximum height of 9.67 metres and will have brick walls to the ground level, lightweight metal cladding and perforated metal to the first floor and a skillion roof.
- The site is zoned General Residential Zone – Schedule 1 and is affected by the Development Contributions Plan Overlay.
- The mandatory garden area requirement is not applicable, as the application is not for the construction or extension a dwelling or residential building on a lot.
- There is no restrictive covenant on the title for the subject land.
- Two (2) objections were received against this application.
- The proposal is generally consistent with the objectives and standards of Clause 32.08 of the Darebin Planning Scheme.
- It is recommended that the application be supported.

CONSULTATION:

- Public notice was given via two (2) signs posted on site and letters sent to surrounding owners and occupiers.
- This application was referred internally to the Transport Engineering and Strategy, Public Places, and Infrastructure and Capital Delivery units.
- This application was referred externally to VicRoads and Transport for Victoria.

Recommendation

That Planning Permit Application on D/872/2018 be supported and a Notice of Decision to Grant a Permit be issued subject to the following conditions:

- (1) Before the development starts, amended plans to the satisfaction of the Responsible Authority must be submitted to, and approved by, the Responsible Authority. The plans must be drawn to scale with dimensions and must be generally in accordance with the plans submitted with the application (identified as drawing nos TP.00.03, TP.01.01, TP.01.02, TP.02.01, TP.02.02 Revision A, dated 26 November 2018, project no. 1820 and prepared by FMSA Architects) but modified to show:
 - a) The redundant vehicle crossovers are to be removed and the naturestrip, kerb and channel reconstructed.
 - b) The height of fences on the eastern, western and southern common boundaries (except within 2.25 metres of the front boundary of the land) to be a minimum height of 2.0 metres as measured above natural ground level.
 - c) The west-facing first floor windows are to be confirmed to have fixed obscure glazing (not film) with a maximum transparency of 25% to a minimum height of 1.7 metres above finished floor level. The northernmost west-facing first floor window may have a fixed screen with a maximum permeability of 25% to a minimum height of 1.7 metres above finished floor level.
 - d) The eastern, western and southern common boundary fences are to be acoustically treated (in accordance with Condition No. 10 of this Permit) to minimise noise disturbance to the adjoining residential properties.
 - e) A landscape plan in accordance with Condition No. 4 of this Permit.
 - f) Modifications in accordance with the Sustainable Design Assessment (Refer to Condition No. 7 of this Permit).
 - g) Modifications in accordance with the Waste Management Plan (refer to Condition No. 9 of this Permit).
 - h) Any modification and/or annotations in accordance with the requirements of Condition No. 10 of this Permit).
 - i) Modification in accordance with Transport for Victoria Conditions No. 22-24 of this Permit.
 - j) Modifications in accordance with VicRoads Conditions No. 25-27 of this Permit.
 - k) The location of all plant and equipment (including air conditioners and the like). These are to be co-located where possible, screened to be minimally visible from the public realm and adjacent properties, located as far as practicable from site boundaries and integrated into the design of the building.
 - l) A comprehensive schedule of external materials, colours and finishes (including colour samples). Construction materials are to be low maintenance. External materials and finishes (including glazing) are to be of a low reflectivity level. The use of painted surfaces must be minimised.

Annotated coloured elevations showing the location/application of the materials, colours and finishes must be provided.

When approved, the plans will be endorsed and form part of this Permit.

- (2) The development as shown on the endorsed plans must not be altered without the prior written consent of the Responsible Authority.
- (3) This Permit will expire if either:

- The development does not start within three (3) years from the date of this Permit; or
- The development is not completed or the use is not started within five (5) years of the date of this Permit.

As relevant, the Responsible Authority may extend the times referred to if a request is made in writing:

- Before this Permit expires;
 - Within six (6) months after the expiry date; or
 - Within twelve (12) months after the expiry date if the request relates to the completion of the development or a stage of the development.
- (4) Before buildings and works start, a detailed Landscape Plan to the satisfaction of the Responsible Authority must be submitted to, and approved by the Responsible Authority generally in accordance with the plans submitted with the application (identified as drawing nos LP1.P1, LP1.P2, Revision 1, dated 23 November 2018, project no. 3163 and prepared by Mexted Rimmer Landscape Architects). When the Landscape Plan is approved, it will be endorsed and will then form part of this Permit. The Landscape Plan must be prepared by a suitably qualified person and must incorporate:
- a) Landscape plan to be updated to correspond to architectural plans, detailing generator location, bicycle hoops, barbeque facility etc.
 - b) Provision of underground rainwater tank(s) and additional seating area for the barbeque facility/southern area, with outdoor table and seating.
 - c) Fence heights to be shown on the plan.
 - d) Additional canopy trees, with a large canopy tree for the front central landscape area directly north of the deliveries/turning bay and larger canopy trees incorporated to the west and south of the site.
 - e) Replacement of the western boundary planting (*Westringia* spp.) with narrow upright canopy trees.
 - f) Planting along eastern boundary should reach a minimum of 3-4m height at maturity to provide screening to neighbouring property secluded private open spaces.
 - g) Tree Protection measures to be included for all nature strip trees and neighbouring trees.
 - h) Any lighting provisions for the front landscaping to be noted.
 - i) SDA Modifications related to the landscape, WSUD or urban ecology, in accordance with condition 7 of this permit.
 - j) Details of all existing trees to be retained and all existing trees to be removed, including overhanging trees on adjoining properties and street trees within the nature strip. The genus, species, height and spread of all trees must be specified.
 - k) A planting schedule of proposed vegetation detailing the botanical name, common name, size at maturity, pot size and quantities of all plants.
 - l) A diversity of plant species and forms. All proposed planting must be to the satisfaction of the Responsible Authority.

-
- m) Where the opportunity exists, an appropriate number and size of canopy trees are to be shown on the subject site, commensurate with the size of planting area available. All canopy trees are to have a minimum height of 1.6 metres in 40 litre containers at the time of installation. Canopy trees must have the following minimum widths at maturity: small canopy (4 metres), medium canopy (6 metres), large canopy (10 metres).
 - n) Annotated graphic construction details showing all landscape applications and structures including tree and shrub planting, retaining walls, raised planter bed and decking.
 - o) Type and details of all surfaces including lawns, mulched garden beds and permeable and/or hard paving (such as pavers, brick, gravel, asphalt and concrete) demonstrating a minimum site permeability of 20%. Percentage cover of permeable surfaces must be stated on the plan. Where paving is specified, material types and construction methods (including cross sections where appropriate) must be provided.
 - p) Hard paved surfaces at all entry points to the building.
 - q) All constructed items including garbage bin receptacles, lighting, tanks, outdoor storage etc.
 - r) Type and details of edge treatment between all changes in surface (e.g. grass (lawn), gravel, paving and garden beds).
 - s) An outline of the approved building/s including any basement, the location of entry doors, windows, gates and fences must be shown on the landscape plan. The location of both existing and proposed overhead and underground services. Conflicts of such services with the existing and proposed planting must be avoided.
 - t) Clear graphics identifying trees (deciduous and evergreen), shrubs, grasses/sedges, groundcovers and climbers.
 - u) Scale, north point and appropriate legend. Landscape plans are to be clear, legible and with graphics drawn to scale, and provide only relevant information.
 - v) Landscape Specification Notes including general establishment and maintenance requirements.
- (5) The landscaping as shown on the endorsed Landscape Plan must be completed to the satisfaction of the Responsible Authority before the development is occupied and/or the use starts or at such later date as is approved by the Responsible Authority in writing.
- No later than seven (7) days after the completion of the landscaping, the permit holder must advise Council, in writing, that the landscaping has been completed.
- (6) The landscaping as shown on the endorsed Landscape Plan must be maintained, and any dead, diseased or damaged plant replaced in accordance with the endorsed Landscape Plan to the satisfaction of the Responsible Authority.
- (7) The development must incorporate the sustainable design initiatives in accordance with the Sustainable Design Assessment (SDA) (identified as Sustainable Design Assessment – 67-73 Mahoneys Road, Reservoir, dated 08/10/2018, and prepared by RG – Low Impact Development Consulting) to the satisfaction of the Responsible Authority.
- Prior to the occupation of the development, a report from the author of the SDA, approved pursuant to this permit, or similarly qualified person or company, must be submitted to the Responsible Authority. The report must be to the satisfaction of the Responsible Authority and must confirm that all measures specified in the SDA have been implemented in accordance with the approved Plan.
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(8) At the completion of the constructed ground floor level(s), and before the commencement of the building frame or walls, the ground floor level(s) must be confirmed. This confirmation must be in the form of a report from a licensed land surveyor and must be submitted to the Responsible Authority no later than 7 days from the date of the inspection. The upper floor level(s) must be confirmed before an Occupancy Permit is issued in the form of a report from a licensed land surveyor and submitted to the Responsible Authority.

(9) Before the development starts, a waste management plan, to the satisfaction of the Responsible Authority, demonstrating the operation of the garbage and recyclables storage area must be submitted to the Responsible Authority.

The plan/documentation must demonstrate the means by which garbage and recyclables will be stored on the site and must clearly detail: what waste services will be provided (i.e. cardboard, paper, plastic and metals recycling or comingled waste, general waste, hard rubbish and organic waste), types of bins, types of collection vehicles, frequency of collection, times of collection, location of collection point for vehicles, location of on-site bin storage, location of bins for collection and any other relevant matter.

If Council waste services are proposed to be utilised, a plan is to be submitted illustrating the following:

- a) The length and width of the footpath/ nature strip directly abutting the site boundary.
- b) The location of any available on-street car parking, loading zones and/ or bus stops.
- c) The location of all street furniture, light/ electricity poles, driveways, street trees, bus shelters or similar obstructions.
- d) The location of the bins, with a minimum gap of 300mm between bins and other obstructions.

The plan may require bin sharing or that collection be undertaken by a private contractor if it cannot be demonstrated to the satisfaction of the Responsible Authority that the kerb-side collection of individual bins will not cause car parking and/ or amenity issues.

Waste storage and collection must be undertaken in accordance with the approved waste management plan and must be conducted in such a manner as not to affect the amenity of the surrounding area and which does not cause any interference with the circulation and parking of vehicles on abutting streets.

(10) The development must incorporate the acoustic measures in accordance with the Acoustic Memorandum (identified as Acoustic Memorandum, dated 23 November 2018, received by the Responsible Authority on 7 December 2018, and prepared by Octave Acoustics), to the satisfaction of the Responsible Authority.

(11) The amenity of the area must not be adversely affected by the use or development as a result of the:

- a) Transport of materials, goods or commodities to or from the land; and/or
- b) Appearance of any building, works, stored goods or materials; and/or
- c) Emission of noise, artificial light, vibration, smell, fumes, smoke, vapour, steam, soot, ash, dust, waste water, waste products, grit or oil; and/or

in any other way, to the satisfaction of the Responsible Authority.

(12) Noise from the premises must not exceed the relevant limits prescribed by the State Environment Protection Policy (Control of Noise from Commerce, Industry and Trade) No. N-1.

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- (13) All outdoor lighting must be designed, baffled and/or located to ensure that no loss of amenity is caused to adjoining and nearby land to the satisfaction of the Responsible Authority.
 - (14) The land must be drained to the satisfaction of the Responsible Authority.
 - (15) The loading and unloading of goods from vehicles must only be carried out on the subject land within the designated loading bay, as detailed on the endorsed plans, and must be conducted in a manner which does not cause any interference with the circulation and parking of vehicles on the land or on abutting streets.
 - (16) With the exception of guttering, rainheads and downpipes, all pipes, fixtures, fittings and vents servicing any building on the land must be concealed in service ducts or otherwise hidden from view to the satisfaction of the Responsible Authority.
 - (17) No plant, equipment, services or architectural features other than those shown on the endorsed plans are permitted above the roof level of the building/s without the prior written consent of the Responsible Authority.
 - (18) No goods, equipment, packaging material, or any other material/object must be stored, or left exposed, outside a building so as to be visible from any public road or thoroughfare, to the satisfaction of the Responsible Authority.
 - (19) Before occupation of the development areas set aside for the parking of vehicles and access lanes as shown on the endorsed plan(s) must be:
 - a) Constructed;
 - b) Properly formed to such levels that they can be used in accordance with the plans;
 - c) Surfaced with an all-weather sealcoat;
 - d) Drained;
 - e) line-marked to indicate each car space and all access lanes;
 - f) clearly marked to show the direction of traffic along the access lanes and driveways

to the satisfaction of the Responsible Authority

Car spaces, access lanes and driveways shown on the endorsed plans must not be used for any other purpose.

- (20) Before the development is occupied vehicular crossing(s) must be constructed to align with approved driveways to the satisfaction of the Responsible Authority. All redundant crossing(s), crossing opening(s) or parts thereof must be removed and replaced with footpath, nature strip and kerb and channel to the satisfaction of the Responsible Authority.

Transport for Victoria Conditions:

- (21) Before the development starts, the bus stop and all associated infrastructure, must be relocated and upgraded with shelter to the position indicated in the attached plan 1820SD.00.03 at a cost born by the permit holder to the satisfaction of Public Transport Victoria and deemed compliant with the *Disability Discrimination Act* – Disability Standards for Accessible Public Transport 2002 and in accordance with the attached drawing No. DGN00148-01 and STD_0069 or STD_0070.
- (22) The permit holder must provide GPS co-ordinates and high-resolution photos (300dpi) capturing the arrival and departure side of the stop and include the pole, flag, timetable case and braille ID case to the satisfaction of Public Transport Victoria.

- (23) The permit holder must take all reasonable steps to ensure that disruption to bus operation along the frontage of the site is kept to a minimum during the construction of the development. Foreseen disruptions to bus operations and mitigation measures must be communicated to Public Transport Victoria eight (8) weeks prior by telephoning 1800 800 007 or emailing customerservice@ptv.vic.gov.au. If a temporary stop in an alternative location is required during construction, the temporary bus stop must be provided in consultation with, and to the satisfaction of Public Transport Victoria. Once the new stop is deemed suitable for operation, the temporary stop must be removed in consultation with Public Transport Victoria.

VicRoads Conditions:

- (24) Prior to the commencement of any works, a concept plan must be submitted to and approved by the Roads Corporation. When approved by the Roads Corporation, the plans may be endorsed by the Responsible Authority and will then form part of the permit. The plans must be generally in accordance with the plans submitted with the application but modified to show:
- a) Any existing pole, trees and any road infrastructure within 100m of Mahoneys Road.
 - b) Any existing pole, trees and any road infrastructure within 100m of Mahoneys Road.
- (25) Prior to the commencement of the use of the development, a functional layout plan must be submitted to and approved by the Roads Corporation. The plans must be drawn to scale with dimensions and two copies must be provided. The plans must be generally in accordance with the endorsed plans date stamped but modified to show:
- a) Proposed median opening and necessary signs.
 - b) Any existing pole, trees and any road infrastructure within 100m of Mahoneys Road.
- (26) Before the use approved by this permit commences, the following roadworks on Mahoneys Road as approved by Roads Corporation must be completed at no cost to and to the satisfaction of the Roads Corporation which includes:
- a) Proposed median opening and any associated works.

NOTATIONS

(These notes are provided for information only and do not constitute part of this permit or conditions of this permit)

- N1 Any failure to comply with the conditions of this permit may result in action being taken to have an Enforcement Order made against some or all persons having an interest in the land and may result in legal action or the cancellation of this permit by the Victorian Civil and Administrative Tribunal.
- N2 Nothing in the grant of this permit should be construed as granting any permission other than planning permission for the purpose described. It is the duty of the permit holder to acquaint themselves, and comply, with all other relevant legal obligations (including any obligation in relation to restrictive covenants and easements affecting the site) and to obtain other required permits, consents or approvals.
- N3 The amendments specified in Condition 1 of this Permit and any additional modifications which are "necessary or consequential" are those that will be assessed by Council when plans are lodged to satisfy that condition. Any "necessary or consequential" amendments, in addition to those required by this condition, should be specifically brought to the attention of Council for assessment.

If any other modifications are proposed, application must also be made for their approval under the relevant sections of the *Planning and Environment Act 1987*. They can only be approved once the required and consequential changes have been approved and the plans endorsed. It is possible to approve such modifications without notice to other parties, but they must be of limited scope. Modifications of a more significant nature may require a new permit application.

- N4 This Planning Permit represents the Planning approval for the use and/or development of the land. This Planning Permit does not represent the approval of other departments of Darebin City Council or other statutory authorities. Such approvals may be required and may be assessed on different criteria to that adopted for the approval of this Planning Permit.
- N5 To complete a satisfactory Sustainable Design Assessment (SDA) the Responsible Authority recommends the use of the Built Environment Sustainability Scorecard (BESS) to assess the developments environmental performance against appropriate standards.
- N6 The applicant should be informed all Aboriginal cultural heritage is protected under the *Aboriginal Heritage Act 2006*. If Aboriginal cultural heritage is encountered during development, works must cease in the area and OAAV must be contacted.

INTRODUCTION AND BACKGROUND

A review of Council's records does not reveal any previous planning permit history for the subject site.

ISSUES AND DISCUSSION

Subject site and surrounding area

- The subject site is a consolidated site of four (4) allotments. It is slightly irregular in shape with a frontage of 73.16 metres, a depth of 48.92 metres and an overall area of 3557 square metres.
- The land is located within the General Residential Zone – Schedule 1 and is affected by the Development Contributions Plan Overlay.
- The land is located on the south side of the street, approximately 36 metres to the west of the intersection with Long Street.
- The site contains three (3) single storey detached brick dwellings at numbers 69, 71 and 73 Mahoneys Road and the allotment at 67 Mahoneys Road is vacant. Two (2) single crossovers and a double crossover are provided to the Mahoneys Road frontage (to the east, west and centre of the frontage respectively).
- To the north of the site is Mahoneys Rd, a Road Zone Category 1, providing six (6) lanes and a central median strip. On the opposite side of Mahoneys Road is an industrial zone, with single and double storey industrial buildings and an emergency services facility (MFB Fire Station).
- To the south of the site is a residential area, with the rear yards of dwellings fronting Dredge Street to the immediate south.
- To the east is a single storey brick dwelling.
- To the west is a double storey brick dwelling with vehicle access and a garage adjacent to the common boundary.
- On street parking restrictions in the area include Clearways to the east and west of the site (from Pallant Avenue to Long Street along Mahoneys Road) between 6:30am and

9:30pm Monday to Friday and No standing zones. These apply along the length of the street between 51 and 77 Mahoneys Road.

- The site has good access to public transport including the Keon Park Railway Station approximately 1.2km to the east and bus routes 555, 558 and 902 to High Street, Hughes Parade and Mahoneys Road.
- The site is located approximately 1.2 km from the nearest activity centre at the Johnson St Shopping Centre (corner Keon Parade).

Proposal

- Demolition of existing dwellings and construction of a double storey building for use as Reservoir Police Station/Emergency Services Facility, with 24 hour operation. It is envisaged that there will be 93 staff and ten (10) operational vehicles.
- This new 24 hour police station replaces the existing Reservoir Police Station at 25 Edwardes Street. The new facility will provide modern facilities that will enable Victoria Police to better serve the community given current and future demand for police services. The construction of a new fit-for-purpose 24 hour Police Station aligns with Victoria Police's Corporate Plan 2015-2018 and overall vision to deliver a safer Victoria. Broadly speaking, the new police station will be better equipped to meet the operational requirements of the Victoria Police in this area.
- No floor plan is included (for security and confidentiality purposes), nevertheless the applicant has indicated that the ground floor will include a reception, foyer, public WC and the first floor will generally be used for office purposes.
- It is expected that there will be up to a total of 93 staff, from the Uniform Branch, administration and the Family Violence unit. Staff from the Uniform Branch are proposed to operate over three (3) shifts (7am to 3pm, 3pm to 11pm and 11pm to 7am), while staff of the Family Violence unit generally operate over two shifts (7am to 3pm, 3pm to 11pm). It is understood that the shift times for sworn Police staff will be staggered and shift periods are indicative and will vary. Additionally, administrative staff will generally work at various periods between 7am and 7pm.
- There are to be five (5) visitor car spaces and 43 staff car spaces, with construction of a 6.4 metre wide crossover to the north east corner of the site. The existing crossover is to be retained to the west and remaining crossovers are to be removed. A delivery/turning area is located to the north east section of the building.
- The building will have a contemporary design with a maximum height of 10.3 metres and will have brick walls to the ground level, lightweight metal cladding and perforated metal to the first floor and a skillion roof.
- The proposal will also include waste storage area to the rear of the site and a generator, a barbeque area for staff and rainwater tank to the rear of the building.
- Landscape areas are provided to the perimeter of the site.

Objections summarised

- The development breaches a registered restrictive covenant (single dwelling covenant).
- Impact of the 24 hour operation of the emergency services facility on standards of living.
- Proposed development does not comply with neighbourhood character.
- Visual bulk of the proposed building.
- Noise impacts.
- Overlooking impacts.

- Non-residential building in a residential zone.
- Increased traffic congestion.
- Decreased property value on surrounding land.
- Entrance of the facility is too close to bedrooms.
- Increased criminal activity in the area.
- Amenity impacts during construction (noise, dust, construction vehicles).
- Structural damage to neighbouring properties during construction.

Officer comment on summarised objectionsThe development breaches a registered restrictive covenant (single dwelling covenant)

As per the Certificate of Title for the land, the subject site is not affected by a Registered Restrictive Covenant.

Impact of the 24 hour operation of the emergency services facility on standards of living

While it is acknowledged that the proposed emergency services facility will operate 24 hours a day (by nature of it being a 24 hour Police Station), it is considered that the information provided with the application (notably the traffic management report and acoustic report) adequately justify the proposed use and provide sufficient detail ensuring that the offsite impacts from the proposal will be minimised. Conditions on permit will ensure the recommendations of the relevant reports are incorporated on the plans, where not already shown. In addition, the site is located on a major east/west transport corridor, with extensive commercial operations also in operation along the northern side of Mahoneys Road.

Proposed development does not comply with neighbourhood character

It is noted that the proposal is for a non-residential use in a residential zone, which is acceptable as per the controls at Clause 32.08-2 (General Residential Zone) of the Darebin Planning Scheme. As per the assessment against Clause 22.02 provided below, the proposal should not necessarily have a residential form. In the case of an emergency services facility, a non-residential form is considered to be appropriate in order to adequately distinguish the use and development for the surrounding residential character. In addition, the subject site is located along a major arterial road, rather than a quiet residential street.

Visual bulk of the proposed building

The development proposes a maximum height of 9.67 metres. The scale of the development is considered to be appropriate. While there is no maximum height established under the zone for non-residential buildings, it is worth noting that at 9.67 metres, the emergency services facility complies with the maximum height detailed under the zone for residential buildings (being 11 metres and not more than three storeys at any point). With regard to the intensity of the proposed development, the building is adequately set back from the side and rear boundaries to minimise impacts on the adjoining dwellings. These setbacks are in excess of those that would be required under Clause 54/55. Furthermore, conditions on any permit issued for the proposal will ensure additional planting along the southern, western, and eastern boundaries to reduce the visual impacts of the building.

Noise impacts

An acoustic assessment has been provided with the application. This details the likely noise impacts of the proposed use and the associated noise attenuation measures to minimise impacts to the neighbouring properties. Should the application be supported, a condition will incorporate the recommendations of the acoustic report into the permit and endorsed plans. Generally it is considered that the noise levels generated by the development will be of an acceptable level to ensure that any impacts to the neighbouring sites are minimised. It is noted that the applicant provided an acoustic assessment for the proposal which has identified acoustic treatments to the site and building to ensure noise levels are within an acceptable range.

Overlooking impacts

Although the requirements of Clause 54/55 have limited applicability in the case of a non-residential use in a Residential Zone, the proposal displays high level of compliance, in particular with limiting overlooking to adjoining properties. To the south and east overlooking compliance (with regard to the requirements of Clause 54/55) is achieved through adequate setbacks (in excess of 9 metres). It is acknowledged that additional information on the plans is required to ensure overlooking is limited to the west. A condition on permit will ensure fixed screening to a minimum height of 1.7 metres above FFL and a maximum 25% transparency to the proposed perforated screens at the first floor of the building.

Non-residential building in a residential zone

As per the table of uses at Clause 32.08-2 (General Residential Zone) of the Darebin Planning Scheme, the use of land for an Emergency Services facility requires a planning permit (being a use not directly listed in the table). Importantly, the use is not prohibited within a residential zone, indeed there are numerous examples of emergency services facilities being located within residential zones. As the assessment below attests, the proposal is considered to be generally acceptable in terms of planning merit and a notice of decision to grant a planning permit is recommended.

Increased traffic congestion

A traffic report accompanied the application. The applicant's report, Council's traffic engineer, Transport for Victoria, and VicRoads (the relevant road management authority) are satisfied that the site can accommodate the traffic numbers generated by the development in a safe manner without leading to unreasonable congestion across the site. The traffic experts are satisfied that the proposed vehicle accessways to the site and onto Mahoneys Road are sufficient to provide safe access and egress for the site given anticipated traffic related to the use of the land as an emergency services facility.

Decreased property value on surrounding land

Fluctuations in property prices are a not relevant consideration in assessing medium density development under the provisions of the *Planning and Environment Act 1987*, or the Darebin Planning Scheme.

Entrance of the facility is too close to bedrooms

The development proposes a new vehicle accessway and crossover adjacent the eastern property boundary. This accessway is provided with a 6.6 metre setback from this boundary, given the location of site services and car parking spaces along the boundary itself. It is considered that the setback from the adjoining dwelling to the accessway is sufficient to protect the amenity of residents of that dwelling. Importantly, the site boundaries are to be provided with 2.4 metre high fences, which together with the landscaping within the provided setbacks will further minimise the potential impacts of the vehicle accessway on the neighbouring dwelling.

Increased criminal activity in the area

There is no evidence to link the development of land for an emergency services facility (note: Police Station) with increased crime rates or reduced safety for nearby residents. With respect to the functions of the proposed use of the land, the opposite is more probable.

Amenity impacts during construction (noise, dust, construction vehicles)

Amenity impacts during the construction phase of a development (e.g. noise from construction, traffic attributed to construction vehicles, and other emissions from the site etc.) is a temporary and unavoidable consequence of development and not a reason to refuse development. Noise and other emissions are regulated through the Environmental Protection Authority and the construction of the facility is required to comply with these.

Structural damage to neighbouring properties during construction

Building work can sometimes affect adjoining properties. An owner who is proposing building work has obligations under the Building Act 1993 to protect adjoining property from potential damage from their work. If building work is close to or adjacent to adjoining property boundaries, then an owner may be required to carry out protection work in respect of that adjoining property. This is to ensure that the adjoining property is not affected or damaged by the proposed building work. Protection work provides protection to adjoining property from damage due to building work. It includes but is not limited to underpinning of adjoining property footings, including vertical support, lateral support, protection against variation in earth pressures, ground anchors, and other means of support for the adjoining property. Council notes that this process is not controlled or overseen via planning process and regulations, rather this is addressed under the Building Act and relevant Regulations.

PLANNING ASSESSMENT

This report assesses the application on its planning merit. Matters such as the preferred location of a police station are not relevant to council's decision about a planning permit and cannot be considered by council in its role as Responsible Authority. As such, these matters are not discussed in this report.

At Clause 73.03 of the Darebin Planning Scheme, the proposal is defined as an Emergency Services Facility, i.e.:

Land used to provide facilities for emergency services, such as fire prevention and ambulance services. It may include administrative, operational or storage facilities associated with the provision of emergency services.

The applicant has indicated that the state government is to provide a new police station in Reservoir to give Victoria Police access to modern facilities, to better serve the community.

The proposal is for construction of a 24-hour emergency services facility (police station) to meet current and future demand for police services in Reservoir and to allow deployment within the Darebin municipality.

Importantly the site is located within a General Residential Zone – Schedule 1 and although a residential use would normally be encouraged, the zone purposes allow for provision of educational, recreational, religious, community and a limited range of other non-residential uses to serve local community needs in appropriate locations.

In this instance, the site abuts a Road Zone with a wide road reserve, allowing ample vehicle access to and from the site. In addition, the industrial character to the north, with a fire station allow some latitude to the use and development of the site.

Broadly, the proposal is considered to be appropriate with regard to the physical and strategic/policy context and it is considered that the design response is appropriate.

Planning Policy Framework

Clause 15.01 Built Environment

Relevant urban design objectives and strategies are provided at Clause 15.01 to assess the built environment:

Clause 15.01-1S Urban design

It is considered to that the development has addressed the opportunities and constraints of the site and the site context to form the basis for the consideration of height, scale and massing of the development. The subject site is large, comprising four formerly residential lots. The proposed two storey building is sufficiently set back from the front and side boundaries and maintains an appropriate height and scale. There are ample rear setbacks and landscaping is provided to the front and common boundaries. The height is also an appropriate transition to adjoining buildings. The development responds to its context in terms of character and surrounding landscape.

The site is located in proximity to public transport, with access to walking and cycling facilities and public transport.

Although a high fence is provided, this is acceptable in the context of some nearby fences, the road frontage and the use.

The car parking areas are mostly to the rear, with the visitor parking and access taking up a small proportion of the frontage. The forecourt is appropriate and provides a high standard of design and sense of entry.

The setbacks allow adequate landscaping to the boundaries (subject to condition regarding tree species).

Complies

Clause 15.01-1R Urban design - Metropolitan Melbourne

The proposal is contemporary and well-designed. It is considered to contribute to a distinctive and liveable city with quality design and amenity.

The landscape and design will contribute to an interesting and improved boulevard.

CompliesClause 15.01-2S Building design

The applicant has undertaken an appropriate site analysis as part of the design process and the design takes appropriate consideration of the policy and physical context.

The height, scale and massing are an appropriate transition to the adjoining properties, given that the wide frontage allows adequate setbacks to provide an appropriate transition to adjoining properties.

The detrimental impact of the development on neighbouring properties is minimised by separation provided by the proposed setbacks and landscaping. It is considered that there are no unreasonable amenity impacts.

The building maintains safety, appropriate pedestrian entry, perceptions of safety and security, an appropriate sense of address with an active frontage.

The form, scale, and appearance of development contributes appropriately to a preferred public realm character. The proposal will provide a building of an appropriate height in this location and will not dominate distant views

CompliesClause 15.01-3S Subdivision design**Not applicable**Clause 15.01-4S & 15.01-4R Healthy neighbourhoods

A development in this location encourages access to cycling and walking networks.

The site has safe and convenient access to public transport and other services available.

The site is in proximity to public transport and to the Metropolitan Ring Road, Mahoneys Road, and High Street, which will allow for easy access for Victoria Police vehicles.

The premises has appropriate bicycle parking.

The frontage of the building (cantilevered built form over the entry area) provides weather protection for those accessing the site.

CompliesClause 15.01-5S Neighbourhood character

As noted above, the proposal responds to the character with appropriate scale and setbacks. This also enables landscaping to respond to the context.

The site is not located in a Heritage Overlay.

CompliesClause 15.02-1S Energy and resource efficiency

The adequacy of the amenity and daylight to the internal areas cannot be determined, given the internal layout has not been provided, due to security and confidentiality reasons.

Nevertheless, the applicant has submitted a Sustainable Management Plan, which may be subject to condition.

Complies

Clause 15.03-2S Aboriginal cultural heritage

The site is located in an area of Aboriginal cultural heritage significance, where the objective is to ensure the protection and conservation of places of Aboriginal cultural heritage significance.

The applicant has provided an appropriate Cultural Heritage Management Plan approved under the *Aboriginal Heritage Act 2006*.

Complies

Urban Design Guidelines for Victoria

Clauses 15.01-1S, 15.01-2S and 15.01-4S require that responsible authorities must consider as relevant:

- *Urban Design Guidelines for Victoria (Department of Environment, Land, Water and Planning, 2017).*

The relevant aspects of this assessment are addressed broadly in other areas of assessment in this report and are not required to be repeated.

Clause 19.02-5S - Emergency services

The objective at this Clause is:

To ensure suitable locations for police, fire, ambulance and other emergency services.

The strategies are to ensure emergency services are provided for in or near activity centres and located together in newly developing areas. Although the site is not located in an activity centre, it is located in proximity to the Johnson Street Shopping Centre. In addition, the site is appropriately located on Mahoneys Road, which presents a wide carriageway, allowing ample vehicle access for the emergency services. In addition, it is noteworthy that there is a fire station to the northern side of the street (indicating the appropriateness of the location for access).

MSS and LOCAL POLICIES

The Strategic Framework Plan at Clause 21.01-6 indicates that Mahoneys Road is a Strategic Corridor, with Heidelberg Road and Mahoneys Road being key east-west transport routes and interfaces to adjoining municipalities. Both corridors attract commercial activity due to frontage exposure. Although the use is not commercial, it is a similar emergency service use. It is noted that various commercial uses operate along the northern side of Mahoneys Road in the City of Whittlesea.

Clause 21.02-3 Built Environment

In addressing the relevant key issues and objectives, it is considered the development exhibits good urban design with an appropriate contemporary building transitioning to the lower traditional dwellings, using traditional and contemporary elements and a distinctive, attractive and engaging streetscape presence. Furthermore, the emergency services facility has been sited on a key east-west transport route, which interfaces with the adjoining municipality.

Clause 21.03-2 Housing Development

This Clause recognises that in residential areas, there is a need to accept and accommodate some non-residential activities at appropriate scale and format to service the needs of the local community. One of the key issues is that, as population increases along with demand for services, there is a need to achieve a balance between wider community benefit and potential amenity impacts of non-residential uses in residential areas.

Objective 1 – Housing Provision: *to facilitate housing development that has an appropriate scale and intensity in locations across the municipality.*

Strategies to achieve the objective:

- *In Minimal Housing Change Areas, encourage housing development that is consistent with the type, scale, style and prevailing character of the area and allows for minimal housing growth and change over time*

Objective 2 – Housing density: *To achieve higher density housing outcomes in identified locations to accommodate Darebin's projected population growth.*

Objective 3 – Residential Amenity: *to facilitate residential and mixed use developments that display a high standard of design, limit off-site amenity impacts and provide appropriate internal amenity for residents.*

Strategies to achieve the objective:

- *Require a high standard of design (including architectural quality and environmentally sustainable design) be achieved in residential and mixed use developments through the use of design and development overlays, urban design frameworks, development plans and local policies as appropriate.*
- *Ensure non-residential use and developments in residential zones are designed to minimise negative amenity impacts on the surrounding residential neighbourhood.*

Although the proposal is not residential, the above objectives indicate the preferred character and built form for this area.

It is considered that the proposal has an appropriate height and setbacks, so that it is designed to minimise negative amenity impacts on the surrounding residential neighbourhood

Although the site is located in a broader Minimal Housing Change Area, it is considered that the proposal meets the above policies, with an appropriate height, scale and setbacks. It also provides a site responsive design that respects the lower-scale residential area.

As has already been noted, this non-residential use has been proposed adjacent to a major east-west transport corridor, where it is likely to have reduced impacts on the surrounding residential area.

Darebin Housing Strategy and Clause 21.03 (Housing)

The Darebin Housing Strategy 2013-2033 provides a housing change framework plan that indicates *"the appropriate level of change in terms of the intensity and type of residential development that could be accommodated in areas that permit residential use."*

Although the proposal is not for housing, it is noteworthy that the subject site is identified as an area of minimal change in the Strategic Housing Framework Plan and is defined as:

"Residential areas that have a limited capacity to accommodate future residential development. Minimal Change Areas do not prohibit all residential development, but seek to allow a modest level of development that respects the type, scale and character of the area." (Clause 21.03)

The Strategic Housing Framework Plan builds on the directions for residential land use and development in Darebin as set out by the Darebin Housing Strategy (2013). The policy states that Minimal Change Areas generally display one (1) or more of the following characteristics:

- *Are Heritage Overlay precincts*
- *Are identified in the Darebin Neighbourhood Character Study (2007) as 'potential Neighbourhood Character Overlay areas'*
- *Have a highly intact pattern of subdivision in favour of detached dwellings on individual lots, generally evidenced by more than 80 per cent of housing stock having this attribute*
- *Have a strong neighbourhood character, evidenced by a high degree of consistency in architectural style and streetscape, in particular where 80 per cent or more of the housing stock is consistent with precinct descriptions in the Darebin Neighbourhood Character Study (2007), and where restoration of original housing stock is prevalent*
- *Have identified environmental or landscape significance, including land with frontage to Creek bodies*
- *Excluding heritage precincts, are located:*
 - *Outside an 800 metre walkable catchment of an activity centre.*
 - *Generally outside an 800 metre walkable catchment of train, tram or SmartBus services.*

In looking at the above, it is noted that the site is not located in a Heritage Overlay or subject to a Neighbourhood Character Overlay or Neighbourhood Residential zone. It has a character of varied dwelling forms and is not an area of environmental or landscape significance.

In addition, the site is located in an established area, in proximity to services and facilities and adjacent a key east-west transport route. It is also opposite another emergency services facility, being the Metropolitan Fire Brigade Fire Station 7.

Although the proposal is not for housing, the proposed development of a double storey building over four (4) allotments on an overall site with an area of 3557 square metres is considered to be an appropriate scale and form and is considered to be an acceptable change for the site and surrounding area. This level of change is supported under Clause 21.03 of the Darebin Planning Scheme.

Clause 22.02 - Neighbourhood Character

Neighbourhood Character Precinct Guideline Assessment - Precinct G4

Although the proposal is not for a residential use, the works should be assessed under Clause 22.02 (Neighbourhood Character), as it is located in a residential zone. Nevertheless, in addressing the neighbourhood character and built form, it is important to consider that the proposal is not a residential use and should not necessarily have a residential form. Indeed a non-residential form is appropriate to distinguish the use and development for the surrounding character. In this respect, the application of Clause 22.02 and the Neighbourhood Character Study must be made in a broad sense, as it has limited applicability.

Vegetation

Objective

- *To enhance the landscape quality of the area.*
- *To integrate garden settings with creek-side environs.*

Comment

- There are no significant trees on the site and any vegetation may be removed without requirement for a planning permit. The proposal provides a reasonable amount of landscaping to the front, rear and common boundaries. The level of landscaping is considered to appropriately maintain acceptable the garden setting of the area and streetscape. A condition on permit will ensure appropriate planting across the site.
- There is an accompanying landscape concept plan indicating that the proposal is appropriately sited and designed to incorporate space for the planting of vegetation, such as canopy trees. However, this may be improved with further provision of canopy trees (as recommended in the internal referral comments from the Planning Landscape Architect).
- Although the design provides a large parking area and impervious surfaces, there are adequate landscaped areas around the building. In addition, the main parking area is to the rear, so that it does not represent a large paved area to passers-by.

Complies subject to condition**Siting**Objective

- *To provide space for front gardens.*
- *To encourage new development to contribute to a continuous canopy of trees across the precinct.*
- *To maintain and reinforce the side boundary setback pattern and the existing rhythm of spacing between dwellings.*
- *To minimise the loss of front garden space and the dominance of car parking structures.*

Comment

- The building is set back from the street frontage for planting of vegetation, to enable the continuation of the garden setting in this area.
- The proposal allows sufficient side and rear garden space for landscaping.
- The building is set back from the common boundaries, so that adequate separation is maintained in building forms to the street, respecting the rhythm of dwelling spacing.
- Although there are two (2) crossovers to the street, the site has a wide frontage so that accessways will not dominate the streetscape. In addition, the number of crossovers to the street will be reduced. Although there are some parking areas to the front setback, these represent a minor proportion of the overall frontage, the majority of the parking is to the rear and the loading/turning area is screened by the perforated metal screen to the façade. It is considered that the parking areas are adequately screened from the street, so that they do not dominate the front façades.

Complies**Height and Building Form**Objective

- *To ensure that buildings and extensions respect the predominant height and form of buildings in the streetscape.*

Comment

- Dwellings in the area are largely single storey; however, there are buildings with a double storey height nearby.
- Although the proposal is double storey, it is considered that the overall height represents an appropriate graduated increase in height over adjoining single storey dwellings, with ample setbacks to the boundaries. It is therefore considered that the development is not out of scale with the nearby buildings and does not dominate the streetscape.

Complies with objective**Materials and Design Detail**Objective

- *To encourage buildings that contribute positively to the streetscape through the use of innovative architectural responses and by presenting visually interesting facades to the street.*
- *In areas close to the Merri, Edgars and Central creeks, to use materials that harmonise with creek settings.*

Comment

- The proposal provides brick, lightweight cladding and perforated metal screening for wall materials. Although contemporary, these are considered to be acceptable as the Design Objective encourages innovative architectural responses and by presenting visually interesting facades to the street. In addition, the materials, fenestration and setbacks provide adequate articulation to the streetscape and other elevations.
- The proposal provides skillion roofs, which complement the contemporary design and minimise the overall height.

Complies**Front Boundary Treatment**Objective

- *To maintain the openness of the streetscape and views to established gardens and dwellings.*

Comment

- There is to be a high front fence of approximately 2 metres in height. Although not generally encouraged, this is an acceptable design response in this instance given the frontage to a Road Zone Category 1 (where there are a number of high fences) and the requirement to provide security to the premises (given the nature of the use).

Complies with objective**Clause 22.12 Environmentally Sustainable Development**

This policy applies throughout the City of Darebin to residential and non-residential development.

The policy contains a number of objectives relating to:

- Energy performance
- Water resources
- Indoor Environment Quality
- Stormwater Management
- Transport
- Waste management
- Urban Ecology

The applicant has provided a Sustainable Management Plan. This may be further addressed and incorporated onto any Planning Permit issues for the proposal by condition.

Zone

The site is located in a General Residential Zone, where the purpose includes:

To implement the Municipal Planning Strategy and the Planning Policy Framework.

To encourage development that respects the neighbourhood character of the area.

To encourage a diversity of housing types and housing growth particularly in locations offering good access to services and transport.

To allow educational, recreational, religious, community and a limited range of other non-residential uses to serve local community needs in appropriate locations.

In discussing zoning controls, it is important to note that although the site is in a residential zone, the zone purpose allows non-residential and community uses that *serve local community needs in appropriate locations*. However, given the zoning and surrounding uses, a balance must be reached, to respect neighbourhood character and maintain an acceptable level of amenity, if residential amenity is to be protected in these areas. Therefore, although an appropriate non-residential use may be accommodated on a site in this area, the operation, noise/amenity, intensity of the use and the car parking are important considerations and must be addressed by conditions where appropriate.

Broadly, it is considered important to provide appropriate non-residential uses in residential areas, to serve the local needs. Indeed, such non-residential uses are often evident in established residential areas and provide a level of amenity, so that the local community are not required to travel significant distances to access services.

In this instance the location of the facility is considered to be appropriate, as it allows access and deployment via Mahoneys Road and nearby streets, as well as the nearby ring road. Overall, it is considered that the use of this site for an emergency services facility (police station) is acceptable in this location, as it caters to the local and wider community needs and allows crucial road access.

Nevertheless, the adjacent area to the east, west and south is residential and therefore amenity should be protected. This can be achieved by a sensitive operation of the premises relating to limitations on emissions, appropriate car parking, waste storage (to be located away from the public realm).

These measures are to ensure that there is an acceptable level of operation that will limit the adverse effects on the amenity of surrounding properties and the area in general.

Clause 32.08-10 provides the decision guidelines that the Responsible Authority must consider in deciding on an application:

- The proposal provides an appropriate emergency/community use in the area. The building form is also considered appropriate and although the use must provide a visible presence that is distinct from the residential area (due to its nature), the building appropriately respects neighbourhood character. It is considered that the proposal complies with the Planning Policy Framework.
- The nature of the use is that it will predominantly serve local community needs as well as a wider need. As noted above, the proposal complies with the zone purpose.
- The proposal is considered to be compatible with residential uses, given a parking and landscape buffer is provided around the building, most activities are internal or concentrated to the street frontage, and the site is located along a major east-west arterial road.
- The proposal is not considered to be of an unreasonable scale or intensity for the area (see elsewhere in this assessment).
- The building is set back from the adjoining dwellings, in accordance with the setbacks under Standard B17 and amenity impacts on adjoining residential uses are appropriate. In addition, the height is also considered to be an appropriate scale for a residential area. The appearance is acceptable in that it provides a contemporary design response, with appropriate articulation that respects the neighbourhood character.
- The site is located in an area with varied landscape character. The ability to landscape to the front, sides and rear is appropriate in the context of the use and development in the area.
- The provision of car parking is appropriate (see assessment below).
- Waste is stored to the rear and is to be managed in accordance with a Waste Management Plan (subject to a condition on any permit issued for the proposal).
- The traffic generated by the proposal will not be excessive (given the frontage to a wide Road Zone Category 1), nor will it affect safety, efficiency and amenity.

Given the above, it is considered that the proposal appropriately addresses the context of the site and area and maintains the existing low scale building form of the area and is set back from the adjoining sensitive boundaries.

Buildings and Works

As the proposed use is a section 2 use in the General Residential Zone, a permit is required for buildings and works pursuant to Clause 32.08-9. In looking at the proposed building, it is noteworthy that, although Clause 54/55 does not apply, some elements may be applicable in assessing the appropriate siting and impact on the amenity of adjoining allotments. The assessment is summarised as follows:

- It is noteworthy that the building does not comply with the front setback that would be required under Standard B6 or A3 of Clause 54/55, as it sits in front of the adjoining dwellings (which are both set back approximately 8 metres), in that a setback of 8 metres is required under the standard, whereas a setback of 5.85 metres (ground level) and 5.2 metres (first floor) and approximately 4.8 metres (perforated screen). Nevertheless, this is considered to be acceptable, as this setback allows adequate landscaping to the front and provides ample articulation without unreasonable visual impact to the streetscape. In addition, the neighbourhood character study indicates that '*Buildings are set back 5 – 7 metres*'. The proposal is appropriately within this range. The proposed setbacks also allows efficient use of the site.

- The height of a residential building under Clause 32.08-10 is 11 metres or three (3) storeys. Although the proposal is not a residential building, it has a maximum height of 10.3 metres.
- Acceptable opportunities are available for landscaping around the site.
- There are two (2) crossovers to the street and car parking areas do not dominate the streetscape, given the wide frontage.
- The building complies with the setbacks required under Standard B17, which is an appropriate design response.
- The setbacks are adequate so that the building does not impact the daylight to adjoining habitable room windows.
- Overshadowing of adjoining open space meets the relevant standard, in that the shadows to neighbouring properties to the south and west are minimal, with at least 40 square metres of neighbouring dwellings' secluded private open space with a minimum dimension of 3.0 metres, or 75% (whichever is the lesser) receiving a minimum of five (5) hours sunlight between 9am and 3pm on 22 September.
- Appropriate fences must be required to obscure overlooking at ground level and provided appropriate acoustic dampening from the car parking area. Setbacks of greater than 9 metres to the south and east will ensure there will be no unreasonable overlooking from the upper floor levels to these directions. To the west the first floor levels will have obscure glass. However, these must be confirmed to be fixed to 1700mm (or the perforated screen over the northernmost window to have maximum 25% perforations).

Although the requirements of Clause 54/55 have limited applicability, the proposal has a high level of compliance, so that the proposed building is appropriately sited and will not unreasonably affect the amenity of adjoining dwellings.

Amenity

The main amenity consideration relating to the proposal is noise (aside from buildings and works addressed above). The proposal includes an external parking, plant, waste storage and barbeque area that may impact on the amenity of adjoining dwellings. The applicant has provided an acoustic report to provide advice to achieve SEPP N-1 requirements for plant.

The report recommends:

- A screen around the upper floor plant area
- A 2.4 metres acoustic screen around the generator and the suppliers recommended modifications to the generator.

The report states that acoustic fencing around the perimeter is not required to comply with SEPP N-1 requirements for plant noise. Notwithstanding the above, it is considered that a 2 metre high acoustic fence to the east, west and southern common boundaries is appropriate, given the potential noise impact from vehicles, conversations in the car park and barbeque area, loading of waste, car doors slamming etc. The construction of the fence may be subject to an appropriate condition.

Car Parking:

Pursuant to Clause 52.06-3 a permit is required to reduce the requirement to provide the number of car parking spaces required under Table 1 of this Clause. In looking at the parking requirement for the proposal, it is noteworthy that the use is not listed in the table to this Clause. Consequently, the car parking component for the emergency services facility (police station) does not require a permit pursuant to Section 47 of the Act (as distinct from Council satisfaction pursuant to Section 6(2)(h) of the Act). However, Clause 52.06-6 notes that:

Where a use of land is not specified in Table 1 (of Clause 52.06-5) or where a car parking requirement is not specified for the use in another provision of the planning scheme or in a schedule to the Parking Overlay, before a new use commences or the floor area or site area of an existing use is increased, car parking spaces must be provided to the satisfaction of the responsible authority.

The subject site has access to 47 car spaces. In deciding whether parking has been provided to the satisfaction of the Responsible Authority, the decision guidelines under Clause 52.06-7 are considered to broadly provide appropriate assessment guidelines. Therefore, the following should be taken into account in a discussion about the provision of car parking for the proposal:

- It is expected that there will be up to a total of 93 staff, from the Uniform Branch, administration and the Family Violence unit. Staff from the Uniform Branch are proposed to operate over three (3) shifts (7am to 3pm, 3pm to 11pm and 11pm to 7am), while staff of the Family Violence unit generally operate over two shifts (7am to 3pm, 3pm to 11pm). It is understood that the shift times for sworn Police staff will be staggered and shift periods are indicative and will vary. Additionally, administrative staff will generally work at various periods between 7am and 7pm.
- There are to be a total of 47 car spaces, with 5 visitor spaces to the front and 42 parking spaces provided within a secure 'staff only' parking area to the rear of the building. In looking at the parking allocation, of the 42 parking spaces at the rear, two (2) car spaces will be set aside for the storage of stolen vehicles and ten (10) spaces will be for operational vehicles, with the remaining 30 spaces allocated to staff.
- The applicant has provided a Car Parking demand Assessment, that indicates:
 - Census data indicates that 79% of employees in Darebin drive to work.
 - It is expected that there is to be a peak parking demand of 30 car spaces for staff.
 - Visitor parking demand is generally low and it is anticipated that this will be accommodated in the five (5) spaces set aside for visitors at the front.
 - Based on an analysis, it is anticipated that there will be a demand for 46 car spaces (including staff, four (4) visitors, ten (10) operational vehicles, two (2) car spaces set aside for stolen vehicles with the remaining spaces set aside for staff).
 - Traffic generation will not have a discernible impact to the operation of Mahoneys Road and the nearby intersections.
 - The 902 SmartBus service (Chelsea to Airport West) runs along Mahoneys Road with a bus stop located in front of the subject site (to be relocated). Keon Park Railway Station on the Mernda Railway Line is located 1.2 km's east of the subject site.
- Internal referral comments from Council's Transport Engineering and Strategy Unit have raised no objections.

Given the above, it is considered that the level of car parking provision for the proposed use is satisfactory.

It is considered that the traffic impacts on the surrounding street network will be insignificant.

(a) Design Standards for Car parking

The car parking spaces and the accessways have appropriate dimension to enable efficient use and management.

The car parking facilities are designed, surfaced and graded to reduce run-off and allow stormwater to drain into the site.

The car spaces have dimensions of 4.9 metres length x 2.6 metres width to comply with the minimum requirements of the standard.

Swept paths indicate that car and truck access is acceptable.

The width of the accessway is 3.0 metres.

Access dimensions to the car spaces comply with the standard.

Adequate turning areas are provided to allow vehicles to enter and exit the site in a forward direction.

A passing area is provided at the entrance of at least 6.1 metres wide and 7 metres long.

Appropriate visibility splays are provided at the primary access point.

The secondary access point to the west achieves a splay of only 1.2 metres, due to the adjacent property boundary and fence. This is considered to be acceptable in this instance, as it is not the primary access, vehicles will have to slow to allow the gate to open, and it is similar to existing access.

Additionally, it is proposed to provide a break in the Mahoneys Road median strip opposite the primary access point, to provide emergency egress for operational vehicles to the east. This is a matter for the relevant roads authority, who have not objected to the proposal.

Loading and Unloading

Clause 65.01 requires the Responsible Authority to consider (among other things) the adequacy of loading and unloading facilities and any associated amenity, traffic flow and road safety impacts.

It is considered that there is adequate area for loading on the site (to the front, side and rear), as well as appropriate space for vehicles to turn and exit the site in a forward direction. It is therefore considered that there is adequate provision for loading and unloading vehicles.

In addition it is noted that neither VicRoads or Council's Transport Engineering and Strategy unit have objected to the proposal (subject to conditions included in the recommendation above).

Clause 52.29

Clause 52.29 requires a permit to create or alter access to a road in a Road Zone, Category 1. Pursuant to this Clause and also Clause 66, an application to create or alter access to a road declared as a freeway or arterial road under the Road Management Act 2004, must be referred to the Roads Corporation (i.e. VicRoads) under Section 55 of the *Planning and Environment Act 1987*.

The proposal is to remove the access to the Road Zone and create a new access point, which is considered to be appropriate and will not have an adverse effect on the operation of the road and on public safety. VicRoads has not objected subject to conditions (included in the recommendation above).

REFERRAL SUMMARY

Department/Authority	Response
Capital Works	No objection, subject to condition included in recommendation.
Transport Management and Planning	<p>No objection.</p> <p>Vehicle manoeuvrability is acceptable and the number and location/width of crossovers are acceptable. Parking provision meets the requirements of Clause 52.06. Accessways are at least 3 metres wide and appropriate headroom is provided. Pedestrian visibility splays are provided and the accessway is designed so that cars can exit the site in a forward direction. Car parking spaces and accessways have the minimum dimensions in Table 2 of the Clause and appropriate clearances. Accessway gradients are in accordance with the Design Standard. Based on an assessment of the documentation provided (and listed above), no objection is offered to this application on transport/traffic engineering grounds.</p>
Public Places	No objection, subject to condition included in recommendation, relating to: landscape plan detail; underground rainwater tank; barbeque facility table and seating area; fence heights to be shown; additional canopy trees; planting height along eastern boundary.
Transport for Victoria	No objection, subject to conditions relating to relocation and reconstruction of the existing bus stop adjacent to the subject site.
VicRoads	No objection, subject to condition included in recommendation

PLANNING SCHEME SUMMARY

Darebin Planning Scheme clauses under which a permit is required

- Clause 32.08-2 requires a permit for an emergency services facility.
- Clause 32.08-9 requires a permit to construct a building or construct or carry out works for a use in Section 2 of Clause 32.08-2.
- Clause 52.29 (Land Adjacent to a Road Zone, Category 1, or a Public Acquisition Overlay for a Category 1 Road) requires a permit to create or alter access to a road in a Road Zone, Category 1.

Applicable provisions of the Darebin Planning Scheme

Section of Scheme	Relevant Clauses
SPPF	11.01-1S, 11.02-1S, 15.01-1S, 15.01-1R, 15.01-2S, 15.01-5S, 15.02-1S, 18, 18.01-1S, 18.02-1S, 18.02-2S, 18.02-4S, 19.02-5S, 19.03
LPPF	21.01-6, 21.02-3, 21.03-2, 21.05, 21.05-1, 21.05-3, 22.02, 22.12
Zone	32.08
Overlay	45.06
Particular provisions	52.06, 52.29
General provisions	65.01
Neighbourhood Character Precinct	G4

POLICY IMPLICATIONS

Environmental Sustainability

A Sustainable Design Assessment (SDA) has been required as a condition of approval. The SDA will outline sustainable design initiatives required to be incorporated into the development.

Social Inclusion and Diversity

Nil

Other

Nil

FINANCIAL AND RESOURCE IMPLICATIONS

There are no financial or resource implications as a result of the determination of this application.

FUTURE ACTIONS

Nil

RELATED DOCUMENTS

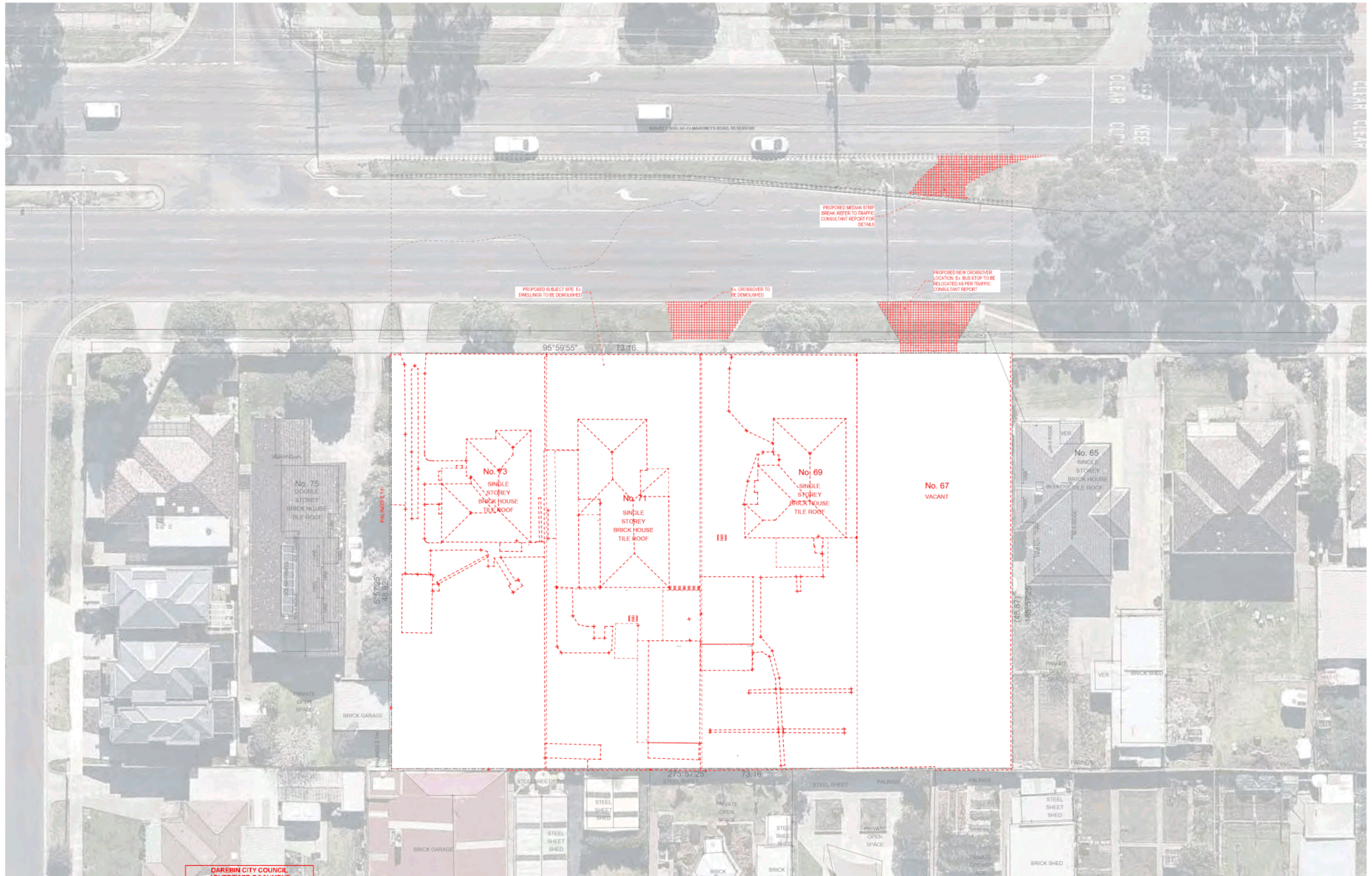
Attachments

- Advertised Plans (**Appendix A**)
- Aerial Map (**Appendix B**)

DISCLOSURE OF INTEREST

Section 80C of the *Local Government Act 1989* requires members of Council staff and persons engaged under contract to provide advice to Council to disclose any direct or indirect interest in a matter to which the advice relates.

The Officer reviewing this report, having made enquiries with relevant members of staff, reports that no disclosable interests have been raised in relation to this report.



**DAREBIN CITY COUNCIL
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NOTES
LEGEND:
- - - ITEMS TO BE REMOVED
- - - ITEMS TO BE RETAINED

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A	26/11/18	KG	DC	TOWN PLANNING APP

TOWN PLANNING APP.
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REV	DATE	BY	CH	DESCRIPTION

PROJECT ADDRESS
67-73 Mahoneys Road, Reservoir, VIC, 3073

FOR
Victoria Police

CAD FILE 1820 Master DD

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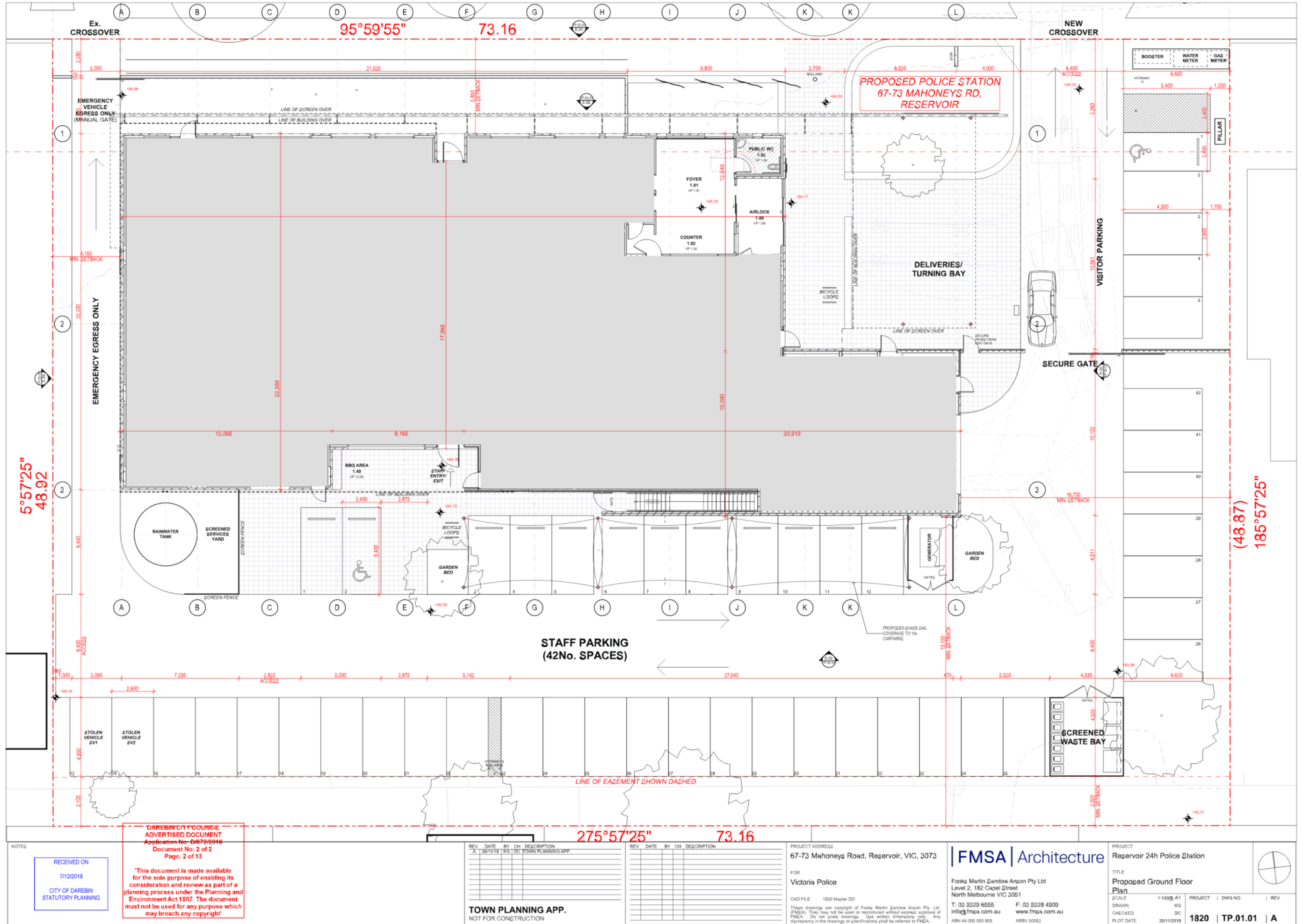
ABN 44 005 000 859 ABRV 50053

PROJECT
Reservoir 24h Police Station

TITLE
Existing/ Demolition Site Plan

SCALE 1:200@A1
DRAWN KG
CHECKED DC
PLOT DATE 29/11/2018

PROJECT | DWG NO. | REV
1820 | TP.00.02 | A



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PROJECT ADDRESS
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FOR
 Victoria Police

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 www.fmsa.com.au
 ARBV 50052

PROJECT
 Reservoir 24h Police Station

TITLE
 Proposed Ground Floor Plan

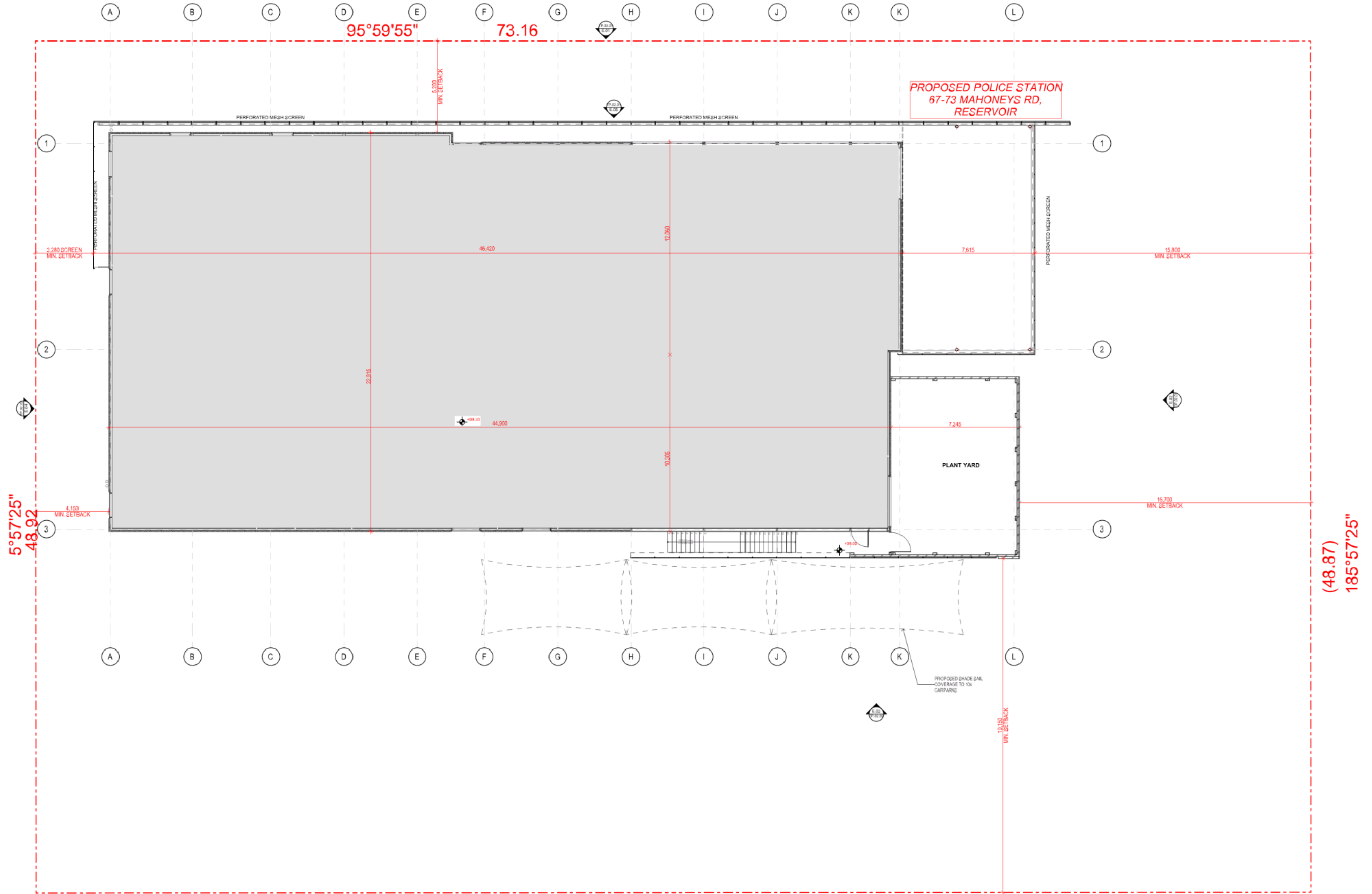
SCALE
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DRAWN
 KG

CHECKED
 DC

PLOT DATE
 20/11/2018

PROJECT | DWG NO. | REV
 1820 | TP.01.01 | A



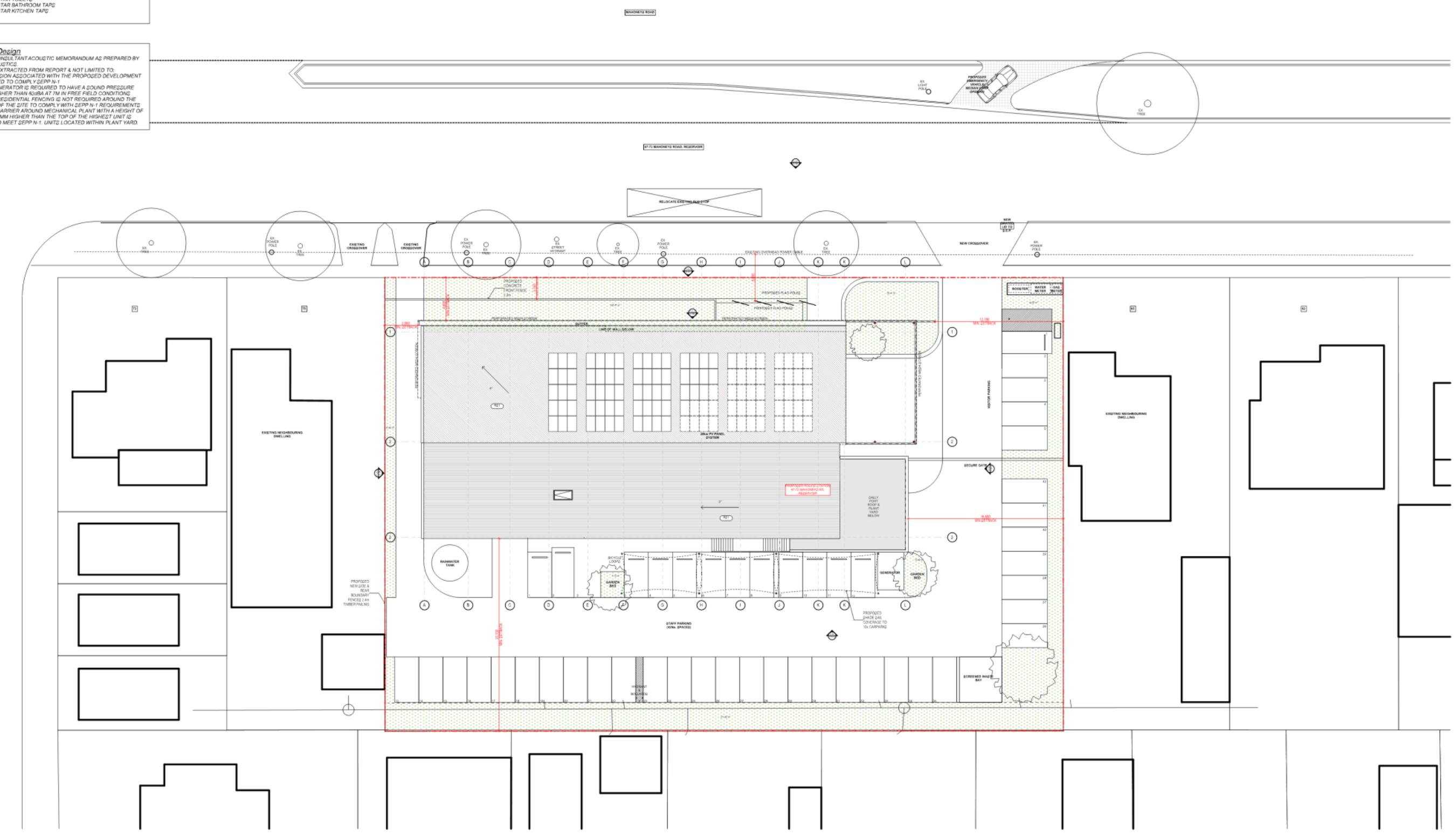
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		TOWN PLANNING APP. NOT FOR CONSTRUCTION	PROJECT ADDRESS 67-73 Mahoneys Road, Reservoir, VIC, 3073	FOR Victoria Police		TITLE Proposed First Floor Plan SCALE 1:100 @ A1 DRAWN KG CHECKED DC PLOT DATE 20/11/2018	

Environmental Sustainable Design
 REFER TO CONSULTANT SUSTAINABILITY DESIGN ASSESSMENT AS PREPARED BY LOW IMPACT DESIGN CONSULTING FOR FULL REPORT. KEY NOTES EXTRACTED FROM REPORT & NOT LIMITED TO:
 - RENEWABLE ENERGY PHOTOVOLTAIC (PV) PANELS OF 27KW SUPPLY TO AT LEAST 20% OF THE ESTIMATED BUILDING CONSUMPTION
 - RAINWATER TANK OF 20,000L, CONNECTED TO ALL TOILETS TO REDUCE POTABLE WATER CONSUMPTION
 - HOT WATER SUPPLY TO BE PROVIDED BY EITHER GAS INSTANTANEOUS UNITS (5 STAR ENERGY EFFICIENCY) OR ENERGY EFFICIENT HEAT PUMPS/CLASSIC HOT WATER UNITS
 - LIGHTING DENSITY WILL BE REDUCED TO AT LEAST 20% BELOW THE MAXIMUM ALLOWED BY THE BCA 2014 EQ 4.9.10.1 RATHER THAN SWIM
 - WATER EFFICIENT FIXTURES, FITTINGS & APPLIANCES TO BE INSTALLED.
 - 3 STAR SHOWERS
 - 4 STAR TOILETS
 - 5 STAR BATHROOM TAPS
 - 5 STAR KITCHEN TAPS

Acoustic Design
 REFER TO CONSULTANT ACOUSTIC MEMORANDUM AS PREPARED BY OCTAVE ACOUSTICS. KEY NOTES EXTRACTED FROM REPORT & NOT LIMITED TO:
 - NOISE EMISSION ASSOCIATED WITH THE PROPOSED DEVELOPMENT ARE REQUIRED TO COMPLY SEPP N-1
 - ON SITE GENERATOR IS REQUIRED TO HAVE A SOUND PRESSURE LEVEL NO HIGHER THAN 68dBA AT 7M IN FREE FIELD CONDITIONS
 - ACOUSTIC RESIDENTIAL FENCING IS NOT REQUIRED AROUND THE PERIMETER OF THE SITE TO COMPLY WITH SEPP N-1 REQUIREMENTS
 - ACOUSTIC BARRIER AROUND MECHANICAL PLANT WITH A HEIGHT OF AT LEAST 300MM HIGHER THAN THE TOP OF THE HIGHEST UNIT IS REQUIRED TO MEET SEPP N-1 UNITS LOCATED WITHIN PLANT YARD

Site Coverage Key:

SITE AREA:	3,877m ²
PROPOSED SITE COVERAGE:	1,160m ² (30.4%)
PERMEABLE AREA:	738m ² (19.0%)



NOTES

PERMEABLE AREA (APPROX. 25% OF SITE AREA)

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PROJECT ADDRESS
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FOR
 Victoria Police

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ABN 44 005 030 809 ARBV 50052

PROJECT	Reservoir 24h Police Station
TITLE	Proposed Roof & Site Plan
SCALE	1:200 @ A1
DRAWN	KG
CHECKED	DC
PLOT DATE	20/11/2018
PROJECT	1820 TP.00.03 A



Legend

	Tree planting		'River Rocks' - 20-100mm to form 'Dry Creek Bed' to take overland flow.
	Shrub planting within wood mulched garden bed area.		'Flat top mudstone' or similar, approx. size 500 x 800 within 'Dry Creek Bed'
	Existing trees to be retained.		Large barrier 'Flat top mudstone' or similar, approx. size 800 x 1200.
	Groundcover planting within wood mulched garden bed area.		600mm high concrete seating element (refer Architect's drawings)
	Groundcover planting within 'River Pebbles' - 14mm' garden bed area.		900mm high concrete wall (refer Architect's drawings)
	Compacted 14mm 'Tuscan Toppings'.		'Stone' Concrete pavers. Nom. size 1000 x 500, colour 'Manhattan' or similar. (refer Architect's drawings)

Plant Schedule

KEY	BOTANICAL NAME (COMMON NAME)	MATURE SIZE HEIGHTxWIDTH	PURCHASE SIZE
Trees			
AF	<i>Agonis flexuosa</i> 'Burgundy'	5.0 x 3.0	45 Litre
EL	<i>Eucalyptus leucocylon</i> 'Euky Daxal'	6.0 x 5.0	45 Litre
LI	<i>Lagerstroemia indica</i> x <i>fauriei</i> 'Biloxi'	8.0 x 5.0	45 Litre
Shrubs			
Cve	<i>Callistemon viminalis</i> 'Endeavour'	1.8 x 1.8	300mm
Cvs	<i>Callistemon viminalis</i> 'Slim'	1.3 x 3.0	300mm
Wf	<i>Westringia fruticosa</i> 'Wynyabie Gem'	1.8 x 1.5	300mm
Groundcovers			
As	<i>Adenanthos sericeus</i> 'Silver Lining'	0.4 x 1.5	150mm
Ac	<i>Arthropodium cirratum</i> (Renga Renga Lily)	0.8 x 0.8	150mm
Ct	<i>Carex testacea</i> (Orange sedge)	0.5 x 0.5	150mm
Ca	<i>Chrysocephalum apiculatum</i> (Common Everlasting)	0.3 x 0.5	150mm
Cg	<i>Conostylis candidans</i> (Grey Cottonheads)	0.4 x 0.4	150mm
Bm	<i>Brachyscome multifida</i> (Cut Leaf Daisy)	0.3 x 0.3	150mm
Dj	<i>Dianella caerulea</i> 'Little Jess'	0.5 x 0.5	150mm
Dr	<i>Dichondra repens</i> (Kidney Weed)	0.2 x 1.0	150mm
Dg	<i>Diets grandiflora</i> 'Grand Star'	0.7 x 0.7	150mm
DrI	<i>Diets robinsoniana</i> (Lord Howe Wedding Lily)	1.0 x 1.0	150mm
Fn	<i>Ficinia nodosa</i> (Knobby Club-rush)	0.7 x 0.7	150mm
Lt	<i>Lomandra longifolia</i> 'Tanika'	0.6 x 0.6	150mm
Mp	<i>Myoporum parvifolium</i> (Creeping Boobialla)	0.1 x 1.5	150mm
Pt	<i>Phormium tenax</i> 'Flamin'	0.9 x 0.9	150mm
Pp	<i>Poa poliformis</i> 'Kingsdale'	0.4 x 0.4	150mm

* Plant found in the 'Guide to The Indigenous Plants of the City of Darebin'

No.	Issue	Date
01	Planning Issue	23.11.2018

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Reservoir Police Station
67-73 Mahoneys Road, Reservoir VIC 3073

Landscape Plan

MEXTED RIMMER
INDUSTRIAL ARCHITECTS

MEXTED RIMMER ASSOCIATES PTY LTD
A B N O E I O O S E O O S P
14 MC LARRY PLACE GEEELONG VIC 3290
TEL: 03 5229 1414 FAX: 03 5223 2530
EMAIL: mail@mextedrimmer.com

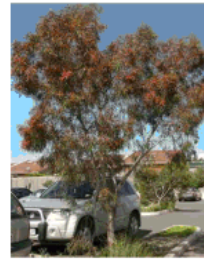
project no: 3163
drawing no: LP1.P1
sheet no: 1 of 2
designed by: GM
drawn by: jd
date: 23.11.2018
scale: 1:125 @ A1
1:250 @ A3



Trees



Agonis flexuosa 'Burgundy'



Eucalyptus leucoxydon 'Eukey Dwarf'



Lagerstroemia indica x lauriei 'Biloxi'

Shrubs



Callistemon viminalis 'Endeavour'



Callistemon viminalis 'Slim'



Westringia fruticosa 'Wynabbie Gem'

Groundcovers



Adenanthos sericeus 'Silver Lining'



Arthropodium cirratum (Renga Renga Lily)



Carex testacea (Orange sedge)



Chrysocephalum apiculatum (Common Everlasting)



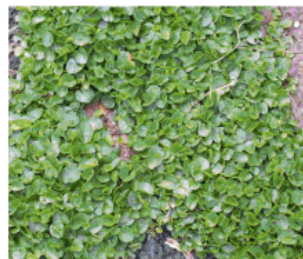
Conostylis candidans (Grey Cottonheads)



Brachyscome multifida (Cut Leaf Daisy)



Dianella caerulea 'Little Jess'



Dichondra repens (Kidney Weed)



Diets grandiflora 'Grand Star'



Diets robinsoniana (Lord Howe Wedding Lily)



Ficinia nodosa (Knobby Club-rush)



Lomandra longifolia 'Tanika'



Myoporum parvifolium (Creeping Boobialla)



Phormium tenax 'Flamin'



Poa poiformis 'Kingsdale'

Hardscape



Dry creek bed with tussock planting



'Flat top mudstone'



'River Rocks-20-100mm'



'River Pebbles - 14mm'



'Tuscan Toppings'



Wood mulch

No.	Issue	Date
01	Planning Issue	23.11.2018

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Reservoir Police Station
67-73 Mahoneys Road, Reservoir VIC 3073

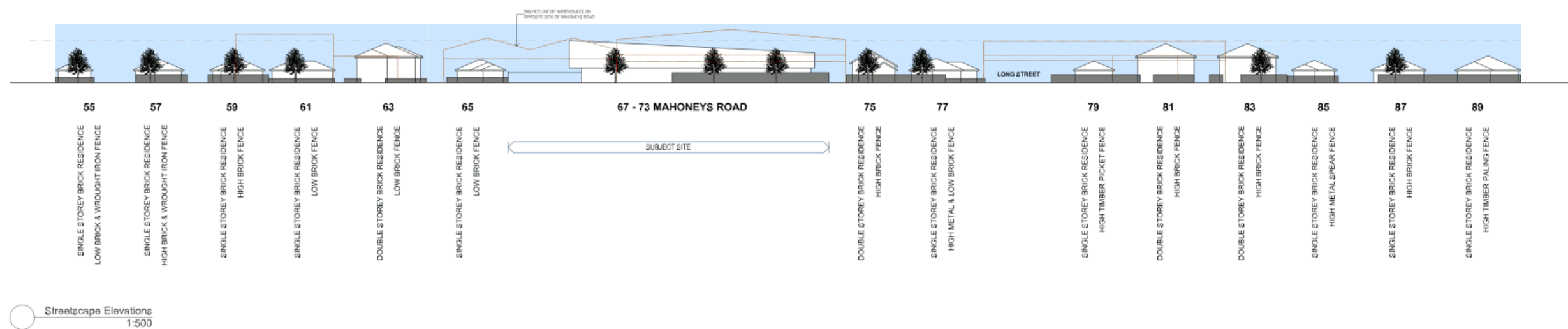
Landscape Images



MEXTED RIMMER ASSOCIATES PTY LTD
A B N O S E O S O S
14 MC LARRY PLACE, GEEELONG VIC 3209
TEL. 03 5229 1414 FAX. 03 5229 2530
EMAIL: mail@mextedrimmer.com

project no: 3163
drawing no: LP2.P1
sheet no: 2 of 2
designed by: GM
drawn by: jd
date: 23.11.2018





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				SCALE 1:500 A1 DRAWN KG CHECKED DC PLOT DATE 29/11/2018		PROJECT DWG NO. REV 1820 TP.04.01 A



PEDESTRIAN ENTRY & APPROACH



PUBLIC FORECOURT



STREET PASSING VIEW & CONTEXT

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FMSA | Architecture

TP.04 TP.04.02 - Artists Impressions 1

Reservoir 24h Police Station

Project Address | 67-73 Mahoneys Road, Reservoir, VIC, 3073 | Not to Scale



STREET INTERFACE



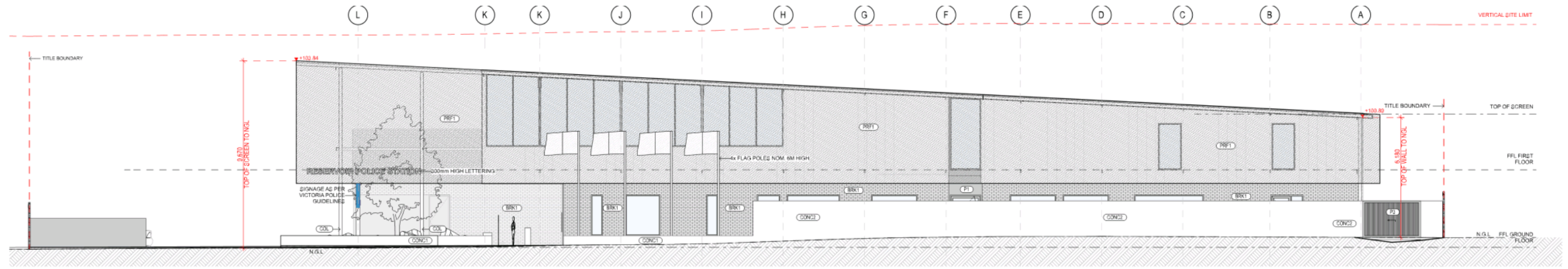
ROAD APPROACH & ENTRY FORECOURT



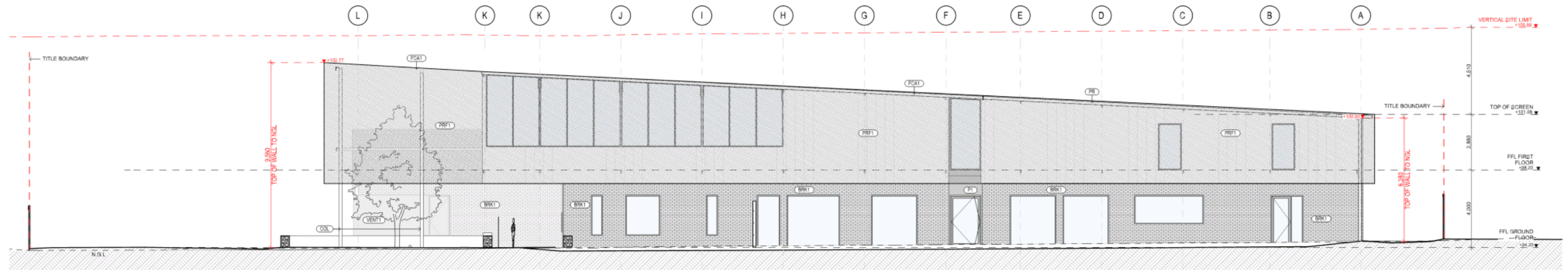
STAFF ENTRY & CARPARK



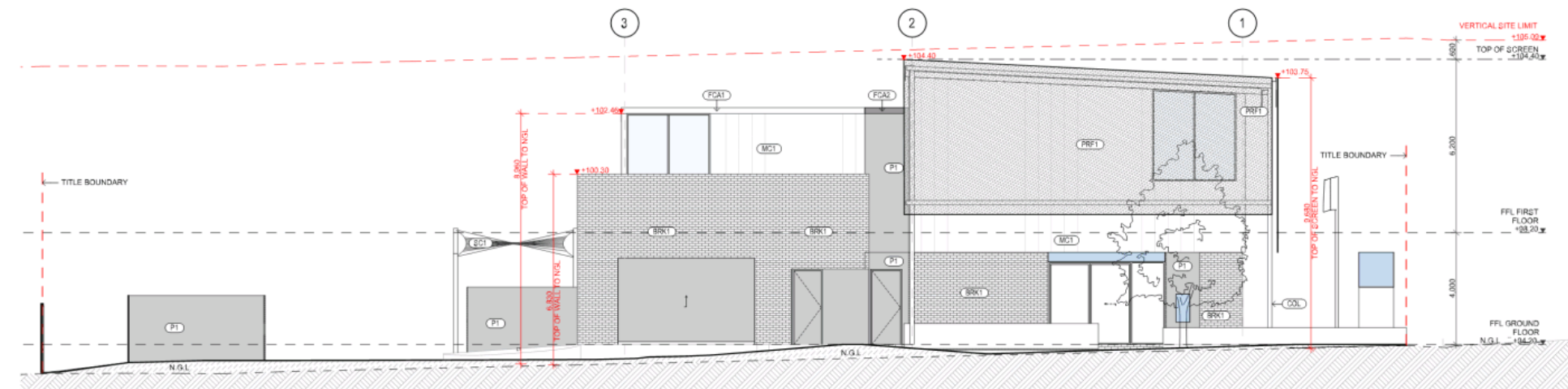
SIDE ACCESS & COVERED CARPARKING



1 NORTH ELEVATION
1:100



2 NORTH ELEVATION (NO FENCE)
1:100



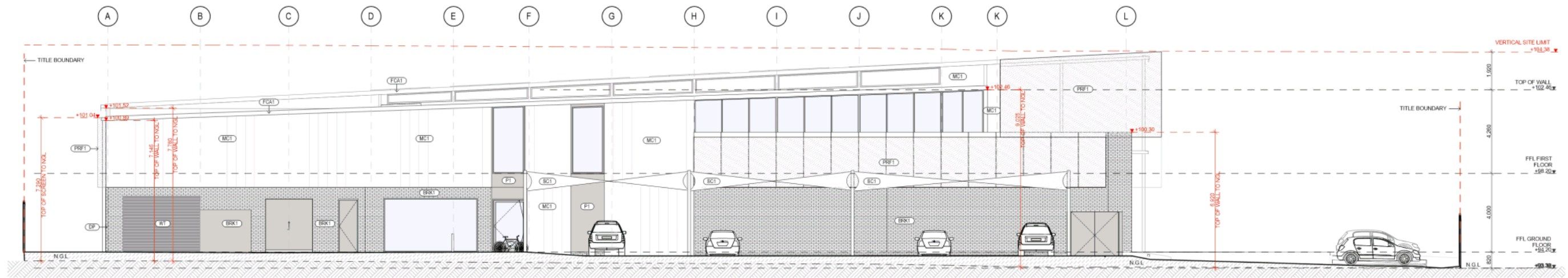
3 EAST ELEVATION
1:100

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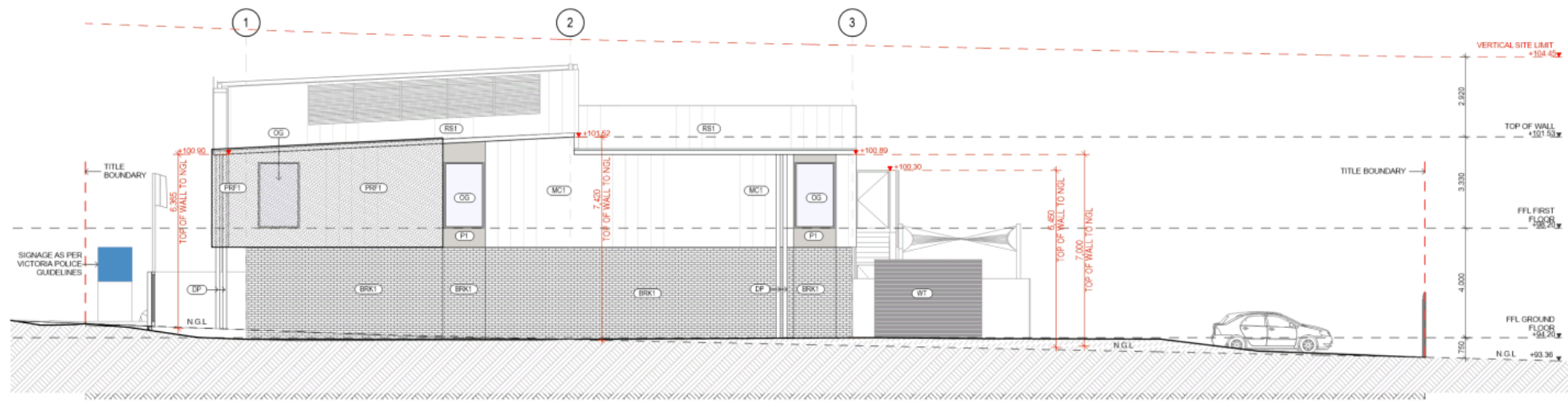
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			FOR		TITLE
			Victoria Police		Elevations 1
			CAD FILE 1820 Meller DD	Fooks Martin Sandow Anson Pty Ltd Level 2, 182 Chapel Street North Melbourne VIC 3051	SCALE 1:100 A1
			These drawings are copyright of Fooks Martin Sandow Anson Pty Ltd (FMSA). They may not be used or reproduced without express approval of FMSA. Do not scale drawings. Use written dimensions only. Any discrepancy in the drawings or specifications shall be referred to FMSA.	T: 03 9320 6555 info@fmsa.com.au ABN 44 005 030 809	F: 03 9328 4900 www.fmsa.com.au ARBY 50052
			TOWN PLANNING APP. NOT FOR CONSTRUCTION		PROJECT DWG NO. REV
					1820 TP.02.01 A

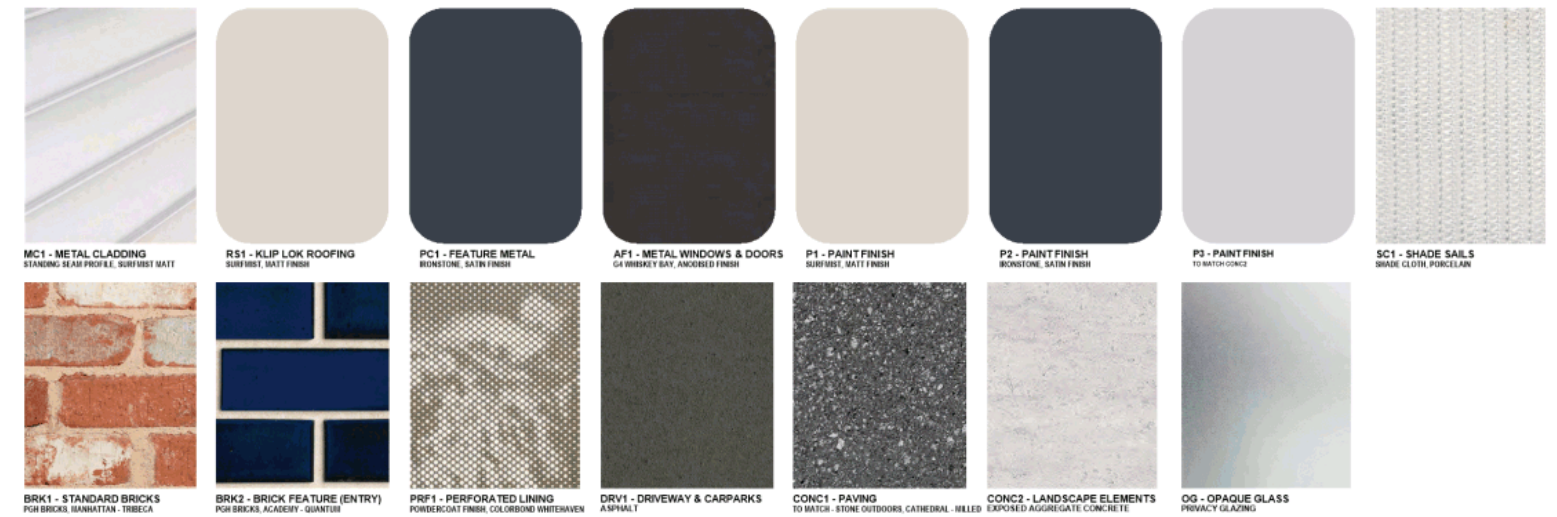


E.03 SOUTH ELEVATION
1:100



E.04 WEST ELEVATION
1:100

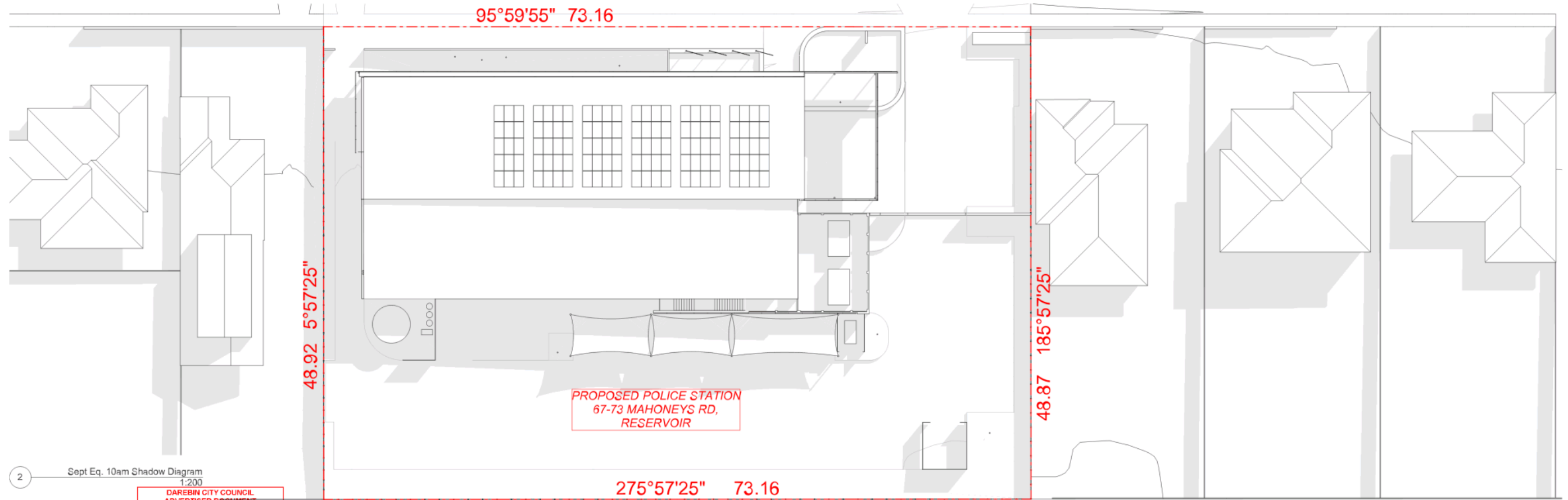
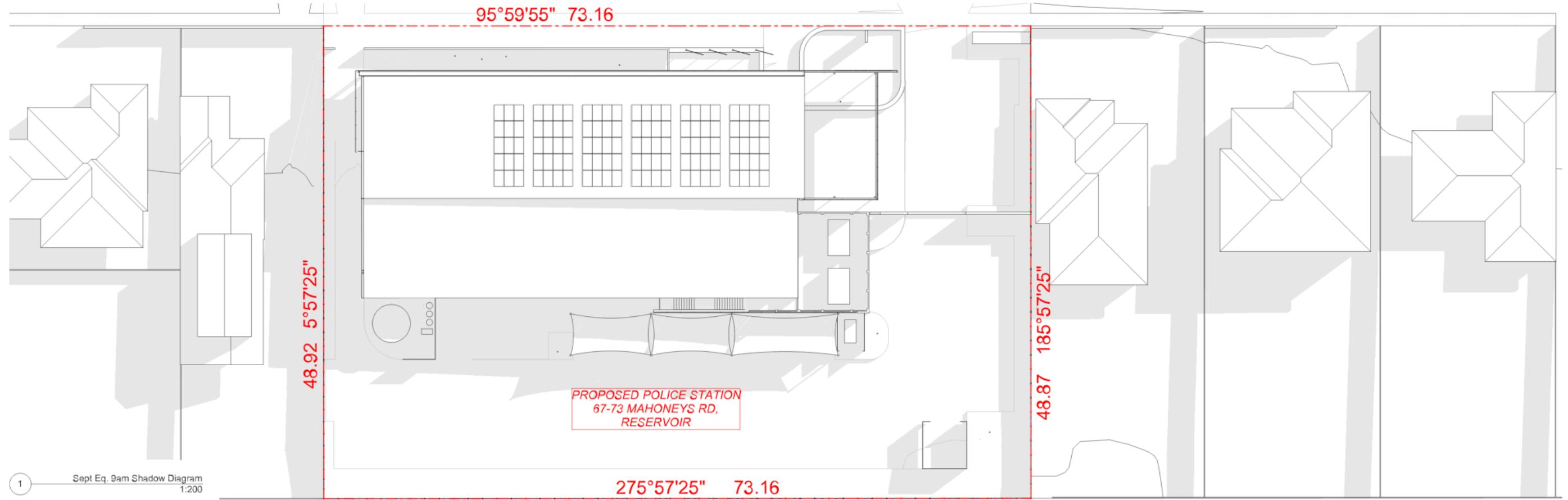
PROPOSED FINISHES SCHEDULE:



PROPOSED EXTERNAL FINISHES PRECEDENT IMAGE:



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	TOWN PLANNING APP. NOT FOR CONSTRUCTION		PROJECT ADDRESS 67-73 Mahoneys Road, Reservoir, VIC, 3073 FOR Victoria Police CAD FILE 1820 Master DD These drawings are copyright of Fooks Martin Sandow Anson Pty Ltd (FMSA). They may not be used or reproduced without express approval of FMSA. Do not scale drawings. Use written dimensions only. Any discrepancy in the drawings or specifications shall be referred to FMSA.		FMSA Architecture Fooks Martin Sandow Anson Pty Ltd Level 2, 182 Chapel Street North Melbourne VIC 3051 T: 03 9329 6555 F: 03 9328 4909 info@fmsa.com.au www.fmsa.com.au ABN 44 005 000 859 ARBV 50053	PROJECT Reservoir 24h Police Station TITLE Elevations 2 SCALE 1:100@A1 DRAWN KG CHECKED DC PLOT DATE 29/11/2018



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A	26/11/18	KIS	DC	TOWN PLANNING APP.

TOWN PLANNING APP.
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REV	DATE	BY	CH	DESCRIPTION

PROJECT ADDRESS
67-73 Mahoneys Road, Reservoir, VIC, 3073

FOR
Victoria Police

CAD FILE 1820 Meilir DD

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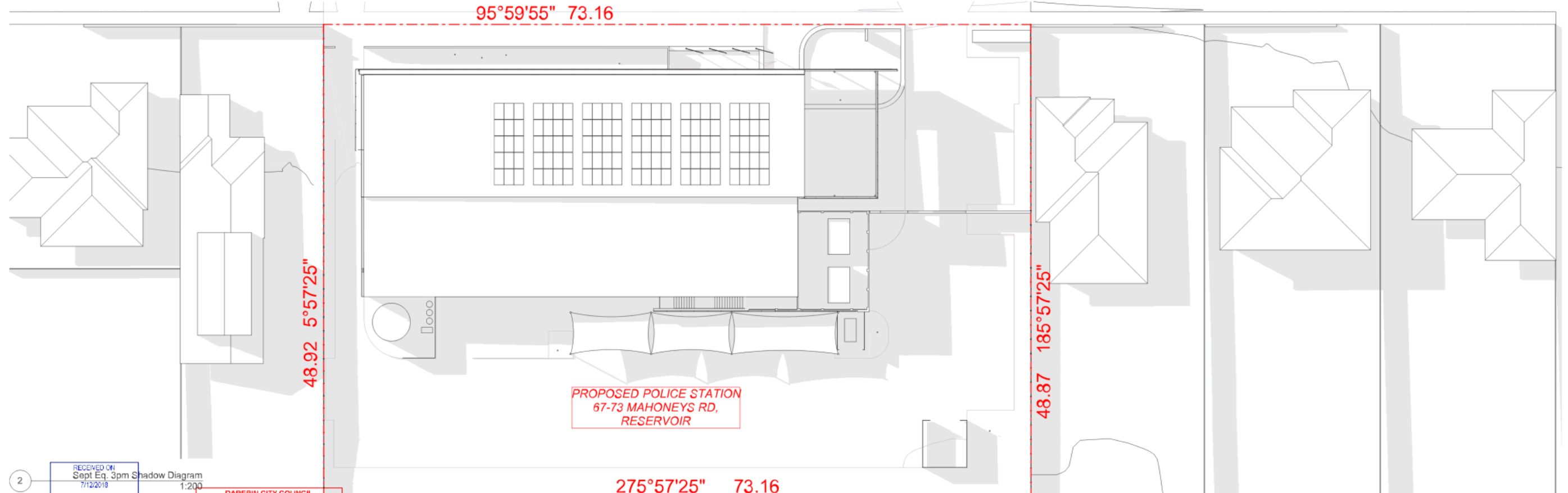
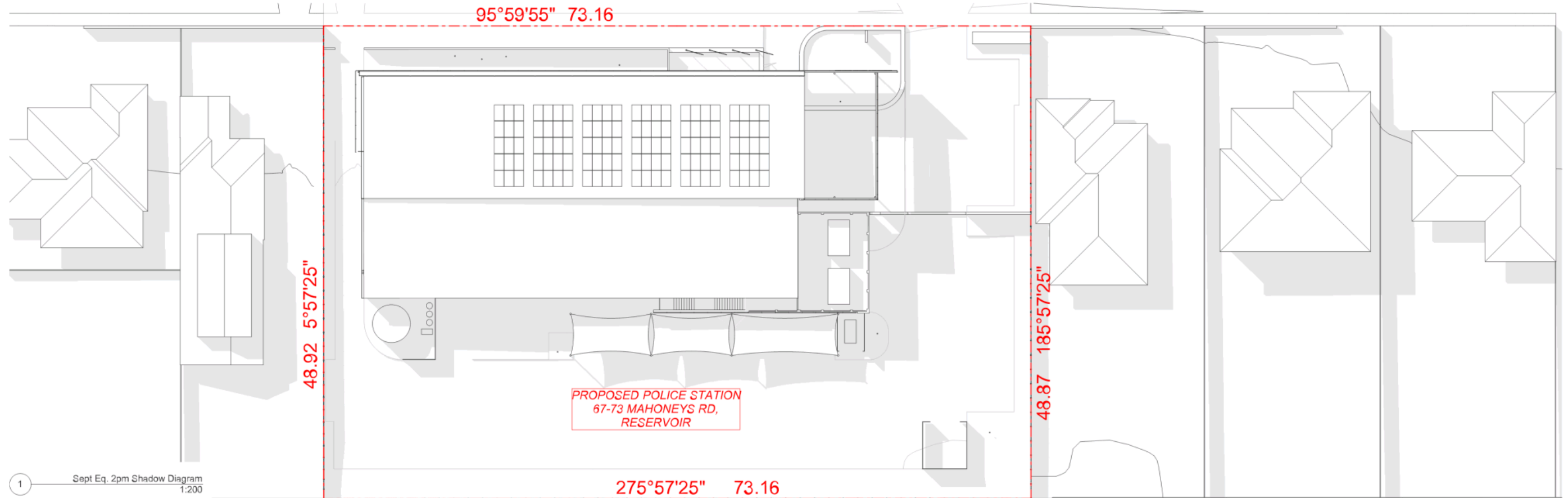
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ABN 44 005 030 809 ARBV 50052

PROJECT	Reservoir 24h Police Station		
TITLE	Shadow Diagrams 1		
SCALE	1:200@ A1	PROJECT DWG NO.	REV
DRAWN	KG	1820 TP.03.01	A
CHECKED	DC		
PLOT DATE	29/11/2018		



2 RECEIVED ON
Sept Eq. 3pm Shadow Diagram
7/12/2018
1:200

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PROJECT ADDRESS
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PROJECT	Reservoir 24h Police Station
TITLE	Shadow Diagrams 2
SCALE	1:200 @ A1
DRAWN	KIS
CHECKED	DC
PLOT DATE	20/11/2018
PROJECT DWG NO.	1820 TP.03.02
REV	A

Darebin City Council



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6. OTHER BUSINESS

Nil

7. URGENT BUSINESS

8. CONSIDERATION OF REPORTS CONSIDERED CONFIDENTIAL

9. CLOSE OF MEETING