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## AGENDA

Planning Committee Meeting to be held at  
Preston Town Hall,  
284 Gower Street, Preston  
on Monday 12 July 2021 at 6.30pm.

**This meeting will be open to the public in line with current health restrictions. Masks are mandatory and the number of people in the public gallery will be restricted.**

The meeting will be available for the public to watch through  
livestreaming via Councils website [www.darebin.vic.gov.au](http://www.darebin.vic.gov.au)





## **ACKNOWLEDGEMENT OF TRADITIONAL OWNERS AND ABORIGINAL AND TORRES STRAIT ISLANDER COMMUNITIES IN DAREBIN**

Darebin City Council acknowledges the Wurundjeri Woi-Wurrung people as the Traditional Owners and custodians of the land we now call Darebin and pays respect to their Elders, past, present and emerging.

Council pays respect to all other Aboriginal and Torres Strait Islander communities in Darebin.

Council recognises, and pays tribute to, the diverse culture, resilience and heritage of Aboriginal and Torres Strait Islander people.

We acknowledge the leadership of Aboriginal and Torres Strait Islander communities and the right to self-determination in the spirit of mutual understanding and respect.



### English

This is the Agenda for the Council Meeting. For assistance with any of the agenda items, please telephone 8470 8888.

### Arabic

هذا هو جدول الأعمال لاجتماع المجلس المحلي للمجلس اعرفي أي من نود جدول الأعمال يرجى الاتصال بالهاتف 8470 8888.

### Chinese

这是市议会会议议程。如需协助了解任何议项，请致电8470 8888。

### Greek

Αυτή είναι η Ημερήσια Διάταξη για τη συνεδρίαση του Δημοτικού Συμβουλίου. Για βοήθεια με οποιαδήποτε θέματα της ημερήσιας διάταξης, παρακαλείστε να καλέσετε το 8470 8888.

### Hindi

यह काउंसिल की बैठक के लिए एजेंडा है। एजेंडा के किसी भी आइटम में सहायता के लिए, कृपया 8470 8888 पर टेलीफोन करें।

### Italian

Questo è l'ordine del giorno della riunione del Comune. Per assistenza con qualsiasi punto all'ordine del giorno, si prega di chiamare il numero 8470 8888.

### Macedonian

Ова е Дневниот ред за состанокот на Општинскиот одбор. За помош во врска со која и да било точка од дневниот ред, ве молиме телефонирајте на 8470 8888.

### Nepali

यो परिषद्को बैठकको एजेन्डा हो। एजेन्डाका कुनै पनि वस्तुसम्बन्धी सहायताका लागि कृपया 8470 8888 मा कल गर्नुहोस्।

### Punjabi

ਇਹ ਵੈੱਸਲ ਦੀ ਮੀਟਿੰਗ ਵਾਸਤੇ ਏਜੰਡਾ ਹੈ। ਏਜੰਡੇ ਦੀਆਂ ਕਿਸੇ ਵੀ ਆਈਟਮਾਂ ਸੰਬੰਧੀ ਸਹਾਇਤਾ ਵਾਸਤੇ, ਕਿਰਪਾ ਕਰਕੇ 8470 8888 ਨੂੰ ਟੈਲੀਫੋਨ ਕਰੋ।

### Somali

Kani waa Ajandaha Kulanka Golaha. Caawimada mid kasta oo ka mid ah qodobada laga wada hadlay, fadlan la xiriir 8470 8888.

### Spanish

Este es el Orden del día de la Reunión del Concejo. Para recibir ayuda acerca de algún tema del orden del día, llame al teléfono 8470 8888.

### Urdu

يہاں اس کی اجنڈا کے بارے میں مدد کے لیے براہ مہربانی 8470 8888 پر فون کریں۔

### Vietnamese

Đây là Chương trình Nghị sự phiên họp Hội đồng Thành phố. Muốn có người trợ giúp mình về bất kỳ mục nào trong chương trình nghị sự, xin quý vị gọi điện thoại số 8470 8888.

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# Agenda

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## 1. MEMBERSHIP

Cr. Lina Messina (Mayor) (Chairperson)

Cr. Gaetano Greco (Deputy Mayor)

Cr. Emily Dimitriadis

Cr. Tom Hannan

Cr. Tim Laurence

Cr. Trent McCarthy

Cr. Susanne Newton

Cr. Susan Rennie

Cr. Julie Williams

## 2. APOLOGIES

## 3. DISCLOSURES OF CONFLICTS OF INTEREST

## 4. CONFIRMATION OF THE MINUTES OF PLANNING COMMITTEE

<b>Recommendation</b>
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**That** the Minutes of the Planning Committee Meeting held on 15 June 2021 be confirmed as a correct record of business transacted.

## 5. CONSIDERATION OF REPORTS

### 5.1 APPLICATION FOR PLANNING PERMIT D/62/2021 667-671 Heidelberg Road Alphington

**Author:** Statutory Planner

**Reviewed By:** Acting General Manager City Sustainability and Strategy

Applicant	Owner	Consultant
Northside Movement Pty Ltd	Nardella Super Admin Pty Ltd	Direct Planning

#### SUMMARY

- The application is for a change of use from motor vehicle sales business to a restricted recreation facility (gym).
- The provision of car parking proposed to be associated with the use is zero (0) spaces.
- Internal alterations are proposed, with no increase in floor area.
- The proposed hours of operation are Monday to Friday 5:30am to 9:00pm, Saturday & Sunday 6:30am to 6:00pm and Public holidays 10:00am to 3:00pm
- The use will have a maximum occupancy of 20 patrons and three staff at any one time.
- The site is zoned Industrial 3 Zone (IN3Z) and is affected by Schedule 1 to the Public Acquisition Overlay (PAO1) and the Development Contributions Plan Overlay (DCPO).
- Heidelberg Road is a Category 1 road (RDZ1) and is managed by the Department of Transport (DoT).
- There is no restrictive covenant on the title for the subject land.
- 10 objections were received against this application.
- 12 letters of support have been received.
- It is recommended that the application be supported.

#### CONSULTATION:

- Public notice was given via a sign posted on site and letters sent to surrounding owners and occupiers.
- This application was referred internally to Council's Assets and Capital Delivery Unit, Property Services Unit, Strategic Planning Unit, Health Unit and Climate Emergency and Sustainable Transport Unit.
- This application was referred externally to the DoT.

**Recommendation**

**That** Planning Permit Application D/62/2021 be supported and a Notice of Decision to Grant a Permit be issued subject to the following conditions:

1. Before the use starts, amended plans to the satisfaction of the Responsible Authority must be submitted to, and approved by, the Responsible Authority. The plans must be drawn to scale with dimensions and must be generally in accordance with the plans submitted with the application (identified as A-100 and A-101 prepared by *Xavier Meade dated 12/02/21*) but modified to show:
  - (a) Construction details of the bicycle parking.
  - (b) A minimum of 50% of the bike parking to be ground mounted with dimensions to comply with Australian Standard 2890.3:2015.

When approved, the plans will be endorsed and form part of this Permit.

2. The layout of the use as shown on the endorsed plans must not be altered without the prior written consent of the Responsible Authority.
3. Before the use starts fifteen (15) bicycle parking spaces shown on the endorsed plans must be provided on the site to the satisfaction of the Responsible Authority.
4. The number of patrons on the premises at any one time must not exceed twenty (20).
5. The maximum number of employees on site at any time must not exceed three (3).
6. The use may operate only between the hours of:
  - 5.30am to 9.00pm Monday to Friday,
  - 6.30am to 6.00pm Saturday and Sunday and
  - 10:00am to 3:00pm Public holidays
7. No external sound amplification equipment or loudspeakers are to be used on the land.
8. The use must not adversely affect the amenity of the area, including through the:
  - transport of materials, goods or commodities to or from the land;
  - appearance of any building, works, stored goods or materials;
  - emission of noise, artificial light, vibration, odour, fumes, smoke, vapour, steam, soot, ash, dust, waste-water, waste products, grit or oil; andand/or in any other way, to the satisfaction of the Responsible Authority.
9. Noise from the premises must not exceed the relevant limits prescribed by the State Environment Protection Policy (Control of Noise from Commerce, Industry and Trade) No. N-1.
10. External lighting must be designed, baffled and located so as to prevent any adverse effect on adjoining and nearby land to the satisfaction of the Responsible Authority.
11. No plant, equipment, services or architectural features other than those shown on the endorsed plans are permitted above the roof level of the building/s without the prior written consent of the Responsible Authority.
12. No goods, equipment, packaging material, or any other material/object must be stored, or left exposed, outside a building so as to be visible from any public road or thoroughfare, to the satisfaction of the Responsible Authority.



13. No intermittent or flashing light may be installed on the land without the prior written consent of the Responsible Authority.
14. The land must be drained to the satisfaction of the Responsible Authority.
15. Provision must be made for the storage and collection of garbage, bottles and other solid wastes in bins or receptacles, to the satisfaction of the Responsible Authority.

All bins and receptacles used for the storage and collection of garbage, bottles and other solid wastes must be kept in a storage area screened from view, to the satisfaction of the Responsible Authority.

All bins and receptacles must be maintained in a clean and tidy condition and free from offensive odour, to the satisfaction of the Responsible Authority.

16. In the event that any buildings and works are carried out which would destroy or break up the existing flooring/capping of the site, before any such work commences, a site assessment of the site, prepared by a member of the Australian Contaminated Land Consultants Association (Victoria) Inc. or other suitably qualified environmental professional, must be submitted to the Responsible Authority to its satisfaction.

The site assessment must include:

- An opinion on the level and nature of contamination (if any), how much is present and how it is distributed;
- Details of any clean up, construction, ongoing maintenance, monitoring or other measures in order to effectively manage contaminated soil (if any) that is present within the site (management measures); and
- Recommendation on whether the environmental condition of the land is suitable for the proposed use and whether an environmental audit of the land should be undertaken.

Should the consultant's opinion be that an environmental audit be undertaken, before any buildings or works or the use commences, either:

- An environmental audit statement under Part 8.3, Division 3 of the *Environment Protection Act 2017* which states that the site is suitable for the use and development allowed by this permit, or
- An environmental audit statement under Part 8.3, Division 3 of the *Environment Protection Act 2017* which states that the site is suitable for the use and development allowed by this permit if the recommendations made in the statement are complied with.

In the event that management measures are required or recommendations are made in the statement which must be complied with under Part 8.3, Division 3 of the *Environment Protection Act*, before any buildings or works or the use commences all recommendations of the environmental audit statement must be complied with to the satisfaction of the Responsible Authority. Written confirmation of compliance with the recommendations or the conditions of the Environmental Audit Statement must be provided by a suitably qualified environmental professional or other suitable person acceptable to the responsible authority. Compliance sign off must be in accordance with any requirements in the environmental audit statement recommendations regarding verification of works.

In the absence of a site management order and where there are recommendations on an environmental audit statement that require ongoing maintenance and/or monitoring, before any buildings or works or the use commences the owner of the land must enter into an Agreement with the Responsible Authority under Section 173 of the *Planning and Environment Act 1987* to the satisfaction of the Responsible Authority. This

agreement must be to the effect that except with the written consent of the Responsible Authority all recommendations or conditions of the environmental audit statement issued in respect of the land will be complied with to the satisfaction of the Responsible Authority. Written confirmation of compliance with the recommendations of the environmental audit statement must be provided by a suitably qualified environmental professional.

A memorandum of the Agreement must be entered on the Title to the land and the owner must pay the costs of the preparation and execution of the Agreement and entry of the memorandum on Title.

Department of Transport Condition (Condition 17):

17. No compensation is payable under part 5 of the Planning and Environment Act 1987 in respect of anything done under this permit.
18. This Permit will expire if the use is not started within three (3) years of the date of this Permit.

The Responsible Authority may extend the time referred to if a request is made in writing before this Permit expires or within six (6) months after the expiry date.

## NOTATIONS

**(These notes are provided for information only and do not constitute part of this permit or conditions of this permit)**

- N1. Any failure to comply with the conditions of this planning permit may result in the issue of an Enforcement Order against some or all persons having an interest in the site. Non-compliance may result in legal action or the cancellation of this permit by the Victorian Civil and Administrative Tribunal.
- N2. This Planning Permit represents the planning approval for the use and/or development of the site and does not represent the approval of other Council departments or statutory authorities. Other approvals may be required before the use/and or development allowed by this planning permit starts.
- N3. Amendments made to plans noted in Condition No. 1 of this Permit are the only ones that will be assessed by Council. If additional amendments are made to the development, they must be brought to the attention of Council as additional planning assessment may be required through a separate planning approval.
- N4. Except where no planning permission is required under the Darebin Planning Scheme, no sign may be displayed on the site without further planning approval
- N5. A suitably qualified environmental consultant acceptable to the responsible authority may include an environmental auditor appointed under the *Environment Protection Act 2017* or an environmental professional with qualifications and competence consistent with Schedule B9 of the National Environment Protection (Assessment of Site Contamination Measure 1999) (as amended 2013).

## INTRODUCTION AND BACKGROUND

Council issued Planning Permit D/148/2002 for 'the use the land for car sales and motor vehicle repairs in accordance with the attached endorsed plans' on 03 June 2002.

## ISSUES AND DISCUSSION

### Subject site and surrounding area

- The land is regular in shape with a frontage of 15.64 metres to Heidelberg Road, and a depth of 45.473 metres and a total site area of 711 square metres.
- The land is located on the northern side of Heidelberg Road approximately 45 meters to the west of the intersection with Clive Street.
- A 3.0-metre-wide Right-of-Way (ROW) is located to the rear of the site.
- The site contains a commercial / industrial building formally used for motor vehicle sales. The building covers 100% of the site and contains an upper floor element in the double height space.
- To the north of the site, on the opposite side of the ROW, are the rear of residential properties located on Clive Street and Lowther Street. No. 2 Clive Street and No. 1/1 Lowther Street are the properties directly adjoining the subject site.
- To the south, on the opposite side of Heidelberg Road, is the Yarrabend development site, located in the City of Yarra. The development covers 16.5 hectares. The two sub precinct's (1A and 2B) located directly adjacent to the site have planning permission for a six storey residential aged care facility and mixed use development containing dwellings, supermarket, shops, food and drink premises, office, restricted recreation facility (gym), childcare centre, education centre (primary school), serviced apartments and place of assembly. The development is currently under construction.
- To the east, is a similar former warehouse building currently being used as an art gallery.
- To the west, is a car wash with permission for the site to also be used for a food and drink premises (food truck) during the evening / afternoon.
- On street parking restrictions are that Heidelberg Road is a clearway between 4pm and 7pm Monday to Friday, Lowther Street has 2-hour parking restrictions between 8am and 5:30pm Monday to Friday and 8am and 12pm Saturday and Clive Street has unrestricted parking
- The site is located just outside the Principal Public Transport Network Area.
- The site has access to public transport within walking distance in the form of Alphington Station and bus services as follows:
  - Alphington Station approximately 550 metres away by foot to the North East.
  - Bus Route 546 (Melbourne University-Heidelberg Station) along Heidelberg Road.
  - Bus Route 350 (City-La Trobe University) along Grange Road.
  - Bus Route 350 (Hawthorn-Fairfield) along Grange Road.



Source: PTV website

**Proposal**

- The application is for a change of use to a restricted recreation facility (gym) and associated internal building works.
- The applicant operates an existing gym known as ‘Northside Movement’ located in Clifton Hill. Due to the re-development of that property they are seeking to relocate to the subject site on Heidelberg Road.
- Zero (0) car parking spaces are proposed to be provided.
- The use will have a maximum occupancy of 20 patrons and 3 staff at any one time.
- Proposed hours of operation are Monday to Friday 5:30am to 9:00pm, Saturday & Sunday 6:30am to 6:00pm and Public holidays 10:00am to 3:00pm.

**Objections summarised**

- Inadequate car parking.
- Traffic and associated safety concerns.
- Increase in litter associated with patrons to the gym.

**Officer comment on summarised objections**

Inadequate Car parking

The resident’s car parking concerns relate to the subject proposal and apprehension that the Yarrabend development occurring on the opposite side of Heidelberg Road will further increase demand for car parking. It is estimated that the Yarrabend development has a further five years to reach completion.

While the provision of zero (0) on-site car parking spaces for a gym is somewhat unusual, it is considered appropriate in this location for the following reasons:

- A relatively low number of patrons will access the site (reduced to 20 patrons, in lieu of 30 patrons as initially proposed).

- The expected peak periods are early morning and evenings outside of normal 9am-5pm business hours and as such sufficient on-street parking is likely to be available.
- 15 on-site bicycle parking spaces are being provided.
- The site has an existing and future customer catchment within walking distance.
- The current building and use as a car dealership make no provision for on-site parking.
- Unlike some other uses that are likely to generate extensive car-trips (bulky goods retailing and other retail types) a gym is a use that patrons can and would readily access via walking or cycling.
- No additional floor area is being proposed.

Council's Transport Engineering Team has advised that if there were an increased car parking demand arising from the use, it could be mitigated by the introduction of temporary or permanent parking restrictions within Clive Street and surrounds. Residents of Clive Street support this approach and have the option to explore this further with Council's Climate Emergency and Sustainable Transport Department, outside the planning application process.

#### Traffic and associated safety concerns

Residents raised concerns that as a result of the increased pressure for car parking in the area, there has been an associated increase in traffic. The increase in traffic is likely to be the result of the Yarrabend development. Nonetheless, and accounting for this increase, it is considered that the surrounding road network has sufficient capacity to accommodate the expected increase in traffic associated with a gym of this size, particularly during the expected early morning and late evening peak periods.

#### Increase in litter associated with patrons to the gym

The use of the land will not directly lead to an increase in rubbish in the local area. The appropriate management of waste on the site would be conditioned as part of any approval for a commercial use.

#### **Letters of support summarised**

The application received 12 letters of support from existing members of Northside Gym which is currently located in Clifton Hill. Many of the letters were received from residents with an Alphington or Fairfield address. Many indicated they would remain members of the relocated gym and would cycle to the site.

Comments received:

- Small family run business with a community focus.
- There are typically less than 15 attendees at any given time.
- Attendance is by appointment only.
- Reasonable operating hours (not 24hrs like other gyms).
- A large proportion of the clientele live locally & the majority of clients walk, run or ride to attend and would not require parking.
- Focus on women only classes.
- The proposal addresses the physical and mental health needs of the community.

- Any concerns regarding car parking are out of proportion due to the current construction of the Yarrabend development located opposite.
- The council could fix the car parking by installing resident only parking and a two-hour limit in the adjoining streets.

## PLANNING ASSESSMENT

### Planning Policy Framework

The starting point with respect to policy analysis is the Planning Policy Framework. The proposal gathers widespread support from State planning policy, in particular:

- Clause 11.02-1S (Supply of urban land). The objective is to *ensure a sufficient supply of land is available for residential, commercial, retail, industrial, recreational, institutional and other community uses.*
- Clause 15.01-4S (Healthy neighbourhoods) seeks to achieve neighbourhoods that foster healthy and active living and community wellbeing. Strategies to achieve this objective include providing connected, safe, pleasant and attractive walking and cycling networks that enable and promote walking and cycling as a part of daily life. Streets with direct, safe and convenient access to destinations. Conveniently located public spaces for active recreation and leisure. Amenities and protection to support physical activity in all weather conditions.
- Clause 15.01-4R (Healthy neighbourhoods - Metropolitan Melbourne). The objective is to *create a city of 20-minute neighbourhoods, that give people the ability to meet most of their everyday needs within a 20-minute walk, cycle or local public transport trip from their home.*
- Clause 17.02-1S (Business) encourages development which meets the community's need for commercial services.
- Clause 19.02-1S (Health facilities) encourages the integration of health facilities with local and regional communities.

### Local Planning Policy Framework

#### Council's Municipal Strategic Statement ('MSS')

Council's MSS provides the strategic guidance for the municipality and sets out (among other things) the provision of appropriate commercial opportunity through land supply for businesses as a key future issue. With reference to Local Centres specifically, new shops and services are generally encouraged.

This area is earmarked in the *Darebin Economic Land Use Strategy (2014)*, to be rezoned to better enable commercial uses, as the current zoning does not reflect existing and is unlikely to reflect future land uses.

The policies in the MSS emphasises that a local service business, such as the proposed gym, can be accommodated in locations such as this.

Clause 21.01-5 (Strategic Vision) encourages *a physical, social and economic environment that supports and enhances community health and wellbeing.*

**Industrial and Commercial Activity (Clause 22.04)**

Council's local planning policy for industrial and commercial activity affects all land zoned Industrial 3 within Darebin. The following policy objectives of Clause 22.04-2 are applicable to the proposal:

- *To ensure a high standard of urban design is achieved to improve the visual character, functioning and layout in industrial and commercial areas.*
- *To minimise the impact on the amenity of surrounding residential areas from traffic, noise and emissions resulting from industrial land uses.*
- *To reduce and minimise conflict between industrial and non-industrial land uses.*

While the proposed gym is not an industrial land use, many of Darebin's small industrial pockets have experienced a shift away from industrial and commercial uses as a result of economic factors and competition from larger industrial estates within Darebin and estates located on the urban fringe. Critically a gym is not a sensitive land use and is unlikely to conflict with any existing industrial uses which remain in the vicinity.

Further supporting the change of use to a restricted recreation facility is the ongoing clustering of traditionally commercial uses within this industrial area. The southern side of Heidelberg Road, within the city of Yarra, has been rezoned to a Mixed-Use zone and Commercial 1 zone, with the area in Darebin currently in the early stage of zoning and policy review. This shift in zoning has changed the dynamic of Heidelberg Road as evidenced by the emergence of some recent mixed-use apartment proposals on either side of Heidelberg Road.

On balance, it is considered that change of use to a restricted recreation facility meets the objectives of Clause 22.04 as the proposed use is not incompatible with the surrounding existing uses, it will not undermine the employment generation capacity of the site and surroundings and it will provide for diversification of local employment opportunities.

**Industrial 3 Zone (Clause 33.03)**

The application is assessed against the relevant purposes and decision guidelines of the Industrial 3 Zone as follows:

Purpose

*To implement the Municipal Planning Strategy and the Planning Policy Framework.*

As set out earlier in this report, the proposed use as a gym has support in both Council's Municipal Strategic Statement and relevant Planning Policy Framework.

*To allow limited retail opportunities including convenience shops, small scale supermarkets and associated shops in appropriate locations.*

A gym is an appropriate use which will serve a local residential catchment.

*To ensure that uses do not affect the safety and amenity of adjacent, more sensitive land uses.*

Given the hours of operation and the nature of the use, the proposal is not expected to detrimentally affect the safety and amenity of the adjacent, more sensitive land uses, in particular the residential uses to the north of the site.

Use

*The effect that the use may have on nearby existing or proposed residential areas or other uses which are sensitive to industrial off-site effects, having regard to any comments or directions of the referral authorities.*

Given the hours of operation and the nature of the use, the proposal is not expected to detrimentally affect the safety and amenity of the adjacent, more sensitive land uses, in particular the residential uses to the north of the site.

*The effect that nearby industries may have on the proposed use.*

Existing uses along Heidelberg Road and adjacent streets are unlikely to have a detrimental effect on the proposed use given that the use will be conducted in-doors. The existing ROW and road network effectively act as a buffer between the proposed use and nearby uses on industrial zoned land.

*The drainage of the land.*

The land is connected to existing drainage. A condition of any approval will require the land to continue to be drained to the satisfaction of Council.

*The availability of and connection to services.*

Services are available and connected to the property.

*The effect of traffic to be generated on roads.*

A condition of any approval will limit the patron numbers to 20 as proposed. A maximum of three (3) staff will be required to operate the use during peak periods.

The associated traffic generation is unlikely to be significant due to the number of patrons and the likelihood that some of these patrons will choose alternative methods of travel, including active transport options, public transport and the sharing of vehicles. Traffic will be concentrated to those times before and after classes which are expected to be outside of business hours.

*The interim use of those parts of the land not required for the proposed use.*

All of the subject site is required for the proposed use.

*The effect on nearby industries.*

The proposed will not have a negative effect on nearby industries.

**Car Parking (Clause 52.06)**

A gym is not a prescribed use under Clause 52.06 of the Darebin Planning Scheme and as such the number of car parking provided must be to the satisfaction of the Responsible Authority.

The provision of zero (0) car parking spaces for the use of the site as a gym is appropriate for the following reasons:

- The number of patrons (reduced to 20 patrons, in lieu of 30 patrons as initially proposed).



- The proposed hours of operation and the expected peak periods are early morning and evening period outside of normal business hours.
- The provision of 15 bicycle parking spaces on site.
- Impractical to provide car parking on site.
- No additional floor area.
- Lack of other suitable sites in the immediate area with onsite car parking.
- Car parking demands associated with other potential land uses at site including those which do not require a planning permit would be similar if not greater than the proposal. These include an office.

A level of comfort is also attained from the 12 letters of support from current members of the gym (which currently operates in Clifton Hill). Many of these members hold a Fairfield or Alphington address, indicated they would remain members of the relocated gym and would either walk or ride a bike to the site. Some residents expressed support for parking restrictions in the surrounding streets to offset the impact of increased car parking demand, particularly demand associated with the Yarrabend development.

Generally, the overspill of parking in residential streets is best managed through the implementation of appropriate parking controls on street parking, and Council has a process in place through which such parking controls are investigated in consultation with residents.

#### **Public Acquisition Overlay (Clause 45.01)**

A permit is required for any Section 1 or Section 2 use in the zone and to construct a building or construct or carry out works. The application was referred to the DoT who have requested a condition that will be included on any approval.

#### **Land Adjacent to A Road Zone, Category 1, Or A Public Acquisition Overlay for a Category 1 Road (Clause 52.29)**

The site is located adjacent to a Road Zone, Category 1 road. As such, a referral to the DoT is required to ensure that the proposed change of use (and alteration to the access associated with the change of use) is appropriate. The DoT have requested a condition that will be included as part of any approval.

#### **Clause 52.34 Bicycle Parking**

Under Clause 52.34-5 of the Darebin Planning Scheme a restricted recreation facility (gym) is nested under minor recreation facility. Clause 52.34-5 recommends the provision of 1 bicycle space to each 200 square metres of floor area which equates to six bicycle spaces.

The proposal includes the provision of fifteen (15) bicycle parking spaces, which will provide the opportunity for patrons and staff to utilise a sustainable transport option rather than visiting the property utilising a private vehicle.

**REFERRAL SUMMARY**

Department/Authority	Response
Assets and Capital Delivery	No objection, subject to condition included in recommendation
Climate Emergency and Sustainable Transport	No objection, subject to condition included in recommendation
Health	No objection, subject to condition included in recommendation
Strategic Planning	No objection
Department of Transport	No objection, subject to condition included in recommendation

**PLANNING SCHEME SUMMARY**

**Darebin Planning Scheme clauses under which a permit is required**

- Clause 33.03-1 (Industrial 3 Zone) – use as a restricted recreation facility (gym) requires a planning permit.
- Clause 45.01-1 (Public Acquisition Overlay) – a permit is required to use land for any Section 1 or Section 2 use in the Zone.
- Clause 52.29-2 (Land Adjacent to a Road Zone, Category 1, or a Public Acquisition Overlay) – a permit is required to Create or alter access to a Road in a Road Zone Category 1.

**Applicable provisions of the Darebin Planning Scheme**

Section of Scheme	Relevant Clauses
PPF	11.03-1S, 17.02-1S, 17.02-2S, 17.03-1S
LPPF	21.01, 21.04, 22.04
Zone	33.03-1
Overlay	45.01, 45.06
Particular provisions	52.06, 52.29, 52.34
General provisions	65.01
Neighbourhood Character Precinct	D4

**POLICY IMPLICATIONS**

**Social Inclusion and Diversity**

Nil

**Other**

Nil

## FINANCIAL AND RESOURCE IMPLICATIONS

There are no financial or resource implications as a result of the determination of this application.

## FUTURE ACTIONS

Nil

## RELATED DOCUMENTS

- Planning and Environment Act 1987
- The Darebin Planning Scheme
- Darebin Economic Land Use Strategy August 2014

## Attachments

- Aerial Photo (**Appendix A**) [↓](#)
- Advertised Plans (**Appendix B**) [↓](#)

## DISCLOSURE OF INTEREST

Section 130 of the *Local Government Act 2020* requires members of Council staff and persons engaged under contract to provide advice to Council to disclose any general or material interest in a matter to which the advice relates.

The Officer reviewing this report, having made enquiries with relevant members of staff, reports that no disclosable interests have been raised in relation to this report.

**Aerial Map - 667-671 HEIDELBERG ROAD ALPHINGTON**

Darebin City Council  
11/06/2021



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Advertised Document D/62/2021 Page 1 of 4

Job Title

# Northside Movement

671 Heidelberg Rd, Alphington 3078

Drawing Status

## TOWN PLANNING

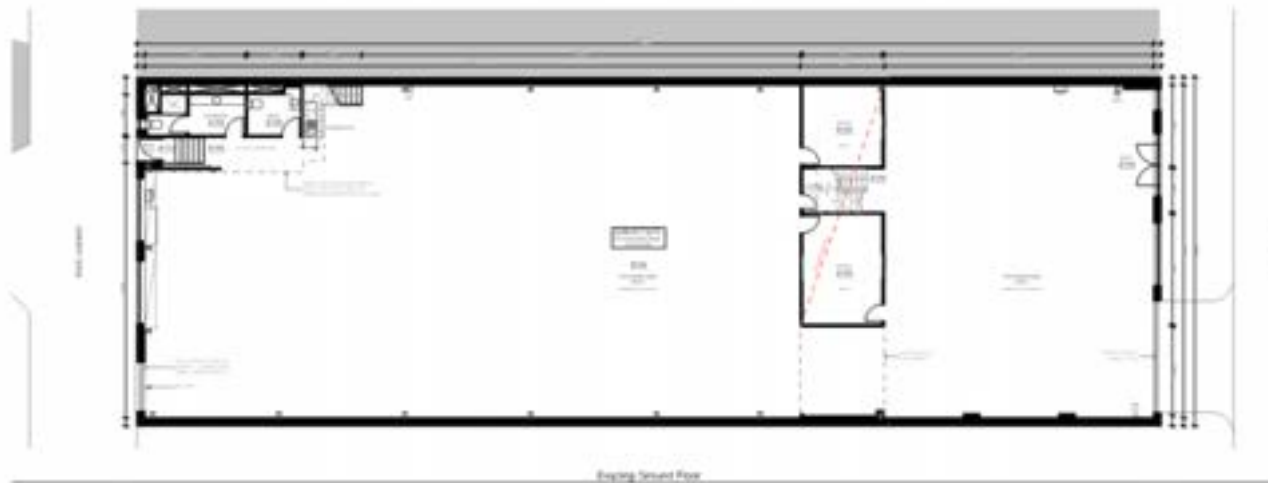
NO.	DATE
1	18/02/2021

### Drawing Schedule

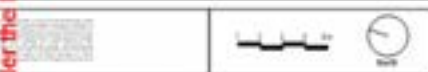
NO.	DATE
A1/00 - EXISTING FLOOR PLAN	-
A1/00 - PROPOSED FLOOR PLAN	-
A1/01 - PROPOSED LANDSCAPE PLAN	-

Darebin City Council Received 18/02/2021

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Darebin City Council Received 18/02/2021

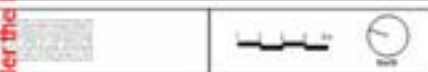


PROJECT 471 Mulgrave Rd, Brighton 3186	PROJECT NO. TOWN PLANNING	PROPOSAL 1:500 S.A.1 1:500 S.A.2	DATE 08/02
DATE 18/02/21	SCALE 50%	DRAWN BY 1	

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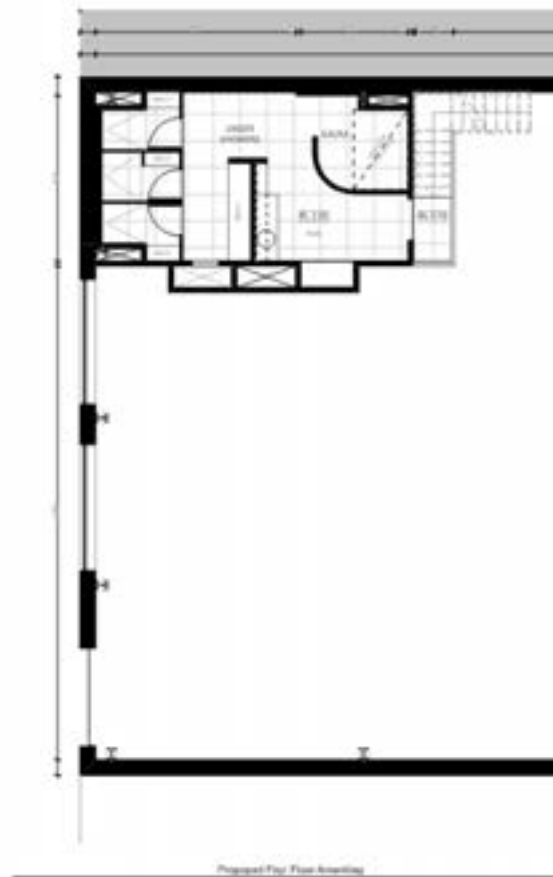


Darebin City Council Received 18/02/2021

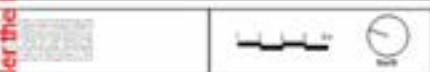


Project Name 471 Mulgrave Rd, Brighton 3186	Project Status TOWN PLANNING	Project No. 1500-0-01-1-000-0-01	Project Date 21/08
Document Proposed Second Floor, Proposed First Floor	Scale 1:500	Author [Name]	Reviewer [Name]

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Darebin City Council Received 18/02/2021



PROJECT 471 Mulberry Rd, Brighton 3186 DRAWING Proposed Ground Floor Assembly, Proposed First Floor Assembly	DRAWING NO. TOWN PLANNING DATE 18/02/21	DRAWING SCALE 1:50 (S.A.), 1:100 (N.A.) SHEET NO. 08	DRAWN BY J.C. DE CHECKED BY J.C. DE
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**5.2 APPLICATION FOR PLANNING PERMIT D/874/2018  
893 High Street Thornbury**

**Author:** Town Planner

**Reviewed By:** Acting General Manager City Sustainability and Strategy

Applicant	Owner	Consultant
C Kairouz Architects	RC Consolidated Industries Pty Ltd	Sustainable Development Consultants Wood and Grieve Engineers Gareth Gale Town Planning & Advocacy Leigh Design

**SUMMARY**

- The proposal involves the partial demolition of the existing building and the development of a five-storey (plus rooftop terrace and basement) mixed use proposal comprising:
  - Partial demolition and partial retention of the existing ground floor building.
  - Five (5) dwellings (4 x 2-bedroom and 1 x 3-bedroom).
  - One (1) convenience restaurant (café) (111 square metres).
  - Six (6) car parking spaces.
  - Three (3) bicycle parking spaces.
- The site is zoned Commercial 1 Zone and is affected by the Heritage Overlay and the Development Contributions Plan Overlay.
- There is no restrictive covenant on the title for the subject land.
- There are currently six (6) objections against this application. It is noted that during the consultation process one (1) objection was withdrawn.
- The proposal is generally consistent with the objectives and standards of Clause 22.05, 22.06, 43.01 and 58 of the Darebin Planning Scheme.
- It is recommended that the application be supported.

**CONSULTATION:**

- Public notice was given via one (1) sign posted on site and letters sent to surrounding owners and occupiers.
- This application was referred internally to the ESD Officer, City Designer, Climate Emergency and Sustainable Transport Unit, Heritage Advisor, City Design Unit, Urban Design Unit, Property Management Unit, and Infrastructure and Capital Delivery Unit.
- This application was not required to be referred to external authorities.
- On the 20 May 2021, a consultation meeting was facilitated by Council between the applicant and objectors in order to identify solutions and measures to mitigate amenity impacts to residents. Please see 'Introduction and background' for details of the outcomes of this meeting.

**Officer Recommendation**

**That** Planning Permit Application D/874/2018 be supported and a Notice of Decision to Grant a Permit be issued subject to the following conditions:

- (1) Before the development starts, amended plans to the satisfaction of the Responsible Authority must be submitted to, and approved by the Responsible Authority. When approved, the plans will be endorsed and will then form part of this Permit. The plans must be drawn to scale with dimensions and must be generally in accordance with the plans submitted with the application (identified as A07 dated 23.09.2019 and A10-A17 dated 4.6.2020, all prepared by C. Kairouz Architects) but modified to show:
  - a. The ground floor use clearly nominated as a convenience restaurant (café).
  - b. The height of the planter boxes to the balconies of apartments 2.2 and 3.1 nominated and construction detail.
  - c. Traffic management information provided by a qualified traffic engineer on how waste collection via the rear laneway is arranged.
  - d. The minimum dimension of the balcony to apartment 1.2 in accordance with the requirements of Standard D19 of Clause 58.05-3 of the Darebin Planning Scheme.
  - e. The living areas of apartments 2.1 and 3.1 as per Standard D24 of Clause 58.07-1 of the Darebin Planning Scheme and to be provided clear of the dining areas.
  - f. Visitor bicycle parking in accordance with Condition No. 23 of this Permit.
  - g. A single communal antenna for the development in accordance with Condition No. 7 of this Permit. The location of the antenna must be shown on the roof plan and elevations. The height of the antenna must be nominated.
  - h. Any modifications required as a result of the approved Acoustic Report required by Condition No. 9 of this Permit.
  - i. The provision of a Stormwater Management System Plan, including a Water Sensitive Urban Design (WSUD) Plan, in accordance with Standard W1 of Clause 53.18-4 of the Darebin Planning Scheme. Refer to Condition No. 19 of this Permit.
  - j. The provision of a Site Management Plan in accordance with Standard W3 of Clause 53.18-6 of the Darebin Planning Scheme. Refer to Condition No. 20 of this Permit.
  - k. Garage lighting to the rear to be downlight task lighting and designed, baffled and located so as to prevent any adverse effect including direct light into existing windows on adjoining and nearby land.
  - l. The roller door to the garage treated with acoustic measures to further reduce noise impacts to the laneway.
  - m. The car stacker system treated with acoustic measures to ensure any noise impact to adjoining properties does not exceed a decibel range of a maximum of 62 decibels when measured from the laneway to the rear of 893 High Street Thornbury.

When approved, the plans will be endorsed and form part of this Permit.
- (2) The development as shown on the endorsed plans must not be altered without the prior written consent of the Responsible Authority.

- (3) This Permit will expire if either:
- a. The development does not start within three (3) years from the date of this Permit; or
  - b. The development is not completed within five (5) years of the date of this Permit.

As relevant, the Responsible Authority may extend the times referred to if a request is made in writing:

- a. Before this Permit expires.
  - b. Within six (6) months after the expiry date; or
  - c. Within twelve (12) months after the expiry date if the request relates to the completion of the development or a stage of the development.
- (4) The land must be drained to the satisfaction of the Responsible Authority.
- (5) At the completion of the constructed ground floor level(s), and before the starting of the building frame or walls, a report prepared by a licensed land surveyor to the satisfaction of the Responsible Authority must be submitted to the Responsible Authority, confirming the ground floor level(s). The report must be submitted to the Responsible Authority no later than 7 days from the date of the inspection.

The development must not be occupied until a report prepared by a licensed land surveyor to the satisfaction of the Responsible Authority is submitted to the Responsible Authority, confirming the floor level(s).

- (6) Before the development starts a Demolition and Construction Management Plan must be submitted to the satisfaction of the Responsible Authority and approved by the Responsible Authority. When approved, the Demolition and Construction Management Plan will be endorsed and will then form part of this Permit. The Demolition and Construction Management Plan must address, without limitation, the following:
- a. Contact details for key construction site staff including after-hours contact numbers.
  - b. Hours for the construction activity.
  - c. Measures to control the escape of noise, dust, litter, water, and sediment laden runoff from the site.
  - d. Measures to control mud, crushed rock or other debris being carried onto public roads or footpaths from the site.
  - e. The protection measures for site features to be retained (e.g., vegetation, retaining walls, buildings, other structures, and pathways, etc).
  - f. On site facilities for vehicle washing.
  - g. Delivery and unloading points and expected frequency.
  - h. The location of parking areas for construction vehicles and construction workers vehicles, to ensure that vehicles associated with demolition and/or construction activity cause minimal disruption to surrounding land uses and traffic flows.
  - i. Any traffic management plans and measures that will be required to allow vehicles to safely access the site and to safely undertake deliveries/works.
  - j. Management of laneway access during construction.
  - k. An outline of requests to occupy public footpaths, bicycle paths or roads, and anticipated disruptions to public transport services.

- l. The processes to be adopted for the separation, re-use and recycling of demolition materials.
- m. The measures for prevention of the unintended movement of building waste and hazardous materials and other pollutants on or off the site, whether by air, water, or other means
- n. The measures to minimise the amount of waste construction materials; the provision for the recycling of demolition and waste materials; and the return of waste materials to the supplier (where the supplier has a program of reuse or recycling).
- o. Any other relevant matters.

The requirements of the endorsed Demolition and Construction Management Plan must be implemented and complied with to the satisfaction of the Responsible Authority.

- (7) Only one (1) communal television antenna may be erected on the building and located to be minimally visible from High Street. Individual antennae for individual dwellings/tenancies must not be erected.
- (8) The development must not adversely affect the amenity of the area, including through the:
  - a. transport of materials, goods, or commodities to or from the land.
  - b. appearance of any building, works, stored goods or materials.
  - c. emission of noise, artificial light, vibration, odour, fumes, smoke, vapour, steam, soot, ash, dust, waste water, waste products, grit, or oil; and

and/or in any other way, to the satisfaction of the Responsible Authority.

- (9) Before plans are endorsed under Condition No. 1 of this Permit, an Acoustic Report to the satisfaction of the Responsible Authority must be submitted and approved by the Responsible Authority. When approved, the Acoustic Report will be endorsed and will then form part of this Permit. The Acoustic Report must be prepared by a suitably qualified acoustic engineer and must include recommended acoustic attenuation measures and treatments to ensure that:
  - a. Noise levels associated with the operation of surrounding and nearby non-residential uses such as retail and commerce / tram lines / road traffic do not impact adversely on the amenity of the dwellings.
  - b. Dwellings are designed to achieve the following noise levels:
    - i. Not greater than 35dB(A) for bedrooms, assessed as an LAeq,8h from 10pm to 6am.
    - ii. Not greater than 40dB(A) for living areas, assessed LAeq,16h from 6am to 10pm.

Noise levels should be assessed in unfurnished rooms with a finished floor and the windows closed.

- c. Noise levels within the development (including the operation of plant, mechanical car stackers, roller doors and the use of the car park) do not impact adversely on the amenity of dwellings within the development and neighbouring residential properties.
- d. Minimisation of noise levels arising from the car stacker on the site to the adjoining site to the west at 1 Benjamin Street.
- e. The roller door to the garage treated with acoustic measures to further reduce noise impacts to the laneway.

- f. The car stacker system treated with acoustic measures to ensure any noise impact to adjoining properties does not exceed a decibel range of a maximum of 62 decibels when measured from the laneway to the rear of 893 High Street Thornbury.

The requirements of the approved Acoustic Report must be implemented and complied with to the satisfaction of the Responsible Authority.

- (10) The car stackers must be routinely serviced and maintained to the satisfaction of the Responsible Authority to ensure satisfactory access to all car spaces and to prevent any adverse effect on adjoining land by the emission of noise.
- (11) All guttering, rainheads, pipes including downpipes, fixtures, fittings, and vents servicing any building on the site including those associated with a balcony must be:
- a. concealed in service ducts or otherwise hidden from view; or
  - b. located and designed to integrate with the development,
- to the satisfaction of the Responsible Authority.
- (12) No plant, equipment, services, or structures other than those shown on the endorsed plans are permitted above the roof level of the building/s without the prior written consent of the Responsible Authority.
- (13) A clothesline must be provided to each dwelling. Clotheslines must not be visible from High Street. If clotheslines are located on a balcony, the balcony balustrade must be solid or obscure material.
- (14) Provision must be made on the land for letter boxes and a slot for newspapers to the satisfaction of the Responsible Authority.
- (15) No goods, equipment, packaging material, or any other material/object must be stored, or left exposed, outside a building that will be visible from any public road or thoroughfare to the satisfaction of the Responsible Authority.
- (16) The walls on the boundary of the adjoining properties must be cleaned and finished to the satisfaction of the Responsible Authority.
- (17) The Sustainable Design Assessment (SDA) to be endorsed and which will then form part of this Permit is the SDA submitted with the application (identified as Sustainable Design Assessment 893 High Street, Thornbury, prepared by Sustainable Development Consultants and dated June 2020).

The requirements of the endorsed Sustainable Design Assessment (SDA) (identified as Sustainable Design Assessment 893 High Street, Thornbury, prepared by Sustainable Development Consultants and dated June 2020) must be implemented and complied with to the satisfaction of the Responsible Authority.

- (18) The development must not be occupied until a report to the satisfaction of the Responsible Authority prepared by the author of the Sustainability Design Assessment (SDA) endorsed under Condition 17 of this Permit, or similarly qualified person, is submitted to the Responsible Authority, confirming that all measures specified in the SDA have been implemented in accordance with the endorsed SDA.
- (19) Before plans are endorsed under Condition No. 1 of this Permit, a detailed Stormwater Management System Report to the satisfaction of the Responsible Authority must be submitted to and approved by the Responsible Authority. When approved, the Stormwater Management System Report will be endorsed and will then form part of this Permit. The report must include:

- a. Details of how the stormwater management system is designed to meet the current best practice performance objectives for stormwater quality contained in the Urban Stormwater - Best Practice Environmental Management Guidelines (Victorian Stormwater Committee, 1999), including:
  - i. An assessment using an industry recognised stormwater tool.
  - ii. The type of water sensitive urban design (WSUD) stormwater treatment measures to be used and details of these treatment measures including cross sections, materials, plants, and drainage directions.
  - iii. The location of stormwater treatment measures in relation to buildings, sealed surfaces, neighbouring properties, and landscaped areas.
  - iv. A plan illustrating where all impervious surfaces will be treated and drained.
  - v. A construction and maintenance schedule.
- b. Details of how the stormwater management system contributes to cooling, improving local habitat and providing attractive and enjoyable spaces.
- c. Consideration of how the WSUD stormwater treatment measures will integrate with on-site detention requirements:

The requirements of the endorsed Stormwater Management System Report must be implemented and complied with to the satisfaction of the Responsible Authority.

- (20) Before plans are endorsed under Condition No. 1 of this Permit, a Site Management Plan to the satisfaction of the Responsible Authority must be submitted to and approved by the Responsible Authority. When approved, the Site Management Plan will be endorsed and will then form part of this Permit. The Site Management Plan must be generally in accordance with Melbourne Water's Keeping Our Stormwater Clean – A Builder's Guide (2002) and must describe how the site will be managed prior to and during the construction period, including requirements for:

- a. Erosion and sediment.
- b. Stormwater.
- c. Litter, concrete, and other construction wastes.
- d. Chemical contamination.
- e. The requirements of the endorsed Site Management Plan must be implemented and complied with to the satisfaction of the Responsible Authority.

- (21) Before occupation of the development, the areas set aside for the parking of vehicles and access lanes as shown on the endorsed plan(s) must be:

- a. constructed.
- b. properly formed to such levels that they can be used in accordance with the plans.
- c. surfaced with an all-weather sealcoat.
- d. drained.

to the satisfaction of the Responsible Authority.

- (22) Before the development is occupied, an automatic external lighting system capable of illuminating car and bicycle parking areas, access lanes and driveways must be provided on the site to the satisfaction of the Responsible Authority.

The external lighting must be designed, baffled, and located to prevent any adverse effect on adjoining and nearby land to the satisfaction of the Responsible Authority.

- (23) Before plans are endorsed under Condition No. 1 of this Permit either:
- a. a contribution must be made (equivalent to one (1) bicycle space) to cycling infrastructure near the site (where possible) or within the municipality, or
  - b. One (1) ground mounted bicycle parking space must be provided in a location conveniently accessible by visitors/patrons.

Visitor bicycle parking spaces on the site must be installed and maintained to the satisfaction of the Responsible Authority.

- (24) C. Kairouz Architects or an alternative suitably qualified person approved by the Responsible Authority must be appointed for the duration of the project to oversee and ensure the design intent and construction quality of the development is followed through to the completion stage of the development, to the satisfaction of the Responsible Authority.
- (25) Garage lighting to the rear to be downlight task lighting and designed, baffled and located so as to prevent any adverse effect including direct light into existing windows on adjoining and nearby land to the satisfaction of the Responsible Authority.
- (26) The roller door to the garage treated with acoustic measures to further reduce noise impacts to the laneway.
- (27) The car stacker system treated with acoustic measures to ensure any noise impact to adjoining properties does not exceed a decibel range of a maximum of 62 decibels when measured from the laneway to the rear of 893 High Street Thornbury.

## NOTATIONS

- N1 Any failure to comply with the conditions of this planning permit may result in the issue of an Enforcement Order against some or all persons having an interest in the site. Non-compliance may result in legal action or the cancellation of this permit by the Victorian Civil and Administrative Tribunal.
- N2 This planning permit is one of several approvals required before use or development is allowed to start on the site. The planning permit holder is required to obtain other relevant approvals and make themselves aware of easements and restrictive covenants affecting the site.
- N3 Amendments made to plans noted in Condition 1 of this Permit are the only ones that will be assessed by Council. If additional amendments are made to the development they must be brought to the attention of Council as additional planning assessment may be required through a separate planning approval.
- N4 This Planning Permit represents the planning approval for the use and/or development of the site and does not represent the approval of other Council departments or statutory authorities. Other approvals may be required before the use/and or development allowed by this planning permit starts.
- N5 This planning permit does not represent approval for residential siting. Contact your appointed Building Surveyor for residential siting requirements when seeking building approval.

- N6 In relation to the requirements of Condition 24 of this Permit, please contact Council's Transport Engineering Unit (Ph: 03 8470 8220) or [Transport@darebin.vic.gov.au](mailto:Transport@darebin.vic.gov.au) for details on how to supply on-street bicycle spaces or to make an equivalent contribution.
- N7 This planning permit is to be attached to the "statement of matters affecting land being sold", under Section 32 of the Sale of Land Act 1962 and any tenancy agreement or other agreement under the Residential Tenancies Act 1997, for all purchasers, tenants and residents of any dwelling shown on this planning permit, and all prospective purchasers, tenants and residents of any such dwelling are to be advised that they will not be eligible for on-street parking permits pursuant to the Darebin Residential Parking Permit Scheme.
- 

## INTRODUCTION AND BACKGROUND

The application was reported to the Council's Planning Committee Meeting held on 10 May 2021. The Committee resolved that the matter be deferred to the next Council Planning Meeting to allow for Council to facilitate a meeting between applicant and objectors in order to find solutions and measures to mitigate amenity impacts to local residents.

A meeting was held on the 20 May 2021 and facilitated by Council officers. All the seven objectors to the proposal were invited to the meeting and three of them attended with one objector also representing the interests of a fourth objector. In addition, three representatives on behalf of the applicant were present and three Council officers being the relevant planner, Coordinator Statutory Planning and Manager City Development.

The discussion revolved mainly around issues related to the management of amenity impacts as a result of the construction works and operation of the car stackers. At the conclusion of the meeting the Applicant agreed to address a number of the concerns after further consultation with their client (the owner of the land) and provided the resident of 1 Benjamin Street (one of the primary objectors most affected by the proposal) withdrew their objection. 1 Benjamin Street is located on the opposite side of the laneway to the rear of the development site.

After further considering their options, the applicant proposed to provide additional measures which included:

- The Architects or an alternative suitably qualified person approved by the Responsible Authority be appointed for the duration of the project to oversee and ensure the design intent and construction quality of the development is followed through to the completion stage of the development
  - This requirement was considered beneficial to ensure that the communications and good will invested between the applicant and neighbouring residents during the planning application process would be carried through the construction phase of development.
- Garage lighting to the rear to be downlight task lighting and designed, baffled and located so as to prevent any adverse effect
- The roller door to the garage treated with acoustic measures to further reduce noise impacts to the laneway
- Sound-proofing measures to address noise impacts and other measures to further address other amenity impacts



- The car stacker system treated with acoustic measures to ensure any noise impact to adjoining properties does not exceed a decibel range of a maximum of 62 decibels when measured from the laneway to the rear of 893 High Street Thornbury (62 decibels is approximately the sound level a dishwasher emits).

These measures were then forwarded to all the objectors. As a result, the owner of 1 Benjamin Street advised in writing that they now withdraw their objection to the proposal.

These measures have now been included through additional proposed conditions imposed on the Notice of Decision to Grant a Planning Permit (see conditions 1(k)-1(m) and 24-27). In addition, a private agreement between the applicant and 1 Benjamin Street was provided to assist the owner of 1 Benjamin Street with additional suitable noise attenuation measures to carry out on their own property.

While the balance of the objectors didn't withdraw their objection to the proposal, the negotiated outcome between the applicant and owner of 1 Benjamin Street is considered to be a positive outcome because:

- The main amenity impacts associated with the development have now been addressed benefitting all neighbouring properties.
- Maintaining the current architects during the construction phase will help to minimise construction related concerns raised by other objectors by maximising communication between parties and therefore enabling timely resolution of any issues.
- The applicant was able to hear from objectors firsthand what their concerns were and beyond that required by condition of permit, will be more likely to further mitigate any amenity impacts during the construction phase.

## ISSUES AND DISCUSSION

### Subject site and surrounding area

- The land is slightly irregular in shape and measures 7.12 metres along High Street and has a depth of 37.12 metres with a site area of 267 square metres.
- The land has a fall of approximately 1.47 metres from east to west.
- The land is located on the western side of High Street approximately 20 metres south of the intersection with Benjamin Street.
- The site contains a single storey brick shop built to all boundaries.
- To the north is a double storey commercial property built to the front and common boundary.
- To the south a single storey commercial property built to the front and common boundary.
- To the east are double storey commercial and mixed-use buildings on the opposite side of High Street.
- To the west beyond the laneway is a dwelling facing Benjamin Street.
- On street parking restrictions are limited to 1 hour on both sides of High Street.
- The site is provided with excellent access to a range of commercial, retail, community services and facilities, employment opportunities and transport facilities. The key locational attributes include:
  - 900 metres to the north – Preston Central Principal Activity Centre.
  - Tram Route 86 on High Street.

- Bus Routes 552 & 553 on High Street.
- 140 metres to the north – Bus route 552 (along Dundas Street).
- 275 metres to south west – Thornbury railway station.
- 890 metres to the north-west – Bell railway station.
- 300 metres to the west – Henderson Park.

### **Proposal**

- The proposal is for a five-storey (plus rooftop terrace and basement) mixed use development comprising:
  - Partial demolition and partial retention of the existing ground floor building.
  - Five (5) dwellings (4 x 2-bedroom and 1 x 3-bedroom).
  - One (1) convenience restaurant (café) (111 square metres).
  - Six (6) car parking spaces are to be provided within a car stacker with access from the existing sealed right of way that abuts the rear of the site. The proposal seeks to waive the car parking requirement for the convenience restaurant (café).
  - Three (3) bicycle parking spaces.
- Each dwelling is orientated with primary views to either the east or west.
- The ground level shop front is to be remodelled to incorporate new glazing, services cupboard, and residential entry for the dwellings above.
- The existing parapet to the first-floor façade facing High Street is to be retained with a balcony located behind. Upper levels have been setback from the front façade and applied with louvers and lightweight cladding in charcoal colours.
- The rear ground floor brick wall will be retained with upper-level balconies provided with planter boxes preventing downward views.
- It is noted that whilst the building is technically described as five (5) stories, the portion of the building that contains the fifth storey is only one room where a mezzanine level has been accommodated and is to the rear of the ground floor. This level/room is not visible externally and it presents as a four (4) storey building. This room allows for better utilisation of the space which otherwise would be a 4.8 metre high waste storage room.

### **Objections summarised.**

- Overshadowing
- (Construction) noise, dust, heavy vehicles, vibration, and congestion
- Overlooking and loss of privacy
- Bulk and scale of the proposal not in keeping with the neighbourhood character
- Commercial use and noise from the use
- Inadequate on-site parking
- Noise from the car stacker and garage door
- Excessive laneway traffic
- Proposal impacts on current residents' human rights to open space and utility
- Limited access to all levels impacting on personal safety of future residents

- Impact of development on adjacent car parking and laneway access during construction
- Excessive height of proposal
- Small apartments with poor internal amenity
- Proposal decreasing property value
- Development damaging adjacent properties
- Proposal is an overdevelopment
- Inappropriate waste collection
- Asbestos removal
- Loss of light

### **Officer comment on summarised objections**

#### Overshadowing, loss of light

This concern has been considered below in the assessment against Clause 22.06 of the Darebin Planning Scheme. It is noted that the proposal is not required to meet the requirements of Clause 55 of the Darebin Planning Scheme in regard to overshadowing as the subject site is located in a Commercial Zone. Nevertheless, overshadowing has been considered when assessing the proposal.

The proposed height results in some overshadowing of the secluded private open space at 1 Benjamin Street at 9am and 10am. The majority of the secluded private open space will however receive adequate sunlight. Overall, given that the impact is largely limited to 9am the proposal is considered to have an acceptable impact on the amenity of the adjoining property. No overshadowing of residential properties occurs after 10am.

#### (Construction) noise, dust, heavy vehicles, vibration, and congestion

Noise and trucks etc. during the construction phase of development is a temporary and unavoidable consequence of development and not a reason to refuse any development. It is noted that there are regulations around noise pollution through the Environmental Protection Authority (EPA). Nevertheless, a construction management plan will be required via condition subject to any approval addressing issues of this kind.

Furthermore, a condition of permit will require the current architect or other suitably qualified person approved by Council to be appointed to oversee the construction phase. This will enable the good will established between the applicant and neighbouring property owners to maximise communication lines and minimise escalation of construction related issues.

#### Overlooking and loss of privacy

These concerns have been considered below in the assessment against Clause 22.06 of the Darebin Planning Scheme. Each have been considered and the proposal is considered appropriate in protecting the privacy of residential properties within the vicinity. Three balconies face the rear and therefore the property at 1 Benjamin Street. Views from the balcony to apartment 1.2 are largely restricted by the existing parapet that is to be retained and obscured glazing which is considered acceptable.

The balconies to apartments 2.2 and 3.1 have been provided with 900mm deep elevated garden beds at the edge of the balconies. The elevated garden beds mean that downward views are restricted from the balconies whilst horizontal and upward views are maintained.

This is considered a good outcome which protects the amenity of the adjoining properties to the west whilst also providing adequate internal amenity to the apartments.

Bulk and scale of the proposal not in keeping with the neighbourhood character

The bulk and scale of the proposal have been considered against the relevant aspects of the heritage overlay and the site context and the design response of the proposal is considered acceptable. Council's Heritage Advisor raised no objections to the proposal.

Commercial use and noise from the use

The proposal is located in a commercial zone where ground floor uses such as the proposed convenience restaurant are encouraged. A condition of approval will ensure the proposed ground floor use is limited to the proposed convenience restaurant (Café). Any change in use would need to be considered subsequently to this application.

Inadequate on-site parking, excessive laneway traffic

The proposal has been assessed against the requirements of Clause 52.06 of the Darebin Planning Scheme below. The proposed on-site parking and car parking arrangements utilising an existing laneway are supported. It is noted that Council's traffic engineers encourage the use of existing right of ways for vehicle access to on site carparking when available and have assessed the application and did not object to the proposal.

Noise from the car stacker and garage door

Car stackers are a recognised form of car parking used to meet car parking requirements and have been approved on numerous occasions by Council and the Victorian Civil Administrative Tribunal (VCAT). An acoustic report will ensure noise impacts arising from the car stacker are addressed appropriately.

Conditions of approval will require further attenuation measures and maintenance details so that noise impacts are addressed.

Proposal impacts on current residents' human rights to open space and utility

The impact of the proposal on the private open space of the existing residence to the west has been considered in the detailed assessment below. Any impacts have been assessed and are considered appropriate in line with the requirements of both Clause 22.06 - Multi Residential and Mixed Use Developments and Clause 22.05 – High Street Land Use and Development Corridor. Further, it is noted that human rights are not a specific planning consideration but rather the planning regulations seek to ensure that new development does not have a detrimental impact on the amenity of existing properties. In this case, the application has been assessed as not having an unreasonable impact on the amenity of adjoining properties.

Limited access to all levels impacting on personal safety of future residents

Accessibility has been considered as part of the Clause 58 assessment below. The ground floor entry area of the residences has been appropriately designed to provide for the safety of the residences by reducing any recess from the street. A lift provides all levels with appropriate access.

Impact of development on adjacent car parking and laneway access during construction

It is likely that the right of way will be temporarily closed while work is undertaken to the west elevation (facing the right of way) of the building. As a condition of any permit that may issue, the applicant is required to submit a construction management plan, to the satisfaction of the Responsible Authority outlining the key events of the construction phase, including the expected length of construction and the hours of construction.

Again, a condition of permit will require the current architect or other suitably qualified person to oversee the construction phase maximising communication lines between the applicant and neighbouring property owners and minimising the likelihood of vehicle access issues.

Excessive height of proposal

The height of the proposal has been assessed against the relevant requirements of Clause 22.05 and 22.06 (included later in this report) and is considered acceptable given the location and context of the site on an arterial road in a commercial zone. The proposal has been designed well and is considered to mitigate any amenity impacts that might arise from the additional height of the development.

Small apartments with poor internal amenity

The proposal has been assessed against the requirements of Clause 58 of the Darebin Planning Scheme and complies with the requirements subject to conditions. Clause 58 identifies a number of standards that are designed to protect future residents and provide them with dwellings that have been designed to satisfy many fundamental design attributes to ensure that they are liveable, functional and efficient.

Proposal decreasing property value

Fluctuations in property prices are not a relevant consideration in assessing medium density development under the provisions of the Planning & Environment Act 1987, or the Darebin Planning Scheme.

Development damaging adjacent properties, asbestos removal

Building work can sometimes affect adjoining properties. An owner who is proposing building work has obligations under the Building Act 1993 to protect adjoining properties from potential damage from their work. If building work is close to or adjacent to adjoining property boundaries, then an owner may be required to carry out protection work in respect of that adjoining property. This is to ensure that the adjoining property is not affected or damaged by the proposed building work. Protection work includes but is not limited to underpinning of adjoining property footings, including vertical support, lateral support, protection against variation in earth pressures, ground anchors, and other means of support for the adjoining property. Council should note that this process is not controlled or overseen via planning process and regulations.

All construction activity must comply with Occupational Health and Safety Standards. This is the responsibility of the successful tenderer appointed for the site development and is not managed through the building process rather than the planning process.

Proposal is an overdevelopment

State policies set targets for established areas of Melbourne to absorb a high proportion of Melbourne's expected growth. State and Local Planning Policy envisage an increase in housing density in well serviced areas such as this. While any increase in population density

will likely increase the level of activity around the site and area, it is not envisioned that such an increase would be detrimental or substantially more intensive than what is currently experienced.

The consideration of a mixed-use apartment development is based on its compliance with a set of criteria outlined in the Darebin Planning Scheme and not based on a subjective concern. The Victorian State Government has a clear policy on urban consolidation which is heavily dependent on higher density housing development in locations such as this.

#### Inappropriate waste collection

Waste collection is proposed to take place twice a week in off-peak hours. Further details are provided in the Waste Management Plan that will be associated with any approval of the development.

## **PLANNING ASSESSMENT**

### **Clause 22.05 - High Street Land use and Development corridor**

This policy applies to the land located along the High Street corridor and implements the High Street land use and urban design objectives and strategies in the MSS and the recommendations of the High Street Urban Design Framework and Precinct Guidelines.

The site is located within Precinct 7 of Clause 22.05. Within precinct 7 it is policy:

- *To encourage offices and showrooms at ground floor level, particularly in the area between Flinders Street and Pender Street on the west side of High Street.*
- *To encourage residential development above and behind commercial premises.*
- *To ensure new development is designed to protect the amenity of adjoining residential properties.*

The convenience restaurant furthers the purpose of the Commercial 1 Zone by providing a retail use at ground level and higher residential densities above. The convenience restaurant supports the directions of Clause 21.04-3 (Retail and Commercial Activity) of the Darebin Planning Scheme which seeks to strengthen the role of Darebin's commercial precincts.

#### Height and Front boundary Setback:

Policy encourages an additional two (2) levels above existing buildings with the built form to extend to the front and side boundaries. The existing buildings on site consist of a single storey shop building facing High Street and a double storey to the rear. The development proposes an additional three (3) levels which will result in the building accommodating four (4) levels facing High Street and four (4) levels plus basement to the rear. Whilst the building presents as four (4) storeys to the rear in fact there is internally a small portion (one room) of the building that is five (5) levels. This fifth level is not discernible from outside the building. The existing parapet/street wall is to be retained in accordance with the heritage overlay. The first and second levels are appropriately setback from the front and rear heritage fabrics with the top-level setback further to generate visual recess ensuring an appropriate heritage response. The upper levels have been provided with dark louvers and cladding appropriately contrasting the heritage fabric.

Overall, it is considered that the proposed height is appropriate given the excellent location of the development site allowing for higher densities with good amenity for the future residents. At the same time the amenity of the adjoining properties can be protected noting some minor impacts are unavoidable. However, the proposal has appropriately considered amenity

impacts. Additionally, the proposal has been considered by Council’s City Designer and Heritage Advisor who both supported the proposed height and built form.

**Clause 22.06 - Multi residential and mixed-use Development**

This policy applies to the consideration of multi-apartment developments.

Element	Comment	Compliance
<b>Sustainability</b>	<p>A Sustainable Design Assessment (SDA) has been prepared. The key sustainable design initiatives are:</p> <ul style="list-style-type: none"> <li>- Average 6.1-star rating</li> <li>- A water tank for rainwater harvesting system for toilet flushing and irrigation.</li> <li>- Cross flow ventilation.</li> <li>- Provision of bike parking (3 spaces).</li> </ul>	Complies subject to condition
<b>Design &amp; Materials</b>	<p>The proposal involves the retention and remodelling of parts of the existing façade. The new shop front includes new glazing treatments, services cupboards, and pedestrian access. The glazing format, along with the brick and shutters to the shopfront provide interest to the street front and is supported.</p> <p>The existing parapet to the street is integrated into the design and utilised as a balustrade to the first-floor balcony. All upper storeys have a consolidated setback of 3.0 metres. This approach to the design of the front form is supported, avoiding a ‘tiered’ profile whilst providing an appropriate setback response to the heritage fabric.</p> <p>The building envelope is proposed to rise sheer on the side boundaries to a height of the balustrade of the rooftop terrace above the fourth floor. This arrangement would not prejudice the form of development on the adjacent sites and generally facilitates an equitable approach to development.</p> <p>The proposed materials palette consists primarily of metal cladding (standing seam), concrete block work and glazing and is supported.</p>	Complies
<b>Building Height</b>	<p>A consideration of height requires a balanced consideration of all the related policy drivers such as housing diversity, affordability, and urban consolidation. Any discussion of height should be balanced against the design and massing of the building and its response to the immediate context.</p> <p>The State planning policy framework generally encourages Melbourne to become a more compact city by accommodating a substantial portion of its future household growth within its established urban areas. Activity centres, strategic redevelopment sites and locations proximate to public transport are the preferred locations for new residential development, which is encouraged to comprise an intensive scale and built form.</p> <p>At a local planning level, the intent to change this part of High Street has strategic support. Policy encourages new development up to two levels above existing buildings. In this case the existing building is part one storey in height and part</p>	Complies with objectives

Element	Comment	Compliance
	<p>two storey. While the development proposes an additional three levels due to the one and two storey height of the adjoining properties, the height of development extending beyond the height of the properties to the north equates to an additional two storeys. The upper storeys are proposed with a consolidated front setback of 3.0 metres. This approach to the design of the front elevations is supported, avoiding a ‘tiered’ profile. The upper storeys have also been appropriately setback from the rear boundary giving it a more moderated built form. In addition to this the existing laneway to the west of the subject site further reduces visual bulk towards the residences to the west.</p>	
<b>Apartment diversity</b>	<p>The proposal provides five (5) dwellings, including four (4) 2-bedroom and one (1) 3-bedroom dwellings. The mix of dwelling sizes and layouts is supported.</p>	Complies
<b>Parking and vehicle access</b>	<p>Refer to the Clause 52.06 assessment below.</p>	Complies subject to condition
<b>Street address</b>	<p>The retail tenancy maintains an active frontage to High Street. The residential entrance is adequately visible. Services are integrated into the façade. The retention of the awning maintains weather protection on High Street.</p>	Complies
<b>Amenity Impacts Including Overshadowing and Overlooking</b>	<p>The site is located within a commercial precinct relatively isolated from adjacent sensitive interfaces.</p> <p>Matters of overlooking and overshadowing are considered to have been addressed appropriately.</p> <p><b>Overshadowing</b></p> <p>It is noted that the proposal is not required to meet the requirements of Clause 55 of the Darebin Planning Scheme in regard to overshadowing as the subject site is located in a Commercial Zone. Nevertheless, overshadowing has been considered when assessing the proposal.</p> <p>The additional height of the proposal results in overshadowing of the secluded private open space at 1 Benjamin Street at 9am and 10am. Whilst an extensive area of the secluded private open space will be overshadowed at 9am the additional shadow at 10am only affects a small area along the eastern part of the property. The majority of the secluded private open space will receive adequate sunlight. Overall, given that the impact is largely limited to 9am the proposal is considered to have an acceptable impact on the amenity of the adjoining property.</p> <p>No overshadowing of residential properties occurs after 10am.</p> <p><b>Overlooking</b></p> <p>Three balconies with adjacent living areas and bedrooms face the rear and therefore the property at 1 Benjamin Street. Views from the balcony to apartment 1.2 are largely restricted by the existing parapet that is to be retained. Where the parapet is below 1.7 metres above the floor level of the balcony obscured glazing has been provided to restrict any</p>	Complies subject to condition



Element	Comment	Compliance
	<p>views, which is considered acceptable.</p> <p>The balconies to apartments 2.2 and 3.1 have been provided with 900mm deep elevated garden beds at the edge of the balconies. The elevated garden beds mean that downward views are restricted from the balconies whilst horizontal and upward views are maintained. This is considered a good outcome which protects the amenity of the adjoining properties to the west whilst also providing adequate internal amenity to the apartments.</p> <p>It is noted that the height of the garden beds has not been nominated on the plans. A condition of approval will require this dimension to be shown to ensure overlooking is addressed appropriately by the elevated garden beds.</p>	
<p><b>On-Site Amenity and Facilities, including Private Open Space</b></p>	<p>The proposal achieves a satisfactory response to Clause 58 (Apartment Developments):</p> <ul style="list-style-type: none"> <li>• A variety of well-proportioned layouts.</li> <li>• Orientation of the apartments to face the front and rear.</li> <li>• Floor to ceiling heights of 3.0 metres.</li> <li>• Provision of bicycle parking.</li> <li>• Excellent cross ventilation.</li> <li>• Sufficient storage provision.</li> </ul> <p>Some very minor aspects of Clause 58 are not met. One relates to the minimum dimension of one of the private open space areas which is 0.03m less than the dimension required. The minimum dimension of 2.37 metres is of minimal discrepancy to the requirement of 2.4 metres. However, given the generous overall area of the main balcony space and the provision of an additional balcony to the rear adjacent to bedrooms, the minimal discrepancy is considered acceptable. The other non compliance is that the living room layout of one dwelling is to measure 3.59m compared to the preferred 3.6m dimension (0.01m less than the required dimension). Most of the development far exceeds the minimal standards. Overall, the proposal is deemed to be a very good housing product. Refer to the Clause 58 assessment below for further details.</p>	<p>Complies subject to condition</p>
<p><b>Waste Management</b></p>	<p>The application is accompanied by a waste management plan detailing the number and size of bins required to service the development and the times, frequency and means of waste collection. The waste storage area appears to provide sufficient space to also provide for FOGO bins in the future.</p> <p>A condition of approval will require further details how traffic is managed during collection times.</p>	<p>Complies subject to condition</p>
<p><b>Equitable Access</b></p>	<p>All levels are provided with lift access.</p>	<p>Complies</p>

**Clause 43.01 - Heritage Overlay:**

The site forms part of the heritage precinct of High Street Thornbury where a Heritage Overlay (Schedule 309) applies. The precinct comprises the properties at 732-848 & 825-927 High Street, Thornbury. Contributory places include the early to mid-twentieth century shops and residences, which comprise the majority of buildings within the precinct. The consistency of scale, form and detailing of the shops within the centre and the extent to which development in one main period is apparent are important characteristics. Places of individual local significance, which have their own citations in this Study, include Collins Corner at No.774, the Drive-in garage at No.802, the shop row at 804-10, the former Regent Theatre at No.851-9, the former Anderson's Furniture Warehouse at No.885, and the shops and residences at No.844-46. The precinct as a whole is of local historic significance to Darebin City.

The decision guidelines contained under the Heritage Overlay require a contextual consideration of the location, bulk, form, and appearance of the proposed buildings. Clause 22.05 (High Street Corridor) of the Darebin planning Scheme encourages re-development of properties for mixed use apartment development within the heritage precinct. At a broader State and local policy level there is support to re-develop sites with good access to public transport, shops, and services (sustainable development).

The site benefits from a number of physical and locational attributes and planning scheme objectives. The site's size, its main road location, its access to tram services and shops are all factors which indicate that a development of the type and scale sought is appropriate.

While acknowledging the site's inclusion in a Heritage Overlay, it is considered that the site's circumstances and physical and policy context do not preclude appropriate sensitive higher density redevelopment.

- The height of the development will be read in the context of other higher density forms that can be expected to the north and south of the site.
- The design approach retains a strong base comprising parts of the original façade with recessed upper levels, providing separation from the heritage façade.
- The upper two storeys are proposed with a consolidated front setback of 3.0 metres. This approach to the design of the front elevations is supported, avoiding a 'tiered' profile, whilst complying with the minimum setback depth sought by policy.
- The materials palette provides visual interest and does not undermine the heritage elements to be retained.
- The heritage façade has been respectfully remodelled and integrated into the overall architectural language of the proposal.
- The rear ROW provides a point of demarcation between the dwellings located to the west and the development.
- Council's Heritage Advisor has raised no objection to the proposed development.

It is considered that the proposal achieves a well resolved design outcome which balances the aims and objectives of the heritage overlay while achieving an increased housing density in a mixed-use building format, as sought by policy.

**Clause 58 - Apartment Developments:**

Std	Comment	Compliance	
		Std	Obj
<b>D1</b>	<b>Urban Context</b>		
	<p>The proposal appropriately responds to the features of the site and the surrounding context. Consideration of the site context has determined appropriate interface treatments, siting, scale, and height.</p> <p>At five (5) storeys (however presenting as four (4) storeys ) plus the rooftop terrace it represents a good fit within the existing and emerging streetscape as sought by policy.</p> <p>The side and rear setbacks provide an appropriate transition in response to considerations of equitable development and amenity.</p> <p>The architectural detailing comprising a restrained palette of materials and clean lines is appropriate to the heritage context.</p>	Y	Y
<b>D2</b>	<b>Residential policy</b>		
	<p>The proposal furthers the objectives of relevant State and Local planning policy outlined in the Darebin Planning Scheme, by providing a mixed-use development in an existing commercial area with excellent access to public transport, retail shopping, services and employment opportunities.</p>	Y	Y
<b>D3</b>	<b>Dwelling diversity</b>		
	<p>Not applicable. This standard only applies to developments of ten or more dwellings.</p> <p>Nevertheless, it is noted that the proposal provides five (5) dwellings, including four (4) 2-bedroom and one (1) 3-bedroom dwellings. The mix of dwelling sizes and layouts is a positive aspect of this proposal.</p>	N/A	N/A
<b>D4</b>	<b>Infrastructure</b>		
	<p>Adequate infrastructure exists to support new development.</p>	Y	Y
<b>D5</b>	<b>Integration with the street</b>		
	<p>The retail tenancy maintains an active frontage to High Street. The residential entrance is adequately visible. Services are integrated into the façade.</p>	Y	Y
<b>D6</b>	<b>Energy Efficiency</b>		
	<p>Given the orientation of the subject site the development has been designed to maximise east- and west-facing dwellings and balconies. A central light court allowing for northern exposure has been provided. Habitable rooms have been designed with windows offering good access to daylight, sunlight, and ventilation. Battle-axe bedroom have been avoided.</p> <p>A Sustainable Design Assessment (SDA) has been prepared and was deemed appropriate by Council's ESD Officer. The key sustainable design indicatives are:</p> <ul style="list-style-type: none"> <li>- Average 6.1-star rating</li> <li>- A water tank for rainwater harvesting system for toilet flushing and irrigation.</li> </ul>	Y	Y

Std	Comment	Compliance	
	<ul style="list-style-type: none"> <li>- Crossflow ventilation.</li> <li>- Provision of bike parking (3 spaces).</li> </ul>		
<b>D7</b>	<b>Communal open space</b>		
	<p>Not applicable.</p> <p>Nevertheless, it is noted that the proposal comprises communal open space to the rooftop terrace facing High Street. This is considered a good usage of the roof space and provides good amenity for the residents while at the same time protecting the amenity of adjoining properties as only the front part of the roof space facing High Street has been utilised.</p>	N/A	N/A
<b>D8</b>	<b>Solar access to communal outdoor open space</b>		
	<p>Not applicable.</p> <p>Nevertheless, it is noted that the proposed communal outdoor open space would receive good solar access.</p>	N/A	N/A
<b>D9</b>	<b>Safety</b>		
	<p>The development is secure and the creation of unsafe spaces has been avoided. The dwelling entry is adequately located and visible from High Street.</p>	Y	Y
<b>D10</b>	<b>Landscaping</b>		
	<p>The site area is below the threshold to trigger a landscaping requirement.</p>	N/A	N/A
<b>D11</b>	<b>Access</b>		
	<p>Vehicle access is provided via the ROW as encouraged by policy.</p>	Y	Y
<b>D12</b>	<b>Parking location</b>		
	<p>Parking facilities are proximate to the dwellings they serve. The car parking area is adequately secure.</p>	Y	Y
<b>D13</b>	<b>Integrated water and stormwater management</b>		
	<p>A water tank is provided underneath the driveway to the rear for irrigation and flushing of selected toilets.</p>	Y	Y
<b>D14</b>	<b>Building setback</b>		
	<p>The proposal is well considered in regard to built form and setbacks. The development maintains parts of the existing façade to High Street with the additional levels setback 3.0 metres from High Street in accordance with policy.</p> <p>The side and rear setbacks provide an appropriate transition to adjacent properties and respond to considerations of equitable development and amenity.</p>	Y	Y
<b>D15</b>	<b>Internal views</b>		
	<p>The dwellings are designed and located to ensure no unreasonable internal overlooking is provided.</p>	Y	Y

<b>D16</b>	<b>Noise impacts</b>		
	All plant and equipment is appropriately located within the basement or on the rooftop area to avoid any unreasonable loss of amenity to occupiers. Services fronting High Street are integrated into the facade to reduce their visibility. The lift well is located away from all but two adjoining bedrooms. These bedrooms have been provided with specific acoustic walls to mitigate noise issues arising from the operation of the lift.	Y	Y
<b>D17</b>	<b>Accessibility</b>		
	This standard recommends that 50% of the dwellings achieve accessibility standards in relation to corridor, bathroom and bedroom dimensions.  All dwellings achieve this requirement.	Y	Y
<b>D18</b>	<b>Building entry</b>		
	The residential entry to High Street is appropriate to the scale of the building. The corridor width of 1.5 metres from the entry to the lift is considered appropriate and convenient to move items and bicycles through this space. The entry is recessed and is distinguishable from the shop front.	Y	Y
<b>D19</b>	<b>Private open space</b>		
	The proposal provides five (5) dwellings, including four (4) 2-bedroom and one (1) 3-bedroom dwellings.  Apartment 1.1 provides a 20.0 square metre balcony with a dimension of 2.63 metres, which is in excess of this standard.  Apartment 1.2 provides a 16.5 square metre balcony with a dimension of approx. 2.4 metres. The dimension has not been nominated on the plans and therefore a condition of approval will require confirmation of the dimension to be shown achieving a minimum of 2 metres as per the requirements of the standard.  Apartment 2.1 provides a 9.5 square metre balcony with a dimension of 2.5 metres, which is in excess of this standard.  Apartment 2.2 provides a 21.0 square metre balcony with a dimension of 2.9 metres. It is noted that this includes the area to be utilised for the garden beds. The usable area has a minimum dimension of 2 metres, which complies with the standard.  Apartment 3.1 has a balcony of 17.5 square metres adjacent to the living areas which exceeds the minimum area requirement of 12 square metres for the 3-bedroom dwelling. The minimum dimension of 2.37 metres is of minimal discrepancy to the requirement of 2.4 metres. However, given the generous overall area of the main balcony space and the provision of an additional balcony to the rear adjacent to bedrooms, the minimal discrepancy is considered acceptable.	N	Y

<b>D20</b>	<b>Storage</b>		
	<p>Each dwelling should have convenient access to a total of 14 and 18 cubic metres of storage space (inclusive of 9 and 12 cubic metres of internal storage) for 2- and 3-bedroom accommodation, respectively.</p> <p>Each dwelling is provided with 5 cubic metres of external storage space within the ground floor. Each dwelling is provided with 12 to 22 cubic metres of internal storage space. The total volume of the storage areas exceeds the standard.</p>	Y	Y
<b>D21</b>	<b>Common property</b>		
	Common property areas are appropriately designed and delineated.	Y	Y
<b>D22</b>	<b>Site services</b>		
	Sufficient areas for site services are provided. Site services are appropriately designed and integrated into the development.	Y	Y
<b>D23</b>	<b>Waste and Recycling</b>		
	Residential and commercial waste is to be collected via a private waste services provider. The waste storage space is centrally located within the building and away from the street frontages. Waste is to be collected once a week per waste stream equating to two collections per week. A condition will be placed on the permit requiring provision of Food and Organic Waste bins as well.	Y	Y
<b>D24</b>	<b>Functional layout</b>		
	<p>The standard recommends bedroom dimensions of 3.0 x 3.4 metres for a main bedroom and 3.0 x 3.0 metres for all other bedrooms.</p> <p>All dwellings meet the relevant minimum bedroom dimensions.</p> <p>The living areas of apartments 1.1 and 1.2 have a minimum dimension of 3.6 metres and provide for an area of 13 square metres.</p> <p>The minimum dimension for the living area of apartment 2.2 is shown at 3.59 metres which is marginally below the 3.6 metres as per the requirement of the standard. This discrepancy is considered acceptable as it is minor and the discrepancy does not impact on the amenity of the residents. In addition, there is scope to extent the living area towards the east to achieve compliance. The living area size provided is 13 square metres.</p> <p>The living areas to apartments 2.1 and 3.1 have been shown with the minimum area (13 and 16 square metres respectively) and minimum dimension of 3.6 metres as per the requirements of the standard. However, both areas are not clear of the dining areas with chairs encroaching into the living area space. It appears though that there is scope to relocate the dining tables within the apartments to provide the living area clear of the dining area. A condition of approval will require this to be shown on the plans.</p>	N	Y
<b>D25</b>	<b>Room depth</b>		
	The floor to ceiling heights are 3.0 metres throughout as encouraged under this standard. The depths of the single aspect open plan living areas of up to 8.0 metres complies with the requirements of the standard which allow for a maximum depth of 9.0 metres given the layout of the living spaces. Therefore, the proposal ensures more than adequate daylight is provided to the living spaces.	Y	Y

<b>D26</b>	<b>Windows</b>		
	All living rooms and bedrooms have direct access to daylight. No battle-axe bedrooms are proposed.	Y	Y
<b>D27</b>	<b>Natural ventilation</b>		
	All dwellings at all levels are provided with natural ventilation.	Y	Y

**Clause 52.06 Car Parking**

Number of Parking Spaces Required

It is noted that the ground floor plan includes notations not clearly identifying the ground floor use but nominating it as retail/café. Retail premises as per the Darebin Planning Scheme includes the use of the site for the purpose of a convenience restaurant (café), but also includes numerous other uses with varying car parking requirements. In order to properly assess the car parking requirements, the proposed ground floor use has been considered as a convenience restaurant (café) and a condition of approval will require the ground floor use to be identified as a convenience restaurant only.

The subject site is located within the Principal Public Transport Network Area (PPTNA) and in accordance with the above has the following car parking requirements as set out under Clause 52.06-5 of the Darebin Planning Scheme:

Use	Rate	Provision	Requirement
Dwelling	1 to each - & 2-bedroom dwelling 2 to each 3+ bedroom dwelling	4 2	4 2
Visitors	Zero for sites within the PPTNA	0	0
Convenience restaurant (café)	3.5 spaces to each 100m <sup>2</sup> leasable floor area	0	3
<b>Total</b>		<b>6</b>	<b>9</b>

Clause 52.06 recommends the provision of nine (9) car parking spaces. The development proposes a total of six (6) spaces resulting in a shortfall of three (3) car spaces. It is noted that the Traffic Impact Assessment provided by the applicant refers to an older version of the development plans which only provided for two (2) car parking spaces. Since then, the proposal has been amended and provides for six (6) car parking spaces.

The overall reduction is considered acceptable for the following reasons:

- The provision for six (6) on site car parking spaces is to be distributed as follows:
  - One (1) space to each of the two (2) bedroom dwellings
  - Two (2) spaces to the three (3) bedroom dwelling
  - Zero (0) spaces for the convenience restaurant (café)
- The site has excellent access to public transport infrastructure, including trains, trams and buses, and the use of public transport is considered a viable alternative to private motor vehicle use in this location.
- Based upon the provisions of the Scheme, the site has an existing parking requirement of nine (9) parking spaces. Zero (0) on-site parking spaces are currently

provided; thus, the existing site has a shortfall of nine (9) parking spaces against the requirement of the Scheme. The current proposal is seeking a three (3) space shortfall against the requirements of the Scheme.

- The sites proximity to public transport, 900 metres to the Preston Activity Centre and walking and cycling routes justifies a reduction of parking.
- The provision of no staff spaces for the convenience restaurant tenancy is considered appropriate given the small size of the tenancy.
- Any overspill of parking can be readily accommodated within available on-street parking areas and would not be considered to represent any adverse impact upon the amenity of the surrounding area, noting the long-term parking demands associated with residents will be accommodated on-site.
- Adequate bicycle parking is provided on site (see below).

Access and Manoeuvrability

Access to and from the two car stacker platforms accords with B85 of AS2890.1:2004. Vehicles can safely manoeuvre to and from each space.

Mechanical Parking (Car Stackers)

The Klaus Multi Base G63-370 (car stacker) will accommodate the six (6) off-street car parking spaces. This system is a three (3) level double platform stacker with a two (2) level pit. The stacker accommodates car heights of 1.7 metres on the lower and middle level and car heights of 1.9 metres on the upper level resulting in 33% of spaces being able to accommodate vehicle heights of at least 1.8 metres in accordance with Clause 52.06-9. A useable platform width of 4.8 metres is also provided.

Traffic Impact

It can be estimated that the development will generate four (4) vehicle trip ends per dwelling per day. This equates to a daily traffic generation of 20 vehicle trip ends per day for the five (5) dwellings proposed. Typically, 10% of this traffic can be expected in the AM and PM commuter peak hours, which equates to two (2) vehicle trip ends in each peak hour. This level of additional traffic generation is not expected to adversely impact on the operation of the surrounding street network.

**Clause 52.34 - Bicycle Parking**

The recommended provision of bicycle parking is set out under Clause 52.34-3.

Number of Bicycle Spaces Required

Use	Rate		Employee / Resident Requirement	Visitor / Shopper Requirement
	Employee / Resident	Visitor / Shopper		
Dwelling (four or more storeys)	1 resident space to each 5 dwellings	1 visitor space to each 10 dwellings	1	0
Retail	1 to each 300m <sup>2</sup>	1 to each 500m <sup>2</sup>	0	0
<b>Total</b>			<b>1</b>	<b>0</b>



The applicant has proposed to provide three (3) resident bicycle parking spaces within the ground floor adjacent to the corridor. All bicycle parking will be provided as ground-mounted rails thereby complying with Australian Standard 2890.3:2015.

Additionally, a visitor bicycle parking space is proposed on the footpath adjacent to the residential entry.

**REFERRAL SUMMARY**

<b>Department/Authority</b>	<b>Response</b>
Capital Works	No objection, subject to condition included in recommendation.
Climate Emergency and Sustainable Transport Unit	No objection, subject to condition included in recommendation.
ESD Officer	No objection.
City Designer	No objection.
Heritage Advisor	No objection.
City Design Unit	No objection, subject to condition included in recommendation.
Urban Design Unit	No objection.
Property Management Unit	No objection.

**PLANNING SCHEME SUMMARY**

**Darebin Planning Scheme clauses under which a permit is required.**

- Clause 34.01-4 – construct a building.
- Clause 43.01-1 - Partial demolition and construction of buildings and works.
- Clause 52.06-3 – Reduce (including reduce to zero) the number of car parking spaces required under Clause 52.06-5.

**Applicable provisions of the Darebin Planning Scheme**

<b>Section of Scheme</b>	<b>Relevant Clauses</b>
SPPF	11.02, 15.01-1R, 15.02, 15.03, 16.01-1R, 17.02-1, 18.02, 19.03-1
LPPF	21.01-6, 21.02-3, 21.02-4, 21.03-2, 21.04-3, 21.05-2, 21.05-3, 22.05, 22.06
Zone	34.01
Overlay	43.01, 45.06
Particular provisions	52.06, 52.34, 58
General provisions	65.01
Neighbourhood Character Precinct	N/A

**RELATED DOCUMENTS**

- *Planning and Environment Act 1987*
- The Darebin Planning Scheme

**Attachments**

- Aerial Photograph (**Appendix A**) [↓](#)
- Advertised Plans (**Appendix B**) [↓](#)
- Additional shadow diagrams for final assessment (**Appendix C**) [↓](#)

**DISCLOSURE OF INTEREST**

Section 130 of the *Local Government Act 2020* requires members of Council staff and persons engaged under contract to provide advice to Council to disclose any general or material interest in a matter to which the advice relates.

The Officer reviewing this report, having made enquiries with relevant members of staff, reports that no disclosable interests have been raised in relation to this report.



<b>SITE AREA</b>	<b>270 m<sup>2</sup></b>
<b>Existing Ground Floor Area</b>	<b>264.0 m<sup>2</sup></b>

Sheet List	
Sheet Number	Sheet Name
A00	COVER SHEET
A01	AERIAL VIEW
A03	NCS
A04	LAND & FEATURE SURVEY
A05	EXISTING STREETScape ELEVATION
A06	PROPOSED STREETScape ELEVATION
A07	EXISTING CONDITIONS & DEMOLITION
A08	DESIGN RESPONSE
A09	DESIGN RESPONSE
A10	PROPOSED GROUND FLOOR PLAN
A11	PROPOSED FIRST FLOOR PLAN
A12	PROPOSED SECOND FLOOR PLAN
A13	PROPOSED THIRD FLOOR PLAN
A15	SECTIONS
A15a	SECTIONS
A16	PROPOSED ELEVATIONS
A16a	PROPOSED ELEVATIONS
A17	COLOUR & MATERIAL
A18	PROPOSED SHADOW DIAGRAMS

**Project:**  
 Proposed Mixed Use  
 Development  
 893 High Street Thornbury

**Stage:**  
 Town Planning  
 Submission  
 REV2

**Client:**  
 ROBERT  
 CALLIPARI

**Municipality:**  
 Darebin

2020.06.04

<b>Ground Floor Retail Area</b>	<b>92.0 m<sup>2</sup></b>
<b>Mezzanine</b>	<b>30.0m<sup>2</sup></b>

LEVEL 1	
Apartment 1.1	89.2 m <sup>2</sup>
Balcony	20.0 m <sup>2</sup>
Apartment 1.2	94.2 m <sup>2</sup>
Balcony	16.5 m <sup>2</sup>
Light Court	14 m <sup>2</sup>

LEVEL 2	
Apartment 2.1	78.8 m <sup>2</sup>
Balcony	9.5 m <sup>2</sup>
Apartment 2.2	77.0 m <sup>2</sup>
Balcony	21.0 m <sup>2</sup>

LEVEL 3	
Apartment 3.1	142.0 m <sup>2</sup>
Balcony	38.5 m <sup>2</sup>




# 893 High Street Thornbury





SUBJECT SITE

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			<b>LOCATION:</b> 893 High Street Thornbury		<b>SCALE:</b>			
<b>CLIENT:</b> ROBERT CALLIPARI	<b>JOB N°:</b> Project Number							

LEGEND: Approved Document D874/2018 pages 1 of 28

S	SHADE
W	WALL
NS	NATURE STRIP
EC	EXISTING CURBSTONE



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**NCS**



**PROJECT:** Proposed Mixed Use Development  
**LOCATION:** 893 High Street Thornbury  
**CLIENT:** ROBERT CALLIPARI

<b>DATE:</b>	2020.06.04	<b>No in SET:</b>	C. KAIROUZ ARCHITECTS
<b>SCALE:</b>	1 : 100		
<b>JOB N°:</b>	Project Number	<b>A03</b>	

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			<p><b>LOCATION:</b> 893 High Street Thornbury</p>			
			<p><b>CLIENT:</b> ROBERT CALLIPARI</p>	<p><b>JOB N°:</b> Project Number</p>		<p>PO BOX 871, Thornbury Plaza, VIC 3078 (03) 9384 2000 Copyright © C. Kairouz Architects. All rights reserved. permission is necessary for reproduction.</p>
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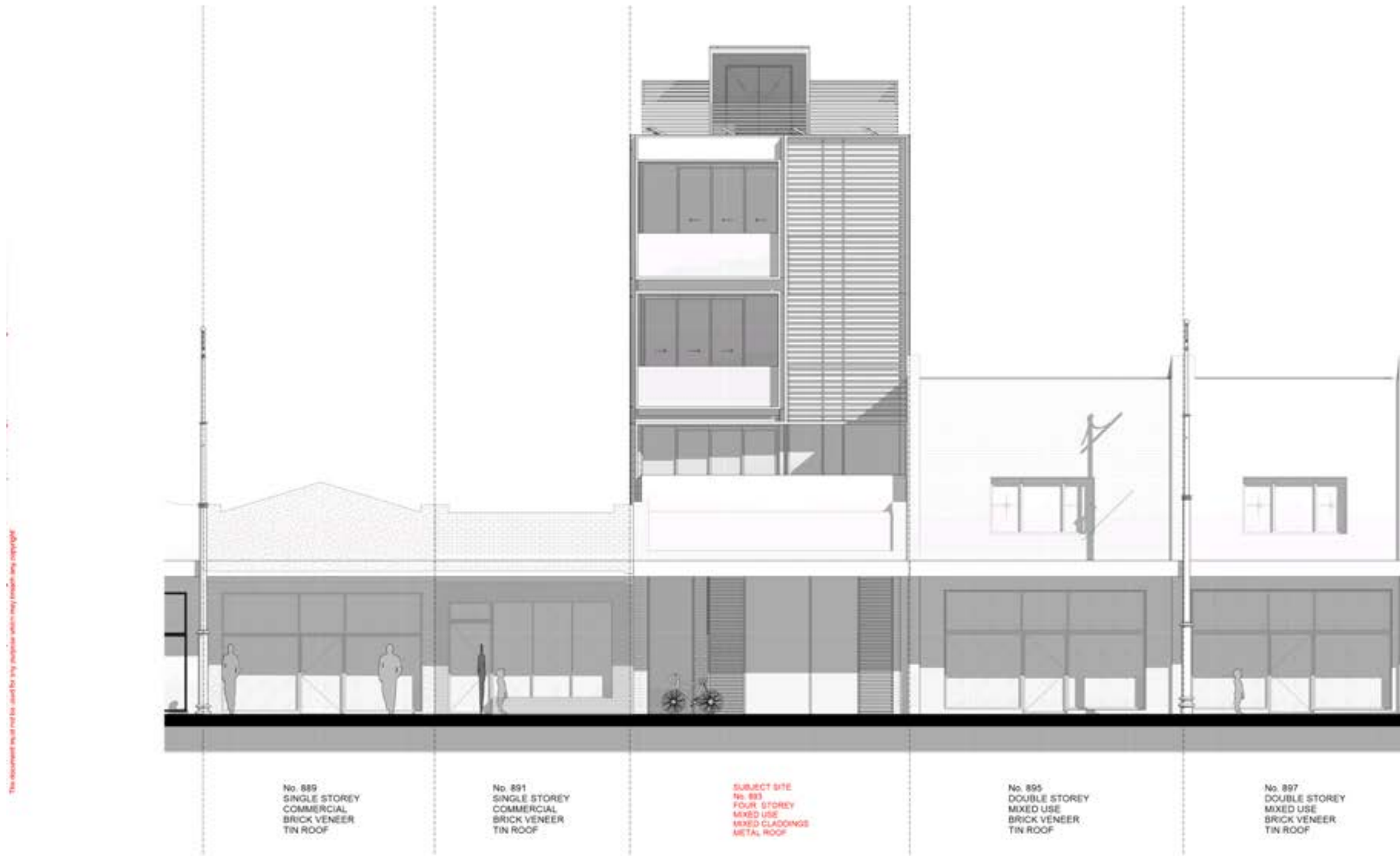
Darwin City Council Received 25/06/2020



HIGH STREET

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		No.		Description	Date										
01	Issue/Revision	21/11/2018													
02	REVISED	21/07/2021													
<p><b>LOCATION:</b> 893 High Street Thornbury</p>	<p><b>SCALE:</b> 1 : 50</p>	<p><b>JOB N°:</b> Project Number</p>	<p><b>C.KAIROUZ ARCHITECTS</b></p> <p><small>Darwin City Council Received 25/06/2020</small></p>												
		<p><b>CLIENT:</b> ROBERT CALLIPARI</p>													





HIGH STREET

No. 889  
SINGLE STOREY  
COMMERCIAL  
BRICK VENEER  
TIN ROOF

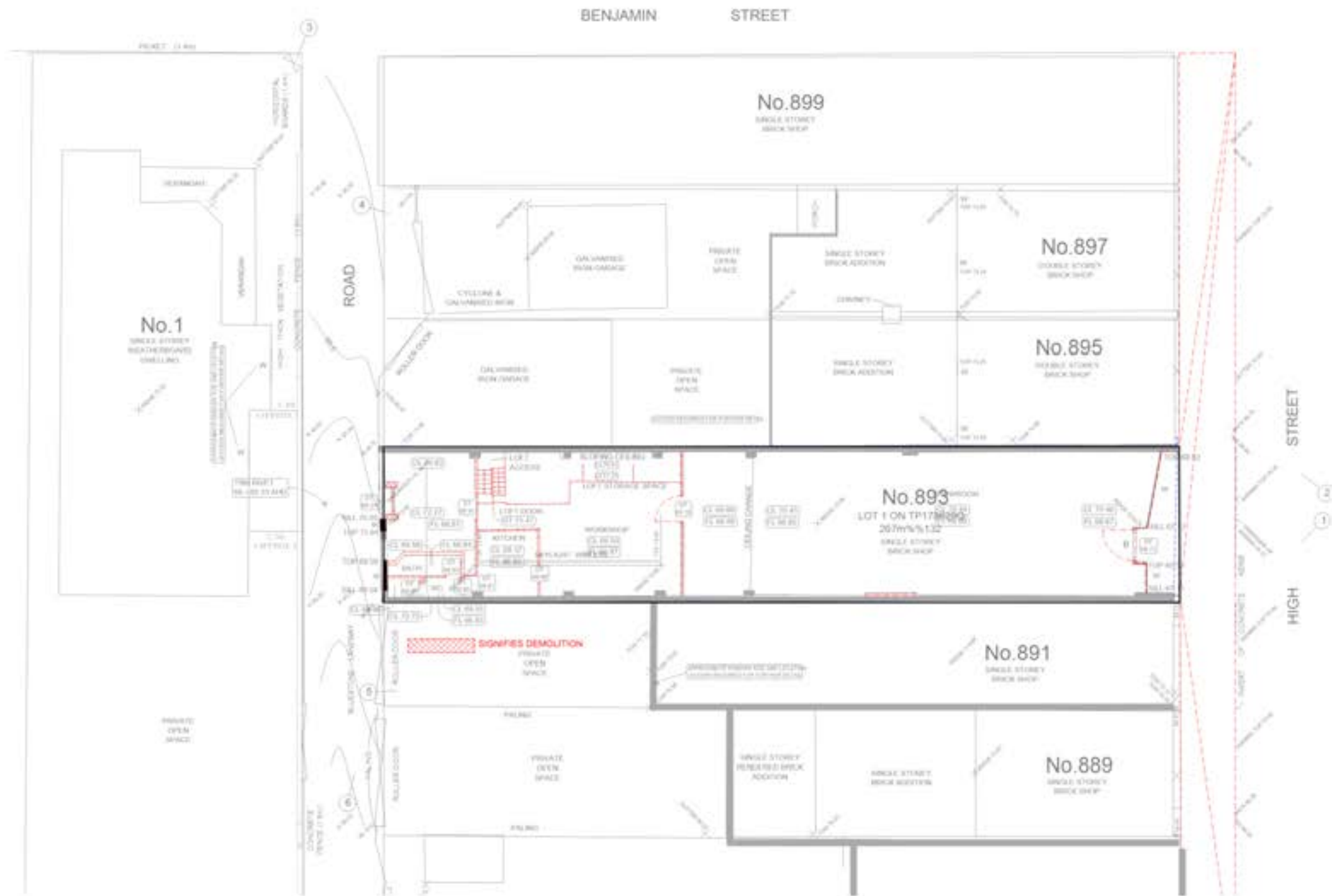
No. 891  
SINGLE STOREY  
COMMERCIAL  
BRICK VENEER  
TIN ROOF

**SUBJECT SITE**  
**No. 893**  
**FOUR STOREY**  
**MIXED USE**  
**MIXED CLADDINGS**  
**METAL ROOF**

No. 895  
DOUBLE STOREY  
MIXED USE  
BRICK VENEER  
TIN ROOF

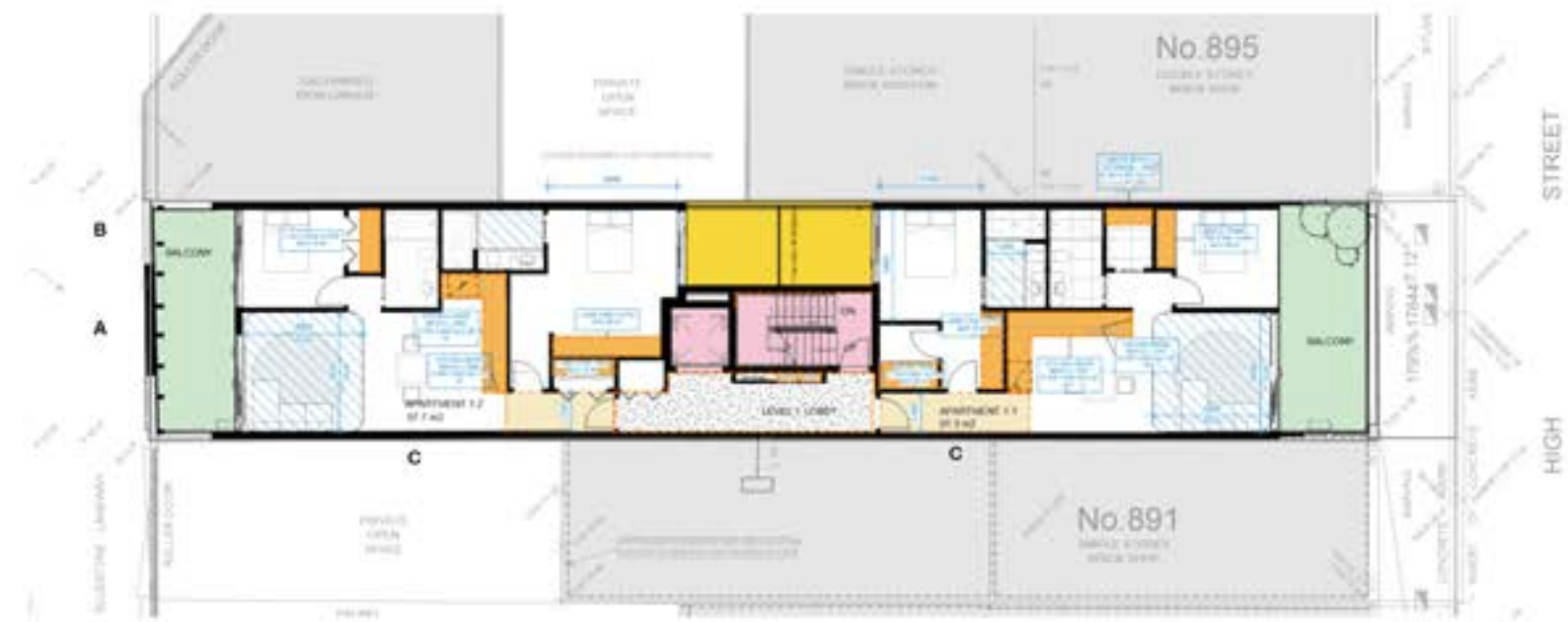
No. 897  
DOUBLE STOREY  
MIXED USE  
BRICK VENEER  
TIN ROOF

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		No.		Description	Date													
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01	Initial Planning Submission	01/11/2018															
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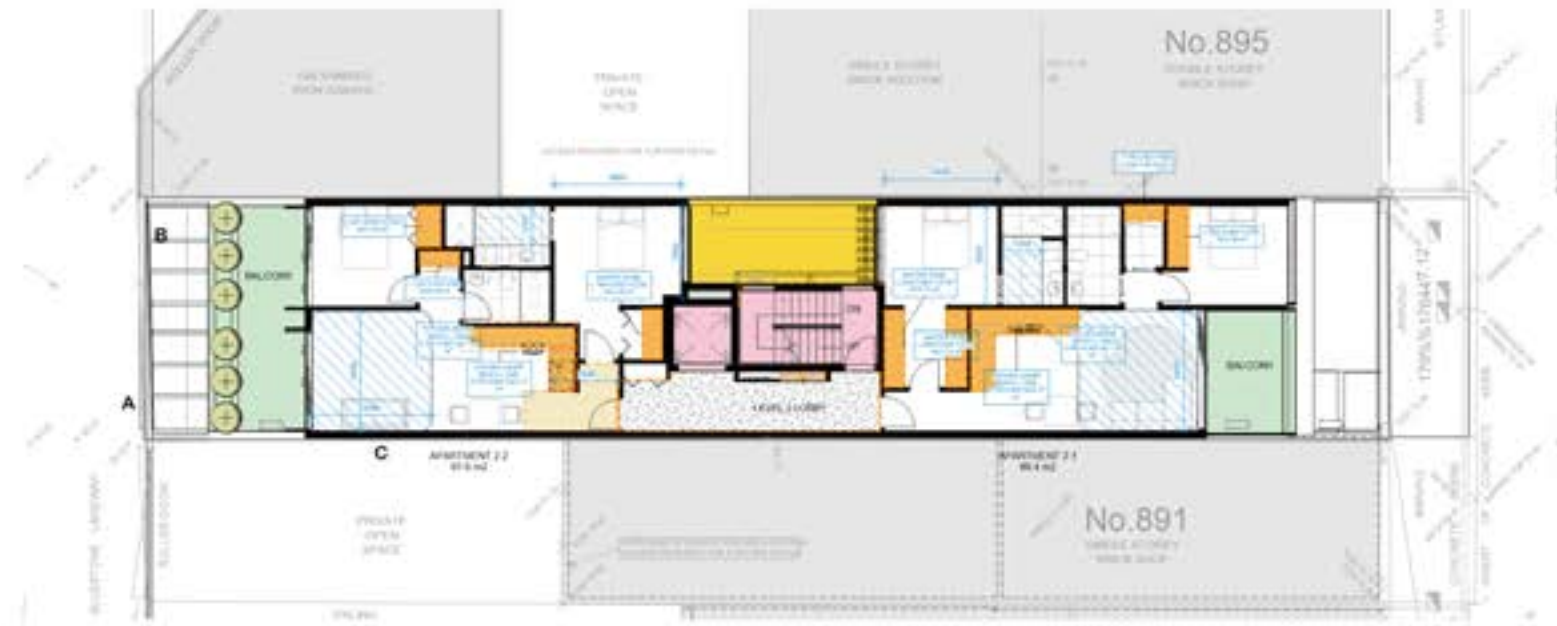


- Dwelling Accessibility
- Built form sufficiently set off rear boundary.
- Overlooking into neighbouring properties minimized by additional landscaping and window screening.
- Standard D17 compliant accessible apartment.
- Sufficient external and internal storage to all apartments.
- Accessible passageway and entries.
- Car and bike parking provided, site is also surrounded by ample public transport mediums.
- Vertical circulation and fire isoalted staircases.
- DDA compliant toilet.
- Private open space provided off main living areas.
- Central light courts provided to all apartments to ensure compliant cross-ventilation.
- Mechanical/services riser.

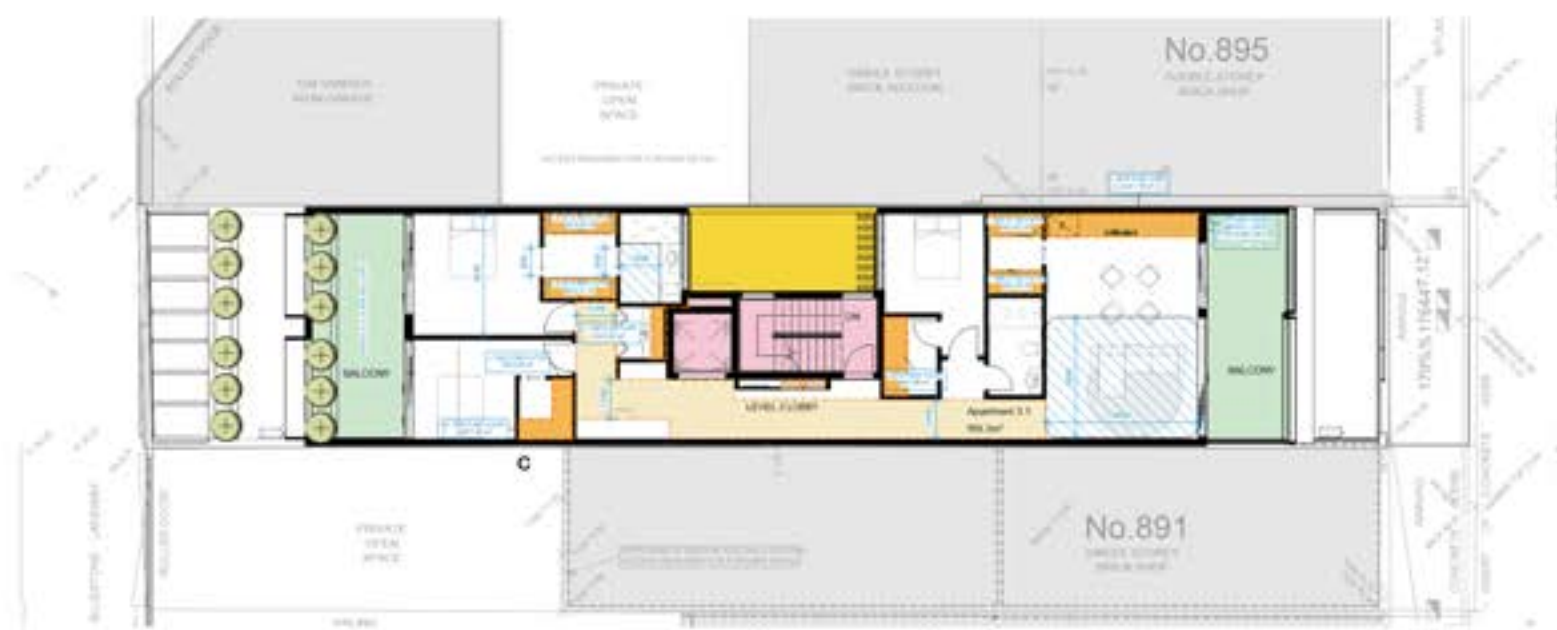
Standard D20			
APT 1.1 Internal Storage	11.03m <sup>2</sup>	APT 1.1 External Storage	0.00m <sup>2</sup>
External Storage	0.00m <sup>2</sup>	External Storage	0.00m <sup>2</sup>
<b>Total</b>	<b>11.03m<sup>2</sup></b>	<b>Total</b>	<b>0.00m<sup>2</sup></b>
APT 1.2 Internal Storage	12.07m <sup>2</sup>	APT 1.2 External Storage	0.00m <sup>2</sup>
External Storage	0.00m <sup>2</sup>	External Storage	0.00m <sup>2</sup>
<b>Total</b>	<b>12.07m<sup>2</sup></b>	<b>Total</b>	<b>0.00m<sup>2</sup></b>
APT 2.1 Internal Storage	11.03m <sup>2</sup>	APT 2.1 External Storage	0.00m <sup>2</sup>
External Storage	0.00m <sup>2</sup>	External Storage	0.00m <sup>2</sup>
<b>Total</b>	<b>11.03m<sup>2</sup></b>	<b>Total</b>	<b>0.00m<sup>2</sup></b>

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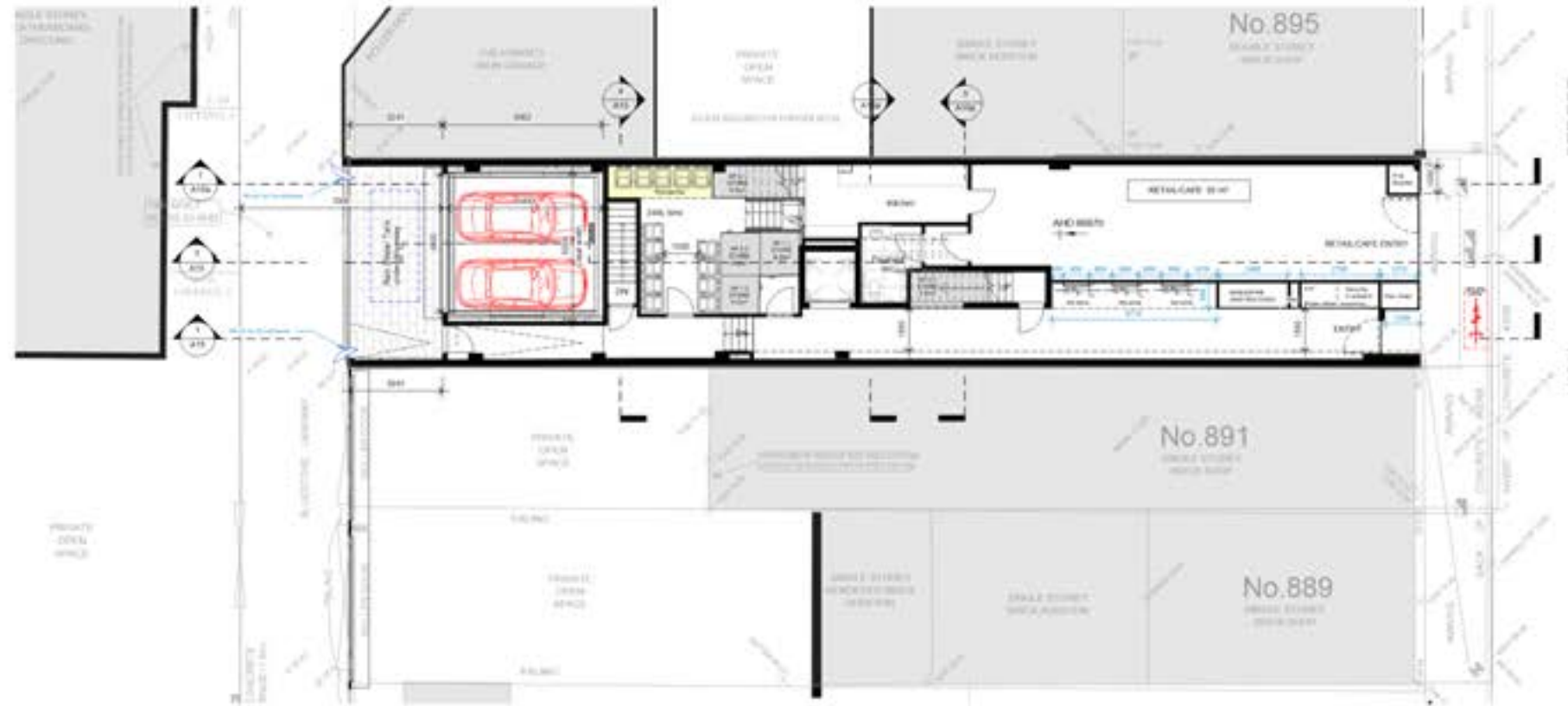


Standard D20			
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External Storage	- 0.00m <sup>2</sup>	External Storage	- 0.00m <sup>2</sup>
Total	- 13.00m <sup>2</sup>	Total	- 16.20m <sup>2</sup>
APT. 1.2 Internal Storage	- 12.00m <sup>2</sup>	APT. 2.2 Internal Storage	- 12.00m <sup>2</sup>
External Storage	- 0.00m <sup>2</sup>	External Storage	- 0.00m <sup>2</sup>
Total	- 12.00m <sup>2</sup>	Total	- 12.00m <sup>2</sup>
APT. 1.3 Internal Storage	- 25.00m <sup>2</sup>		
External Storage	- 0.00m <sup>2</sup>		
Total	- 25.00m <sup>2</sup>		

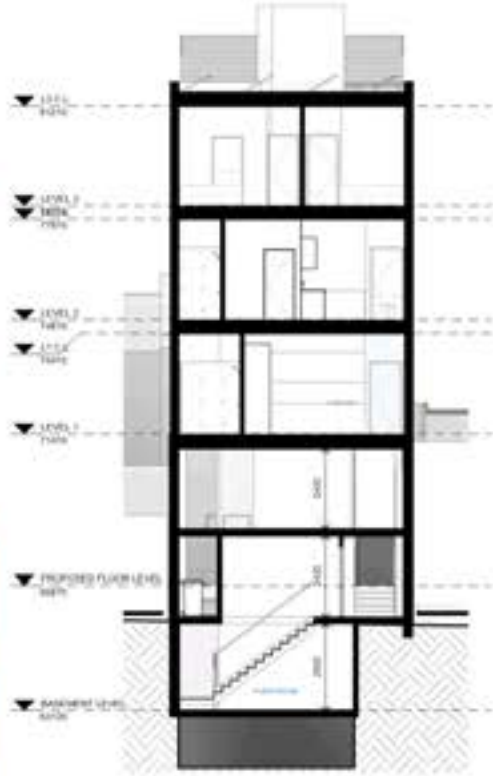
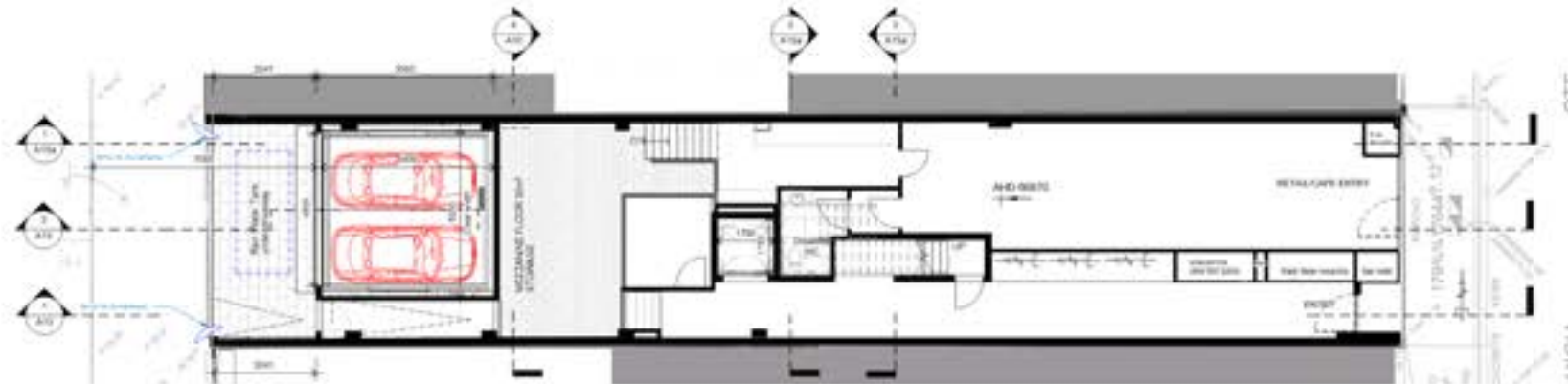
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		No.		Description	Date													
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03	Issue for Construction	12/02/21																
<p><b>LOCATION:</b> 893 High Street Thornbury</p>	<p><b>SCALE:</b> As indicated</p>	<p><b>C.KAIROUZ ARCHITECTS</b></p> <p>Darwin City Council Received 25/06/2021</p>																
<p><b>CLIENT:</b> ROBERT CALLIPARI</p>	<p><b>JOB N°:</b> Project Number</p>																	

3 BASEMENT LEVEL  
1:100



2 PROPOSED GROUND FLOOR PLAN - mezzanine  
1:100



Section 7  
1:100

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			<p><b>CLIENT:</b> ROBERT CALLIPARI</p>																







1 PROPOSED THIRD LEVEL  
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2 PROPOSED ROOFTOP ENTRY PLAN.  
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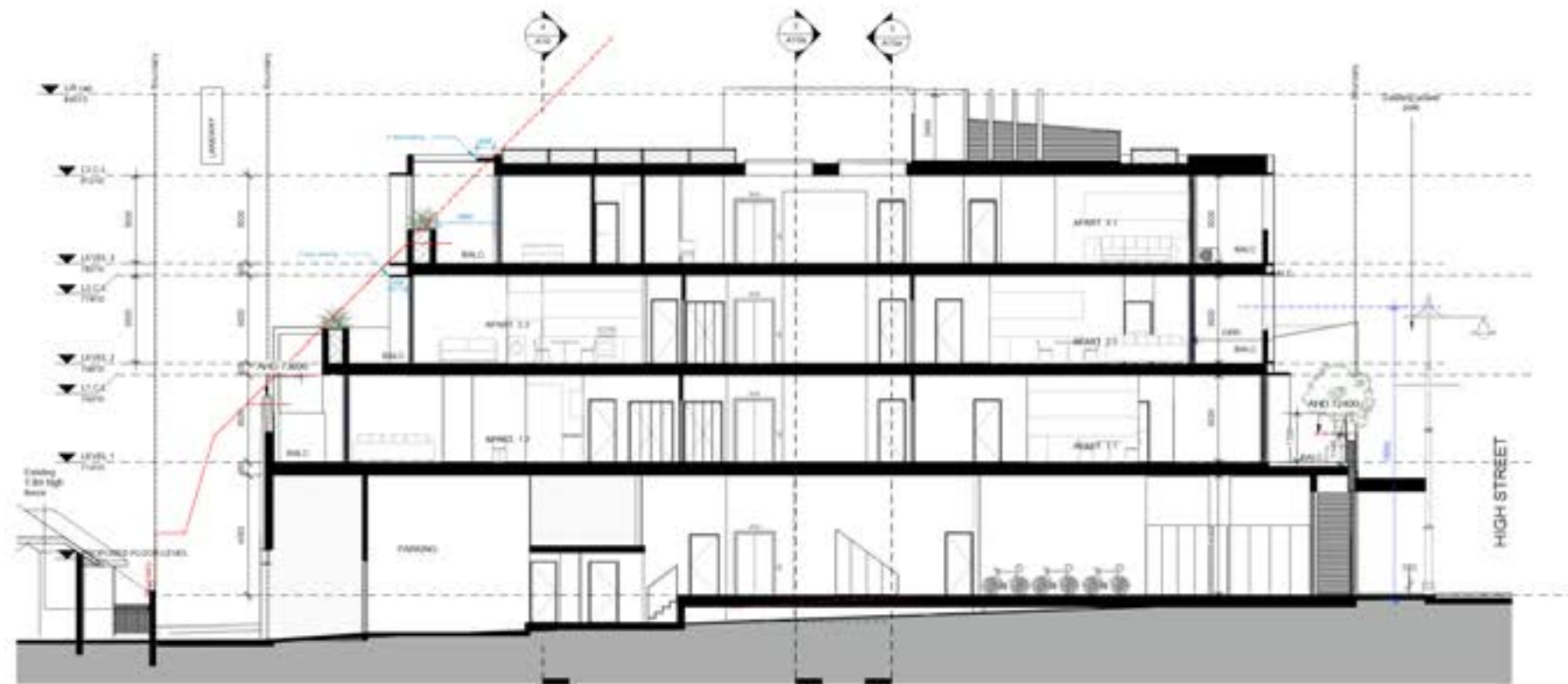
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External Storage	3.20m <sup>2</sup>	External Storage	3.00m <sup>2</sup>
<b>Total</b>	<b>16.20m<sup>2</sup></b>	<b>Total</b>	<b>17.20m<sup>2</sup></b>
APT. 1.2 Internal Storage	12.00m <sup>2</sup>	APT. 2.2 Internal Storage	12.00m <sup>2</sup>
External Storage	5.00m <sup>2</sup>	External Storage	4.00m <sup>2</sup>
<b>Total</b>	<b>17.00m<sup>2</sup></b>	<b>Total</b>	<b>16.00m<sup>2</sup></b>
APT. 3.1 Internal Storage	23.00m <sup>2</sup>		
External Storage	4.00m <sup>2</sup>		
<b>Total</b>	<b>27.00m<sup>2</sup></b>		

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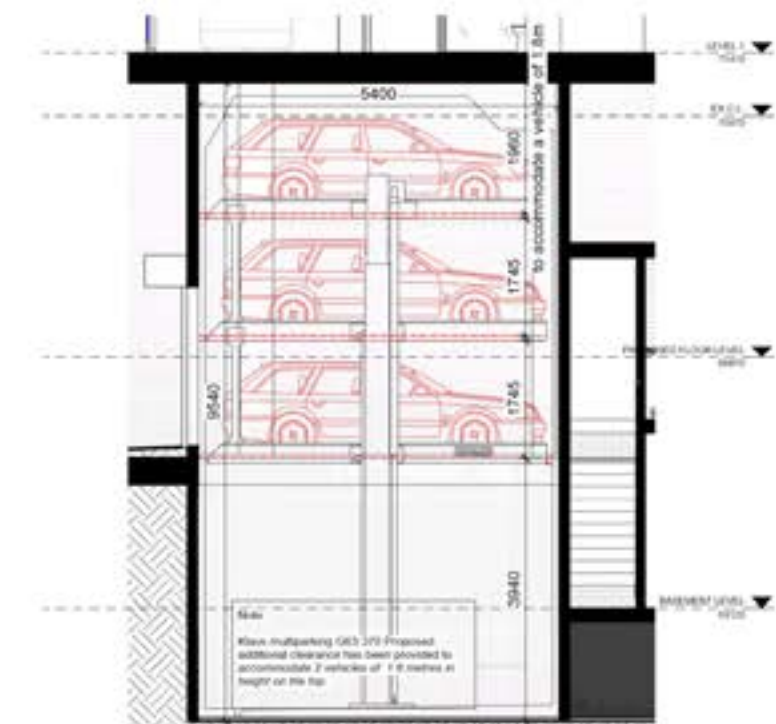
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2 Overlooking Study  
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3 Section B  
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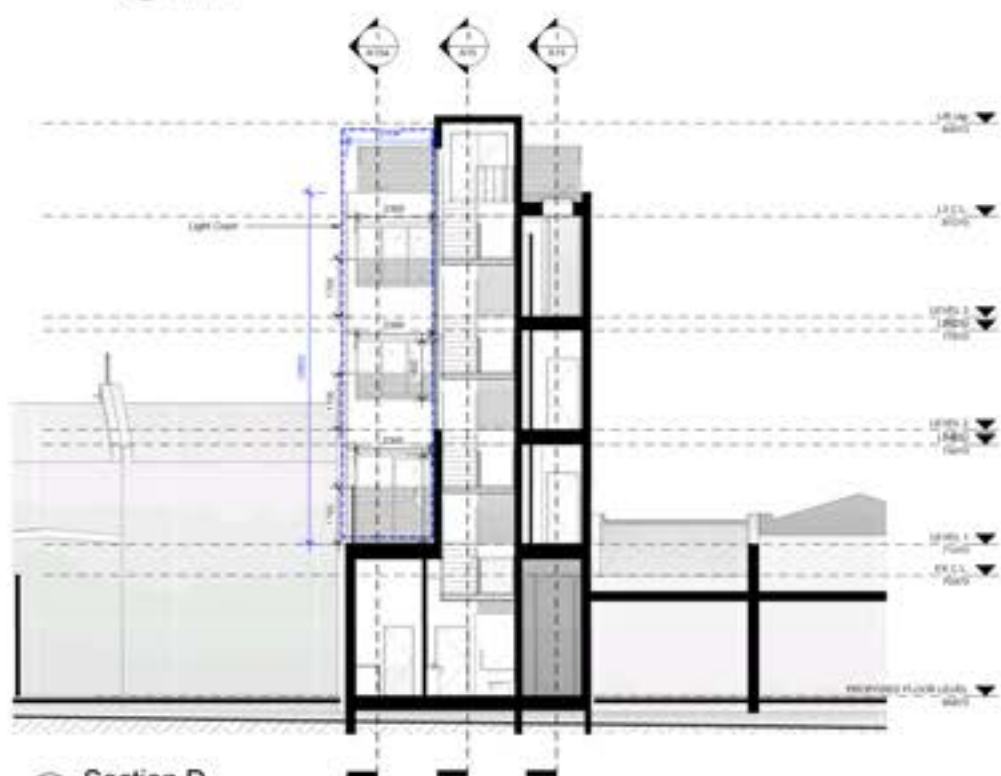
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1 A10 Section C  
1 : 100



2 A10 Section D  
1 : 100



3 A10 Section E  
1 : 100

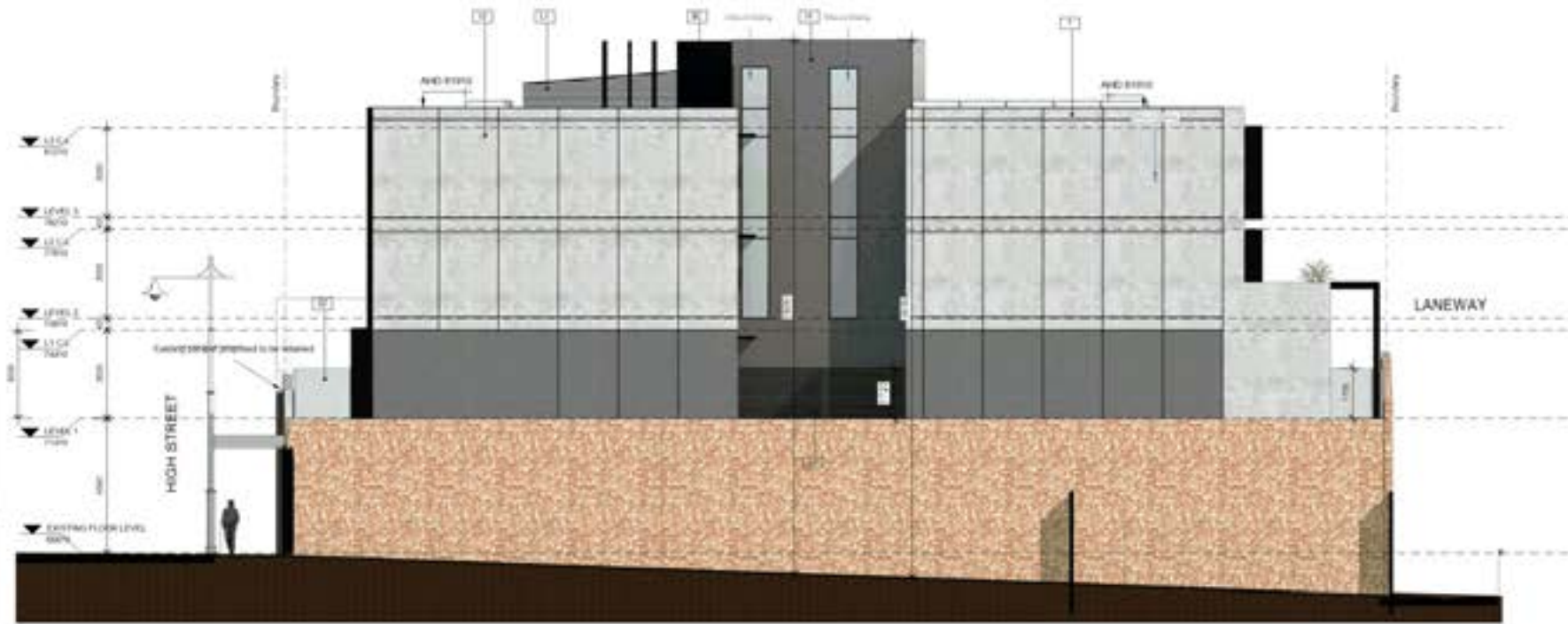
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NORTH ELEVATION

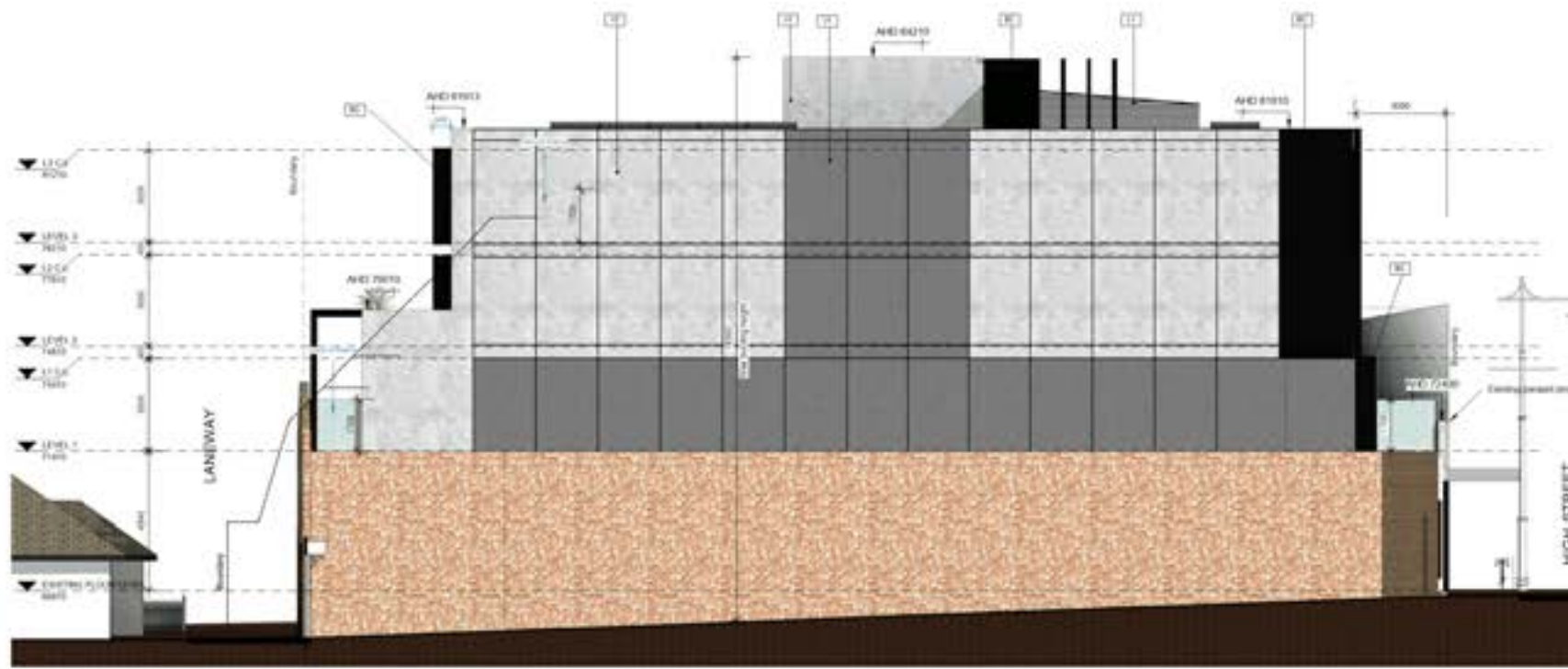


EAST ELEVATION



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		<p><b>CLIENT:</b> ROBERT CALLIPARI</p>				<p>Darwin City Council Received 25/06/2020</p>												



SOUTH ELEVATION



WEST ELEVATION



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		<b>CLIENT:</b> ROBERT CALLIPARI					



Code	Description	Image
AF	Aluminium Framing - Dulux Satin Charcoal	
AS	ALUMINIUM LOUVERS IN DARK GREY	
BA	POWDER COATED ALUMINIUM-Charcoal	
BC	Colorbond -Charcoal	
G2	Clear Glazing	

Code	Description	Image
LV	Metal louvre Panel - Charcoal	
R1	Render Finish - Dulux Summer Cloud Half	
R2	Concrete render	
RB	Recycled Bricks	
SG	Spandrel glass grey	

Code	Description	Image
V1	VITRA panel - Dark Grey Finish	
V2	VITRA panel - Concrete render finish	

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<b>COLOUR &amp; MATERIAL</b>
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<b>PROJECT:</b>	Proposed Mixed Use Development
<b>LOCATION:</b>	893 High Street Thornbury
<b>CLIENT:</b>	ROBERT CALLIPARI

<b>AMENDMENT</b>	
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3	REVISED

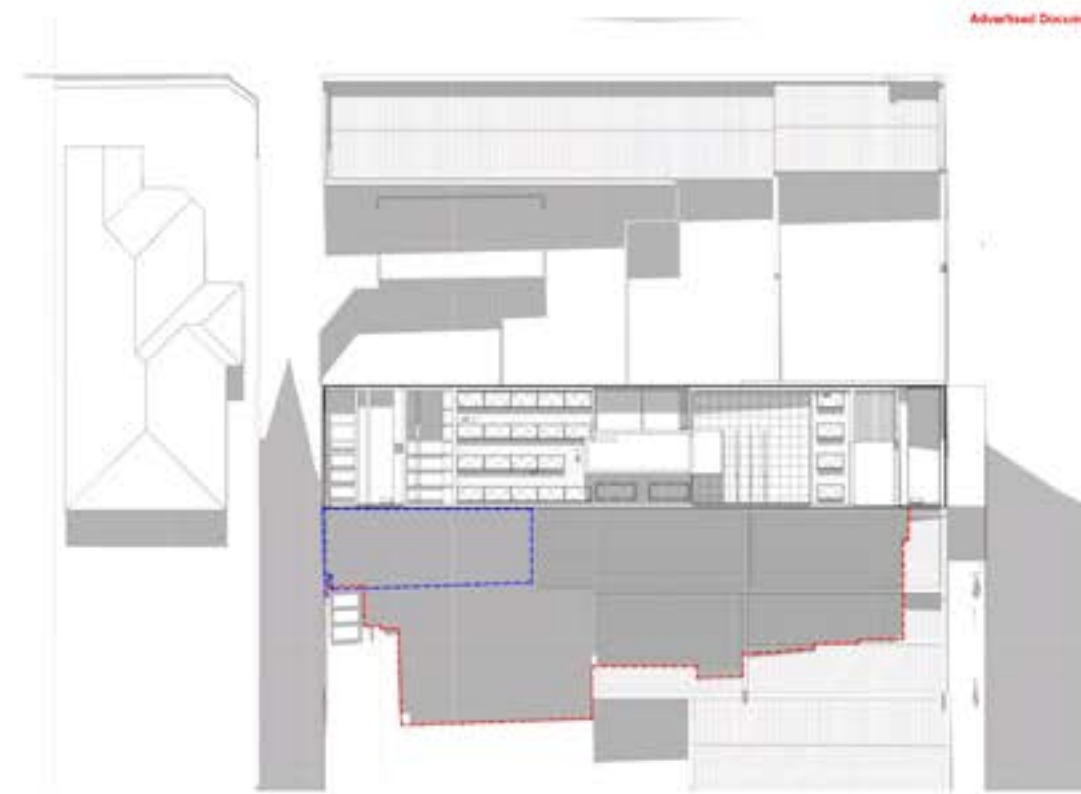
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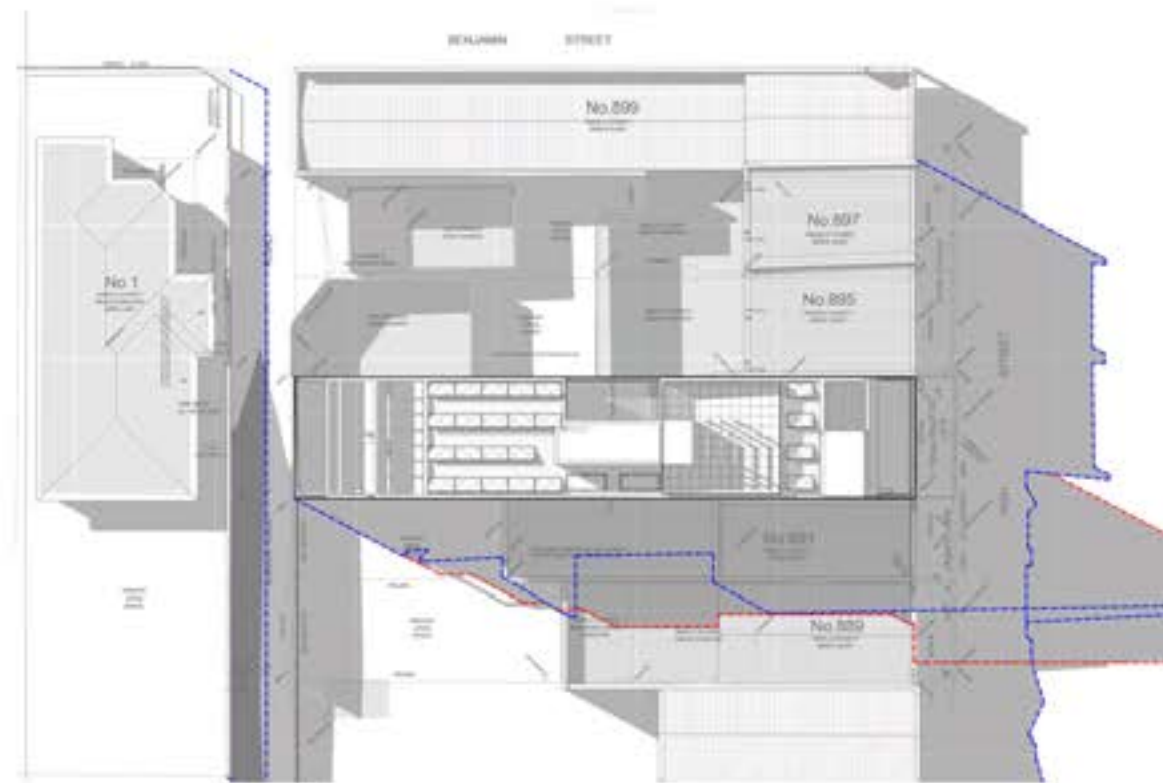
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Darwin City Council Received 25/06/2021



SHADOWS - 21 SEPTEMBER - 9AM




SHADOWS - 21 SEPTEMBER - 12NOON

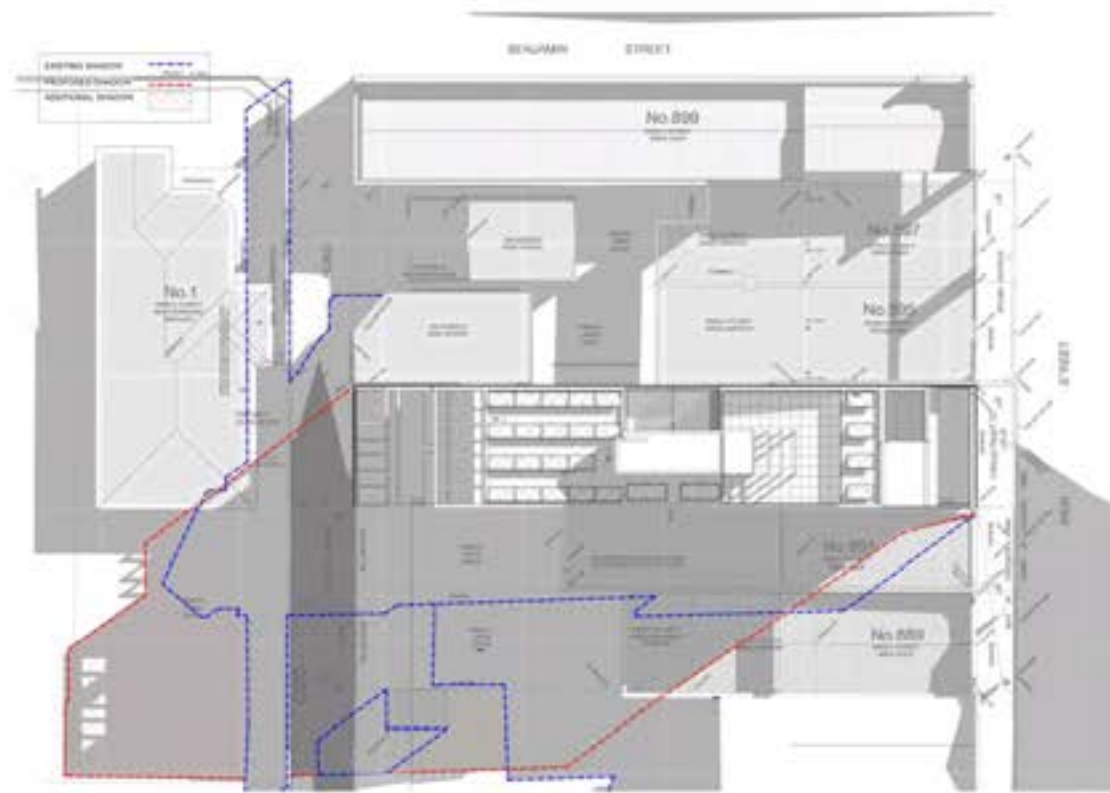


SHADOWS - 21 SEPTEMBER - 3PM

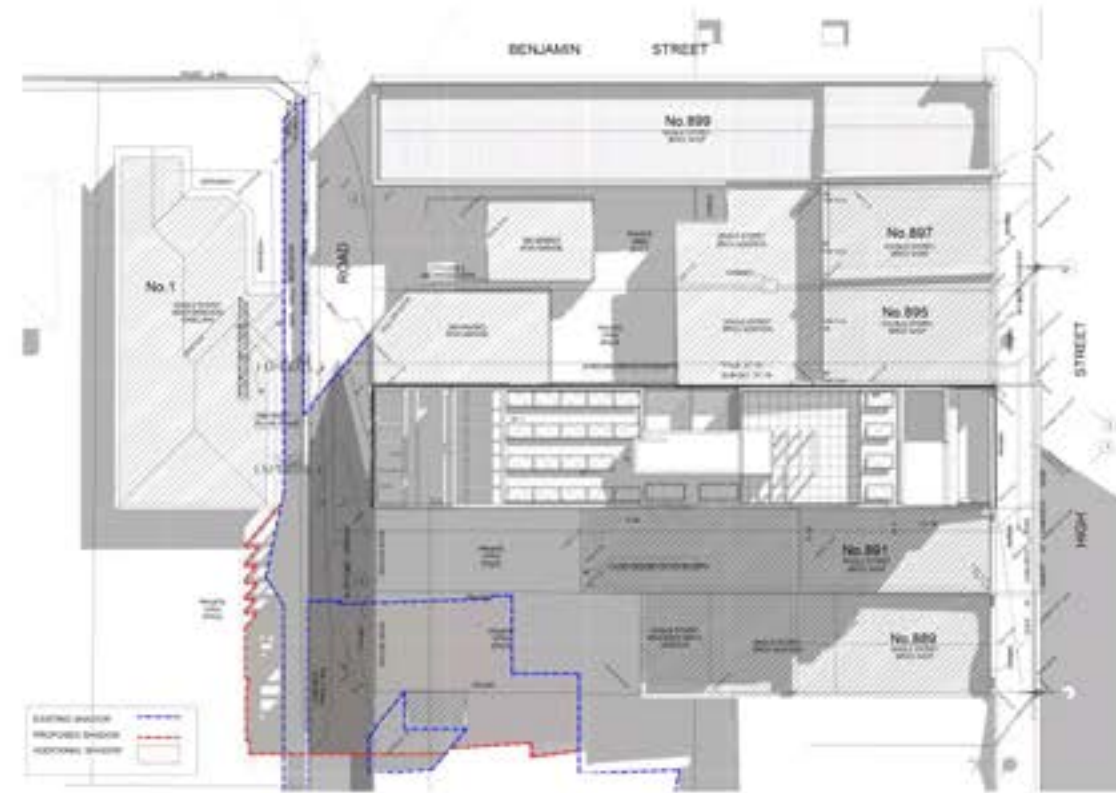
— EXISTING SHADOW  
— PROPOSED SHADOW

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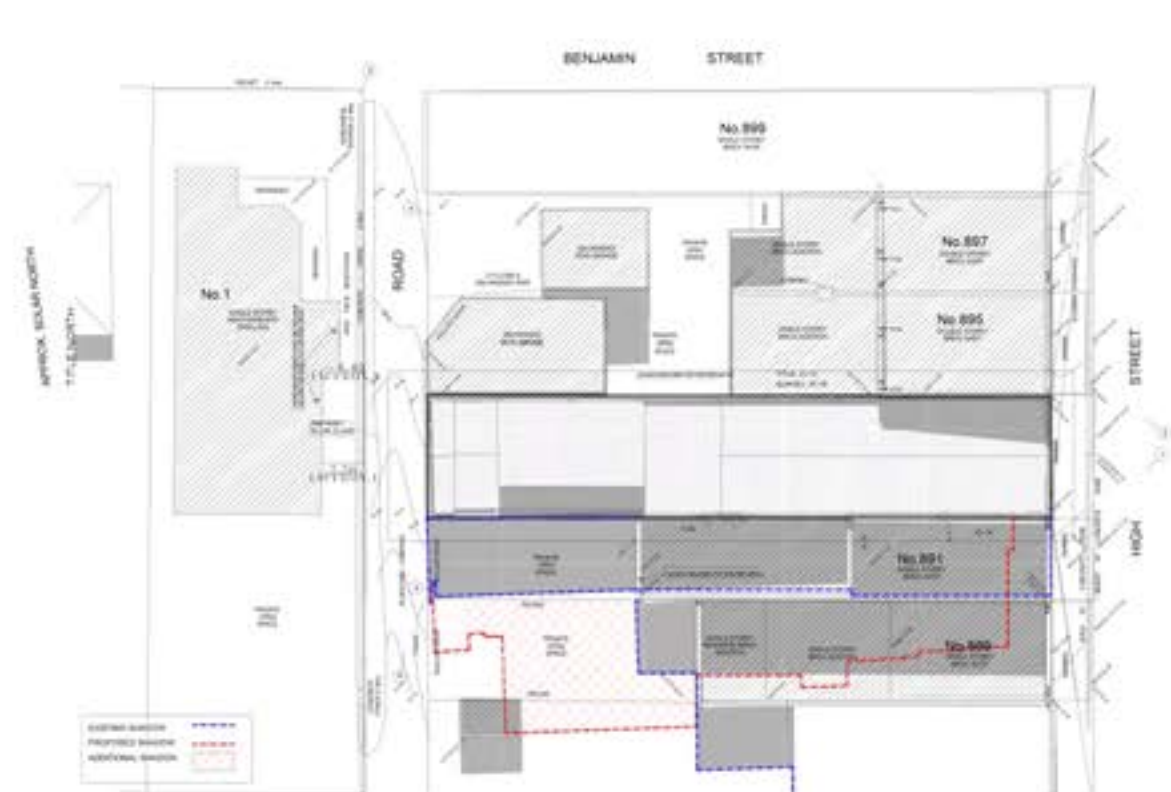
SHADOWS - 21 SEPTEMBER - 9AM



SHADOWS - 21 SEPTEMBER - 10AM



SHADOWS - 21 SEPTEMBER - 11AM



SHADOWS - 21 SEPTEMBER - 12NOON

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**5.3 AMENDMENT TO DEVELOPMENT PLAN POD/1/2007/I  
1056 - 1140 & 1142 Plenty Road, Bundoora  
(Polaris - Former Larundel Psychiatric Hospital)**

**Author:** Principal Planner

**Reviewed By:** Acting General Manager City Sustainability and Strategy

Applicant	Owner	Consultant
SJB Planning	Deal Corporation P/L (Dealcorp)	SJB Planning  Clarke Hopkins Clarke Architects

**SUMMARY**

- It is proposed to amend the Polaris 3083 Precinct Development Plan (PDP) approved under the provisions of the Development Plan Overlay – Schedule 1.
- The amendments sought generally relate to the redevelopment of Lot S3 to increase the number of storeys from 2-3 to 5 storeys and to replace outdated policy content that has been superseded by the current Municipal Strategic Statement and Plan Melbourne.
- The site is in the Mixed-Use Zone (MUZ) and covered (either wholly or in part) by the Development Plan Overlay – Schedule1 (DPO1) and Heritage Overlay (HO107).
- A series of Section 173 Agreements apply across the land.
- Five (5) submissions were received against this application.
- The proposal is generally consistent with the provisions of the Mixed-Use Zone, Heritage Overlay and Development Plan Overlay, of the Darebin Planning Scheme.
- It is recommended that the amendment be supported.

**CONSULTATION:**

- Public notice was given via letters sent to surrounding owners and occupiers and signs erected on respective lots.
- Notice was given directly to VicRoads, Public Transport Victoria and the Minister for Education and Training (Latrobe University).
- This application was referred internally to the following units in Council: Planning Landscape Architect, Climate Emergency and Sustainable Transport; City Designer.

**Recommendation**

**That** amendment POD/1/2007/I be supported subject to Page 44a of the amended Polaris Development Plan being substituted for that submitted to Council on the 3 June 2021.

## INTRODUCTION AND BACKGROUND

### Site History – Lancaster Gate

The site was previously known as the Larundel Psychiatric Hospital which closed in 1999. Following this closure Places Victoria (then known as the Urban and Regional Land Corporation (URLC)), purchased the land for redevelopment.

On 27 November 2001, Council approved the Lancaster Gate Development Plan in accordance with an order from the Victorian Civil and Administrative Tribunal (VCAT). The Lancaster Gate Development Plan divided the Lancaster Estate into three parts; a Residential Precinct (15.38 hectares), the Village Precinct which included the former hospital buildings and the Mixed-Use Precinct (10.61 hectares).

The Residential Precinct was subsequently subdivided, developed into individual dwelling lots and sold off.

In 2006 the remaining Village Precinct and Mixed-Use Precinct were sold by Places Victoria (then VicUrban), forming the 'Polaris' precinct.

The Polaris 3083 Development Plan (PDP) was prepared and approved by Council on the 23 September 2009. The PDP forms an amendment to the original Lancaster Gate Development Plan and relates to the redevelopment of the Village Precinct and Mixed-Use Precincts. Development in accordance with the PDP commenced in 2010 with the construction of the Town Centre, townhouses and apartment developments.

The Village Precinct Development Plan (VPDP) was endorsed in 2013 and builds on the approved PDP. This document guides the redevelopment of the Village Precinct and provides more specific guidance for the redevelopment of the residential lots in the north-east section of the site. It generally sits alongside and is to be read in conjunction with the PDP.

### Polaris

The Polaris 3083 Development Plan (PDP) has been amended several times since it was first approved in 2009.

The most recent version of the PDP was endorsed by Council on the 12 November 2019 (Council application reference No. POD/1/2007/E). This version of amendments allowed building along Plenty Road ranging from five storeys close to the south-east intersection with Copernicus Crescent up to nine storeys at the north-western intersection with Main Drive.

On the 19 December 2019, an amendment in respect of Lot S3 (1B-16) was submitted to Council to vary the built form requirements from a nominated two (2) to three (3) storey building height to seven (7) storeys. The applicant applied to VCAT to review the failure of Council to make a decision of the amended PDP. Council advised VCAT that had it been in a position to make a decision it would have refused the amendments.

The Tribunal affirmed Council's recommendation to not support the amendments however the decision made clear recommendations to assist the applicant and Council in any possible future amendments to the PDP that may be sought by the applicant. The recommendations made by the Tribunal in *Deal (LG) Pty Ltd v Darebin CC [2020] VCAT 966*, at paragraph 60 and 61 are as follows:

60 To assist the applicant and the council in preparing any possible further amendments that may be sought by the applicant to the PDP I set out below changes I find could form the basis of an amendment to the PDP that could meet the provisions of DPO1. These are

- Deletion or amendment of the last paragraph of proposed new page 13a;
- Amendment to the...building in precinct 1B-16 as a predominantly three storey building envelope with opportunity for a recessed fourth or fifth storey, up to a maximum building height of 16 metres. This height will be a maximum, not a 'preferred' maximum building height. The maximum height is not mandatory but needs to be tested as being 'generally in accordance' with the plan in the assessment of a planning application.
- Clarification that building above the three-storey form should include a setback of at least 2 metres from the lower three storeys facing Main Drive. The recession of any level above three storeys should also retain a reference to 'generous setbacks to Linakers Cottage'.
- Deletion of the 'benchmark' image for 1B-16 on page 44 that shows, 'indicative contemporary building typology for student accommodation for building 1B-16'. This figure shows various buildings well exceeding five (and seven) storeys and are not indicative of what I consider should occur on the review site.
- Amendment to section 3.7 and associated figure 30 on page 30 of the PDP to delete reference to the 'La Trobe University agricultural reserve' as this is contrary to the updated information on the proposed new page 13a.

61 There may be other consequential changes that the applicant may also want to consider. These include noting that:

- The current Municipal Strategic Statement and Plan Melbourne supersedes much of the policy context in the existing PDP.
- The physical context, set out on pages 12 and 13, has been superseded by development that has occurred since 2009.

## ISSUES AND DISCUSSION

### Subject site and surrounding area

- The subject site is the Polaris Site (Lancaster Gate) comprising the former Larundel Psychiatric Hospital in Bundoora.
- The site is currently being redeveloped and comprises two (2) sub – precincts – the Town Centre and the Village Precinct.
- The town centre (to which this amendment is relevant) relates to the mixed – use area to the south-west. The town centre provides a full retail offering including full line supermarket and associated retail, food and drink premises and community services.
- Lot S3 (8 - 16 Main Drive, Bundoora) comprises 931 square metres with a 46.48 metre frontage to Main Drive. The rear of the site abuts the two (2) storey car park of the town centre. The land is currently vacant.
- The applicant has lodged a concurrent planning permit application D/262/2021 for Lot S3 which is in the initial stages of assessment.
- To the south-east of the site on the opposite side of the entry into the supermarket car park is Linaker's Cottage, which is currently used as a Childcare Centre.

- Lot S9 is to the north of the site and comprises a total of 3,939 square metres. This site is located on the south-eastern side of Plenty Road and is bound by Plenty Road, Main Drive, Galileo Gateway and Copernicus Crescent. The site is generally irregular in shape with dimensions of 137.49 metres along the north-western boundary to Plenty Road, 156.78 metres along the south-western boundary to Galileo Gateway, 37.45 metres along the south-western boundary and 17.20 metres along the north-eastern boundary. This site is currently vacant.
- Planning permit D/10/2020 was issued on the 24 April 2020 for *Use and development of the land for a residential hotel, office and bar (maximum of 7 storeys) and a reduction of car parking on land affected by a Development Plan Overlay* in accordance with the endorsed plans.
- The proposed development at Lot S9 will contain two (2) buildings (known as Building 1B-01 and Building 1B-02, in the approved development plan). Building 1B-01 is contained within a part-seven (7), part-eight (8) storey building with a maximum overall height of 31.8 metres.
- Building 1B-02 is contained within a part-five (5), part-six (6) storey building with a maximum overall height of 22.3 metres;
- To the south-west of the site is Main Drive, beyond which are individually owned three storey townhouses at 1A Main Drive, Bundoora.

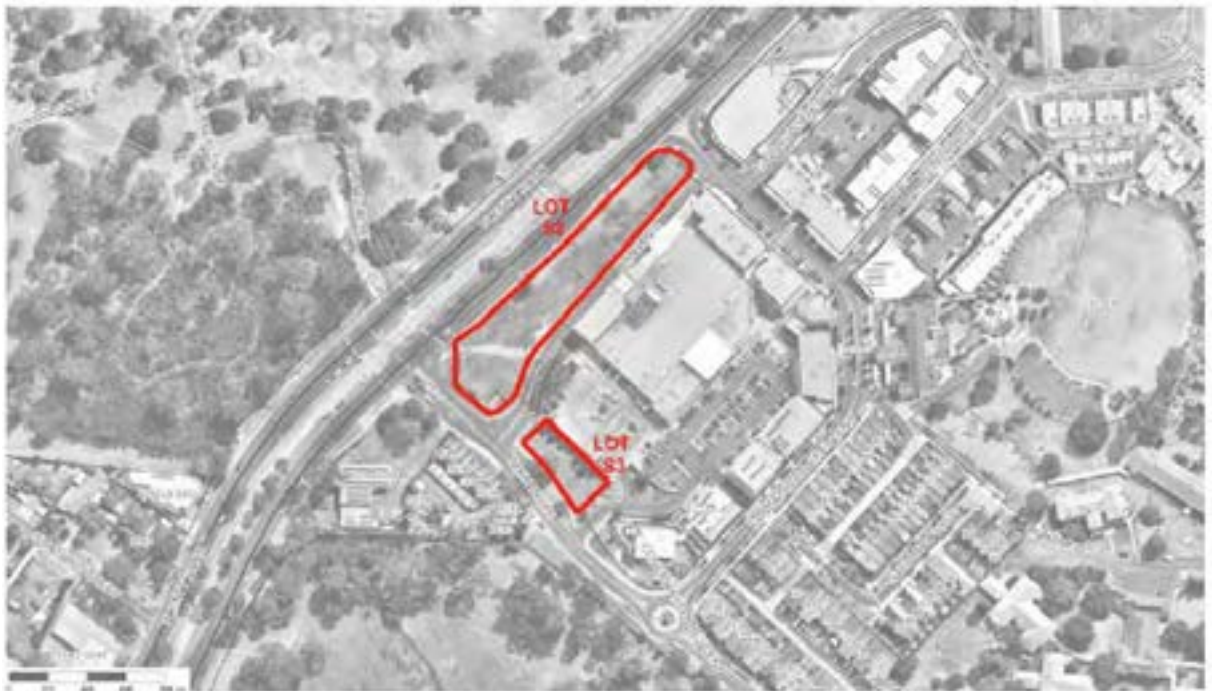


Figure 1: Aerial view of Lot S3 and Lot S9 in the Polaris 3083 Development Plan (PDP).

- More broadly the PDP benefits from its proximity to La Trobe University, Northland Shopping centre and Plenty Road.
- The PDP is adjacent La Trobe University which is to the south-west of the site on the opposite side of Main Drive. In 2014, La Trobe University launched its Melbourne Campus Master Plan (MCMP), providing a comprehensive guide to the future development of its Melbourne Campus, which is undergoing expansion.
- The PDP also benefits from its proximity to the proposed Suburban Rail Loop (though the exact location of this line and future stations is subject to change).

**Proposal**

It is proposed to amend the Polaris Development Plan as follows:

**Lot S3**

- Increase the building height of Building 1B–16 on Lot S3 from 2-3 storeys to 5 storeys. The proposed building will be a predominantly three (3) storey building to Main Drive with a recessed fourth and fifth storey with a maximum building height of 16 metres.
- Upper levels above the street wall are to be setback 2 metres.
- Provisions for 'generous' setbacks to Linaker's Cottage.
- Provision for no setback for the first three storeys to all boundaries.
- The removal of Tree 199 from within the Main Drive street reserve adjacent to Lot S3 and the provision of three (3) replacement trees.

**Other**

- Update policy within the PDP with the current Municipal Strategic Statement, Plan Melbourne and reference to the La Trobe University Melbourne Campus Master Plan.
- Update pages 12 and 13 with a new page 13a which shows the development that has occurred since 2009 and provides an updated local context analysis with regards to La Trobe University.

**Objections summarised**

- Overshadowing of the adjoining childcare centre by the increased building height on Lot S3.
- Overshadowing of adjoining secluded private open space by the increased building height on Lot S3.
- Overdevelopment/Increased building height results in excessive number of apartments.
- Loss of privacy/Overlooking
- Excessive height
- Minimal setbacks
- Insufficient onsite parking/Increased traffic/Parking congestions
- Loss of vegetation
- Memorial for Aija Maasarwe
- Loss of view
- Loss of property value

**Officer comment on summarised objections****Overshadowing of the adjoining childcare centre by the increased building height on Lot S3**

Not applicable. Overshadowing considerations are assessed under future planning applications (D/262/2021). Nonetheless, the open space associated with the childcare centre is approximately 11 metres to the south-east of the subject site and separated by an existing vehicle accessway to the supermarket carpark. As indicated in Figure 2 below, the north-western corner of the childcare centre is already overshadowed by existing vegetation.



Figure 2: Aerial view of vacant Lot S3 and the arrows indicating current overshadowing by the existing tree into the open space of the childcare centre at Linaker's Cottage and Tree 199, taken from Nearmaps, 14 June 2021.

Overshadowing of adjoining secluded private open space by the increased building height on Lot S3

Not applicable. Overshadowing considerations will be assessed under future planning applications (D/262/2021) at which time the proposal will be assessed against the relevant requirements (e.g. Clause 58). Nonetheless, the apartments at 1 Main Drive, Bundoora are located to the west of the subject site, separated by Main Road and the road reserve. Specifically, the dwelling at 1/1a Main Drive, Bundoora is in excess of 20 metres from the edge of the subject site.

Overdevelopment/Increased building height results in excessive number of apartments

The proposed amendment to increase the height of Building 1B - 16 from 2-3 storeys to 5 storeys accords with acknowledged policies for urban consolidation and increased densities. The Victorian State Government has provided clear policy direction on urban consolidation that is based on increased densities in well located activity centres with access to public transport, recreation, education and other convenience services. The surrounding urban context includes Plenty Road, the Polaris Town Centre and La Trobe University and therefore fulfils the locality requirements.

Loss of privacy/Overlooking

Not applicable. Overlooking considerations will be assessed under future planning applications (D/262/2021) at which time the proposal will be assessed against the relevant requirements (e.g. Clause 58). Nonetheless, the site at Lot S3 is bound by road reserves to the north-west, west and south-east and a car park to the north-east (see Figure 2) and is effectively an island site with no direct residential abuttal. The land is in excess of 9 metres from existing property boundaries, therefore any future residential balconies will not require overlooking measures.

Excessive height

The proposed amendment to increase the height of Building 1B – 16 from 2-3 storeys to 5 storeys is acceptable and consistent with the current and future context of the area.

Planning permit D/10/2021 for Lot S9 was approved on the 24 April 2020 for a residential hotel, office and bar with Building 1B-01 a part seven (7), part eight (8) storey building and Building 1B-02 a part five (5), part six (6) storey building.

The 2014 Melbourne Campus Master Plan for La Trobe University also indicates that a 7 – 10 storey building is proposed to the south of Main Drive and interfacing with the Polaris Town Centre.

Importantly, the Tribunal stated at paragraph 60 of the Order (*Deal (LG) Pty Ltd V Darebin CC (2020) VCAT 966*) that Lot S3 was suitable for “a predominantly three storey building envelope with opportunity for a recessed fourth or fifth storey, up to a maximum building height of 16 metres.

More broadly, the character along Plenty Road is heavily guided by Amendment C137 – Plenty Road Corridor, which places an emphasis on increased building heights and dwelling densities.

The proposed amendment responds to the above direction as provided by the Tribunal and draws on the current built form adjacent the site and proposal at Lot S9 and more broadly the 2014 Melbourne Campus Master Plan for La Trobe University and proposed Suburban Rail Loop.

#### Minimal setbacks

The proposed amendments depict the fourth and fifth level of any future building on Lot S3 as being setback 2 metres from the street wall. This is consistent with the direction provided by the Tribunal.

Lot S3 is effectively an island site with no direct residential abuttal. The lot is in excess of 20 metres from the land at 1A Main Drive, Bundoora (on the opposite side of Main Drive), 11 metres from the north-western corner of the outdoor plan area of Paisley Park Early Learning Centre and a further 25 metres from the heritage building known as Linaker’s Cottage.

#### Insufficient onsite parking/Increase traffic/Parking congestion

Not applicable. Car-parking considerations will be assessed under future planning applications (D/262/2021). Nonetheless, the PDP is well served by public transport options along Plenty Road and multiple bus routes that connect to Latrobe University. Council’s Climate Emergency and Sustainable Transport unit have not objected to the amendments.

#### Loss of vegetation

Tree 199 is relevant to this amendment and is a street tree. Council has previously agreed that Tree 199 can be removed, provided a tree amenity off-set payment is paid to Council prior to the first occupation of the approved building and replacement trees are planted to off-set its removal.

#### Memorial for Aiiia Maasarwe

Council and the Maasarwe family have collectively chosen to create a memorial landscape on Council owned land at Bundoora Park. The proposed development of the land at Lot S3 meant the planned memorial is not considered feasible. The Maasarwe family have been working closely with Council to create a memorial landscape in an alternative location.

The owner of Lot S3 has contributed towards the memorial.

#### Loss of view

There is no legal entitlement to a view and VCAT have consistently determined that a right to a view is not a planning consideration.

#### Loss of property value

Property values are speculative. Fluctuations in property prices are not a relevant consideration to be determined under the Planning and Environment Act 1987 or the Darebin Planning Scheme.

## PLANNING ASSESSMENT

The principal requirement of the DPO1 is that the Development Plan should address the elements of the North East Corridor Strategic Plan (NECSP) relevant to the site. It is important to note that this plan has been largely superseded by the current Municipal Strategic Statement and Plan Melbourne and the work undertaken through Amendment C137 for the Plenty Road Corridor, particularly in relation to height controls.

Sub-clause 1.0 of the DPC1 sets out the requirements for what should be included in the development plan, which has previously been assessed and approved.

Sub-clause 2.0 sets out the guidelines for the consideration of a development plan which includes the following:

*The responsible authority will discourage industrial uses from locating on the land.*

*The responsible Authority should consider whether the development plan or permit application is consistent with the North East Corridor Strategic Plan.*

*Any applicable heritage study and any applicable conservation policy should be considered. Whether the location, bulk, height and appearance of any proposed buildings or works will be in keeping with the character of the area.*

*Whether any proposed landscaping or removal of vegetation will be in keeping with the character and appearance of adjacent buildings, the streetscape or the area.*

*The layout and appearance of areas set aside for car parking, access and egress, loading and unloading and the location of any proposed car parking.*

*Whether subdivision will result in development which is not in keeping with the character and appearance of adjacent buildings, the streetscape or the area will adversely affect the significance, character or appearance of the heritage place.*

The NECSP is the current key reference point in the Scheme for the development of this site. The strategic vision of the plan that, amongst other things, is that the Larundel Urban Village be redeveloped to accommodate medium density residential development with mixed use development.

Varied forms of medium density housing have already been completed or are proposed to be undertaken, which maintain a level of activity and safety in the area. Other commercial uses are located in appropriate locations and face each other across the street. The majority of the PDP has been developed with only a couple of lots still vacant.

With regards to vegetation the PDP has sought to retain as many trees as possible. All development proposals must be based on an accurate survey of existing native trees and provide justification for the removal of any native trees.

The key considerations for the current amendment relate to the proposed built form outcomes of Lot S3 and changes to the policy content within the document.

### **Lot S3 (Main Drive)**

Lot S3 is located within sub precinct 1B of the PDP and will contain a single building – 1B-16. The proposed amendments to the PDP in respect of this building increase the height of the building from 2 – 3 storeys to 5 storeys.



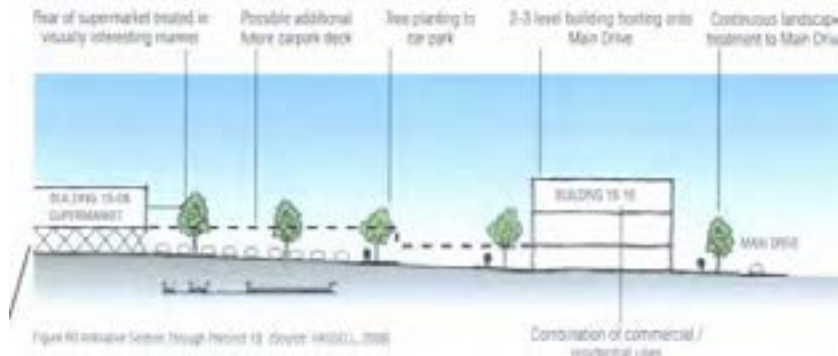


Figure 3: Current endorsed south-west to north-east section of Building 1B-16 on Lot S3

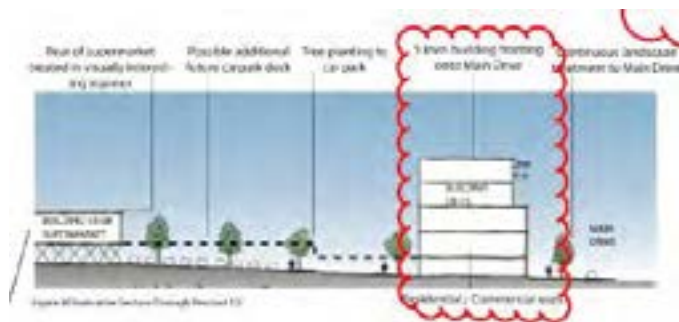


Figure 4: Proposed south-west to north-east section of Building 1B-16 on Lot S3

Building form

One of the key changes envisaged in terms of built form character along Main Drive is the La Trobe University Melbourne Campus Master Plan of 2014. At the time the PDP was first endorsed in 2009, the La Trobe University Melbourne Campus Master Plan had not been prepared so the southern interface of the PDP was largely undefined.

In 2014 La Trobe University launched its Melbourne Campus Master Plan (MCMP), providing a comprehensive guide to the future development of its Melbourne Campus. The Master Plan divides the campus into distinct neighbourhoods with different land use, built form, urban design and transport expectations.

To the south-west of Lot S3 is Main Drive and the northern edge of the La Trobe University Campus. This section of La Trobe University falls within the ‘Town Centre’ neighbourhood of the MCMP and will comprise a major development in the short to medium term. Indicatively, the MCMP proposes between a 7 – 10 storey building with a podium height of 1 – 3 storeys at street level. The building will be opposite the PDP and is known as building TC2. The building will have an activated frontage to Main Drive and Science Drive which aligns with Copernicus Crescent.

Given this proposed future development and given the link between the two interfaces a building on Lot S3 of five storeys with a maximum height of 16 metres is appropriate given the broader context of the area. This was expressly supported by Member Glynn in *Deal(LG) Pty Ltd v Darebin CC [2020] VCAT 966*), at paragraph 53 who stated, “Having regard to local context considerations and the future 7-10 storey podium/tower built form character anticipated at the northern edge of La Trobe University under the MCMP, there is also opportunity for building height of up to 5 storeys for Building 1B-16 with generous setbacks to Linaker’s Cottage.”

The site does not have any direct abuttal to land used residentially and is effectively an island site with Main Drive running along the south-western boundary, Galileo Gateway running along the northern boundary and the vehicle accessway to the car park of the supermarket along the south-eastern boundary and the supermarket car park to the east, therefore the proposed Building 1B-16 will have minimal off site amenity impacts.

In response to the recommendations by VCAT the PDP has been amended to show a building envelop of generally no setback up to three (3) storeys with a recessed fourth and fifth level of 2 metres. Page 58 of the VCAT order includes the following paragraph, *“I find an amendment to the PDP for precinct 1B-16 could be made acceptable if it were only to provide a building envelope that could accommodate a predominantly three-storey building with one to two recessed upper levels above, up to 16 metres high, and utilising a 2 metre setback to Main Drive. I find such an amended envelop could accord with the requirements and decision guidelines of the DPO1.*

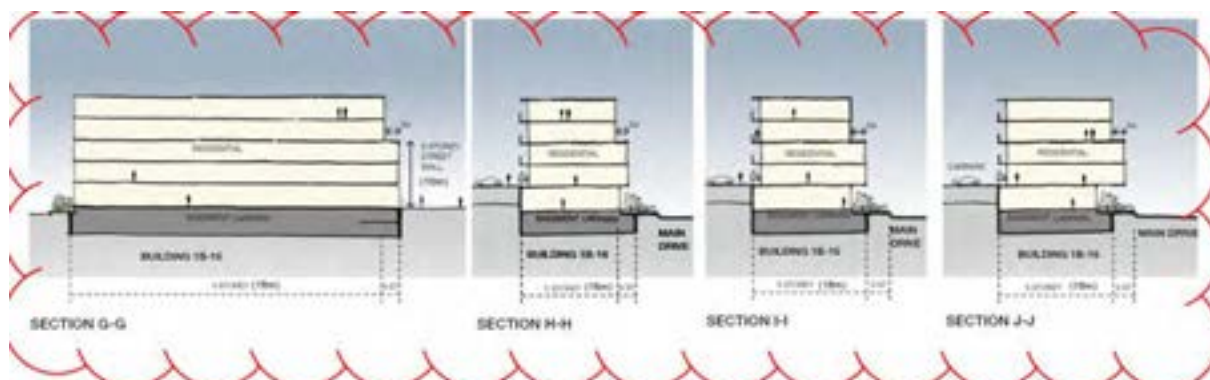


Figure 5: Amended section diagrams of Building 1B-16 in the PDP showing the five (5) storey building with no setback up to three (3) storeys with a two (2) metre setback at the upper levels.

Council’s City Designer is supportive of the proposed changes to the building envelop of Building 1B-16 provided a balcony edge along the south-west edge of the building is included. Sketch plan submitted by the applicant on the 3 June 2021 demonstrates the balcony edge as requested by Council’s City Designer.



Figure 6: Revised sketch plans received by Council on the 3 June 2021 showing a balcony edge along the south-western boundary of the building.

A condition of any approval of the amended development plan would therefore require that this revised sketch plan (which was submitted to Council on the 3<sup>rd</sup> of June 2021) be substituted for Page 44A of the amended Polaris Development Plan.

Land Use

The text within the PDP continues to state that the building *“may possibly be used for either commercial or residential uses.*

However, the use of this building is not determined at this stage and will be proposed at the planning permit stage of the development”. As indicated on Page 40 of the amended PDP, a reference to apartments has been included in the following land use description, “Mixed use neighbourhood activity centre providing retail, commercial, office, supermarket, food and drink premises (take away food premises, restaurant, cafes), residential (shop-top housing, student accommodation, apartments, residential hotel) and civic community centre, plaza land uses.

\*Planning permit application D/262/2021 proposes to use the land as apartments.

**Other Matters**

Vegetation

The approved PDP sets out the tree retention and removal across the Polaris development plan area. Many of the trees listed for removal and a number of additional trees have already been removed from the respective lots. In some cases the additional trees removed are street trees that have been removed by Council, while in other instances subsequent approval was sought or the cause of removal is unknown.

On Lot S3 Tree 199 is proposed to be removed as part of the amendment. As agreed previously with Council’s Planning Landscape Architect in previous applications, its removal from the nature strip is supported, provided the requirements as highlighted in yellow on Page 87 of the amended PDP are adhered to as follows:

*A tree amenity offset payment is to be paid to Council prior to first occupation of an approved building.*

*Replacement trees are to be planted to offset removal of Tree 199. A landscape plan showing the proposed location of the trees inclusive of complementary vegetation is to be prepared in support of any planning permit application lodged with Council. Replacement tree species to be to Council requirements.*



Figure 7: Tree 199 as shown on the current endorsed Landscape Concept Plan

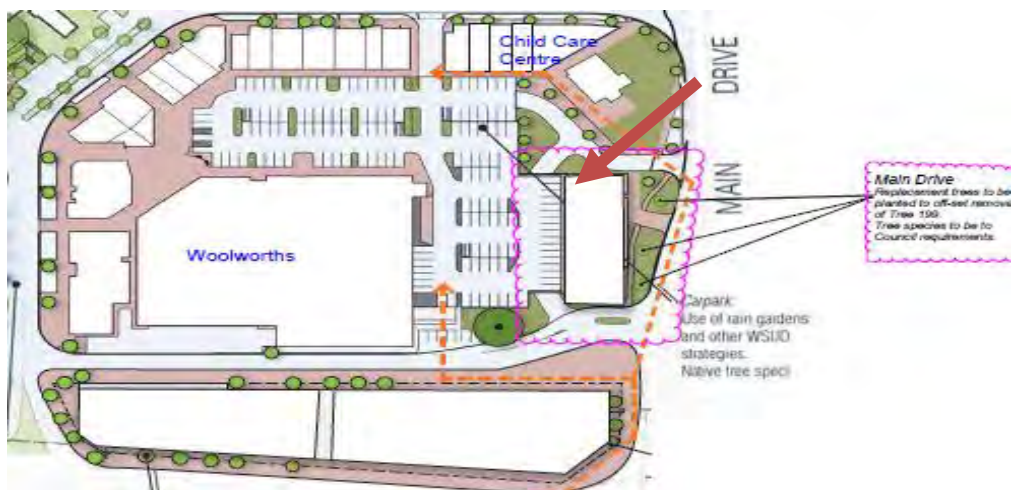


Figure 8: Proposed Landscape Concept Plan with Tree 199 removed and replaced by suitable offsets.

Housekeeping' Amendments

The amended PDP includes a number of amendments which are reasonably considered as 'housekeeping' changes to reflect the current state of affairs on the ground. Examples include:

- Removal of reference to outdated policies (i.e. Darebin Open Space Strategy 1999).
- The provision of a 'Superseded' stamp on relevant sections of the document which relate to outdated data and outdated policies.
- Provision of updated local context to reflect the current state of affairs on the ground.
- Provision of reference to updated Public Transport options and Planning Scheme Amendment VC148 which was gazetted on 31 July 2018 with regards to land within the Principal Public Transport Network (PPTN).
- Removal of reference to La Trobe University Agricultural Reserve and the inclusion of a new reference to the La Trobe University Master Plan.
- Precinct 1B building envelopes updated.

**Conclusion**

The proposed amendments to the Polaris 3083 Development Plan address the recommendations of Member Glynn with respect to Lot S3 and are deemed appropriate with respect to the local and broader context of the site and accord with the requirements and decision guidelines of the DPO1.

**REFERRAL SUMMARY**

Department/Authority	Response
Transport Management and Planning	Comments received 14 April 2021. No objection The Ratio rates contained within the Integrated Transport Plan, 2009 and as referenced on page 78 of the Development Plan are as follows:  <i>"Refer to the <b>Integrated Transport Plan July 2009</b> (Appendix C) prepared by Ratio Consultants for a detailed empirical assessment of the future car parking demand for the site:</i>

	<ul style="list-style-type: none"> <li>• Ratio estimate a rate of 5.0/100sqm for a supermarket</li> <li>• Ratio estimate a rate of 3.0/100 sqm for shops</li> <li>• Ratio estimate a rate of 3.0/100 sqm for offices</li> <li>• Ratio estimate a peak parking demand of 0.35 spaces per seat for a tavern</li> </ul> <p><i>For land uses not listed in the PDP or the Planning Scheme, an empirical assessment will be required in support of any future planning application for the use.</i></p> <p><i>In instances where the Planning Scheme and the PDP carparking rates differ, the lower of the two rates applies.</i></p> <p><i>These rates are broadly in line with the Column B rates of Clause 52.06, noting the Polaris Estate is generally located within the PPTNA. Transport is comfortable with the rates nominated above.</i></p> <p><i>It is understood that for other uses not listed in the PDP, <u>the Planning Scheme rates shall apply</u> which is supported. In stances where the PDP and Planning Scheme rates differ then the lower rate shall be adopted. This is considered a practical approach.</i></p> <p><i>Finally, for uses not listed in either the Planning Scheme or PDP an empirical assessment will be required in support of any future planning application for the use. This is also a sensible approach and is in line with Clause 52.06 expectations for uses that do not have a defined car parking rate.</i></p>
<p>Planning Architect      Landscape</p>	<p>Comments received 20 May 2021</p> <p>No objection.</p> <p><i>Thank you for the opportunity to provide comment on the proposed removal of Tree 199 in POD_1_2007.</i></p> <p><i>As stated in referral responses for previous applications, the retention or relocation of Tree 199 is the preferred outcome given its established form and the presence of the same species along the south side of Main drive. Having said this, Council will support its removal, as long as the requirements highlighted in yellow on Page 87 of the amended PDP are adhered to:</i></p> <p><i>A tree amenity offset payment is to be paid to Council prior to first occupation of an approved building.</i></p> <p><i>Replacement trees are to be planted to offset removal of Tree 199. A landscape plan showing the proposed location of the trees inclusive of complementary vegetation is to be prepared in support of any planning permit application lodged with Council. Replacement tree species to be to Council requirements.</i></p>

<p>City Design</p>	<p>Comments received 19 April 2021.                      No Objection.  <i>Building 1B-16</i></p> <ul style="list-style-type: none"> <li>• <i>From the diagrams the difference between Section HH, II and JJ is not clear. This should be clearly highlighted.</i></li> <li>• <i>Section JJ shows the upper level cantilevered above the footpath. This is not preferred and the upper level should not encroach beyond the edge of the footpath.</i></li> <li>• <i>The need for ground floor setback on Main Drive is not clear. If this is due to private open space for residential use on ground level then the upper level should not cantilever above as this will significantly reduce natural light in the living areas connected to private open space.</i></li> <li>• <i>Balconies should not encroach in upper level setbacks as this would remove the perception of a setback and will present as a shear building.</i></li> </ul> <p>Response: The section diagrams were clarified with regards to the irregular shape of the block hence that is why it appears as an overhang. The fourth dot point was responded to with regards to a balcony edge above level 3.</p>
<p>Ausnet Services</p>	<p>Comments received 27 April 2021.  <i>We advise that Ausnet has no objection to the revision as proposed.</i></p>

**PLANNING SCHEME SUMMARY**

**Darebin Planning Scheme clauses under which a permit is required**

- Pursuant to clause 43.04, a development plan must be prepared to the satisfaction of the Responsible Authority.
- Schedule 1 applies to the land and sets out requirements for the development plan (sub-clause 1.0) and guidelines for considerations (sub-clause 2.0).

**Applicable provisions of the Darebin Planning Scheme**

Section of Scheme	Relevant Clauses
PPF	11, 11.02-1S, 11.03, 15, 15.01-1S, 15.01-2S, 16.01-3S, 17.02-S
Zone	32.04
Overlay	43.01, 43.04, 45.06
Particular provisions	52.06
General provisions	65

**POLICY IMPLICATIONS**

**Environmental Sustainability**

All new dwellings are required to achieve a minimum six (6) star energy rating under the relevant building controls.

**Social Inclusion and Diversity**

Nil

**Other**

Nil

**FINANCIAL AND RESOURCE IMPLICATIONS**

There are no financial or resource implications as a result of the determination of this application.

**FUTURE ACTIONS**

Nil

**RELATED DOCUMENTS**

- *Planning and Environment Act 1987* (as amended)
- Darebin Planning Scheme
- Polaris 3083 Development Plan (as amended)

**Attachments**

- Appendix A - Advertised list of changes - 1056-1140 & 1142 Plenty Road, Bundoora - POD/1/2007/I (**Appendix A**) [↓](#)
- Appendix B - Advertised Amended PDP - 1056-1140 & 1142 Plenty Road, Bundoora - POD/1/2007/I (**Appendix B**) [↓](#)

**DISCLOSURE OF INTEREST**

Section 130 of the *Local Government Act 2020* requires members of Council staff and persons engaged under contract to provide advice to Council to disclose any general or material interest in a matter to which the advice relates.

The Officer reviewing this report, having made enquiries with relevant members of staff, reports that no disclosable interests have been raised in relation to this report.

**Endorsed Polaris Development Plan 2009 (as Amended 2013, 2019, 2021)**

Page	Section	Proposed Changes - 2021
1	Cover page	Amended to include details for amendment date of final document, i.e. the version subject of Council's determination. Bubble added.
2	Planner, Landscape Architects and Architects  Consultant Team	Note added regarding status of listed consultant team.  <i>Note - list only acknowledges consultants who contributed to the PDP of 2009 only.</i>  <i>Consultant team has subsequently changed each iteration thereafter.</i>  Bubble added.
3	Appendices	Note added to provide statement on status of documents in the appendices  <i>*Appendix A - Subsequent subdivision of the original Polaris Development Plan area has been undertaken. Please obtain latest Certificate of Title for details</i>  <i>*Appendix B: Appendix B - Feature Survey Plan is out of date due to multiple stages of the Polaris Development Plan being constructed and completed.</i>  <i>*Appendix C-H: These documents are intended to act as 'base' reference documents and are to be used as a guide only for the future development within the Polaris Village Precinct.</i>
6	1.3 Overview of the Polaris 3083 Development Plan	Amendment of paragraph 6 to remove reference to redundant documents (highlighted).  <i>Chapter 5 provides a response to the requirements of Schedule 1 to the Development Plan Overlay, and a response to the Larundel Urban Village and Development Considerations (Revised 2001), to the City of Darebin's North East Corridor Strategic Plan (1998), and reference to the Lancaster Gate Development Plan (2001, amended 2007).</i>  Below note added to provide link to previous note on status of documents in the appendices (highlighted).  <i>Refer to note regarding Appendices on page 3.</i>
7	1.4 Preparation of the Polaris Development Plan (PDP)	Amendment of bullet point 7 to remove reference to redundant documents. Bubble added.  <i>The review of statutory and strategic planning controls relevant to the site, including the Darebin Planning Scheme, the North East Corridor Strategic Plan (1998), the Darebin Open Space Strategy (1999) and the Darebin Social Profile (1999).</i>
9	All	Information outdated and not required under DPO1.  Superseded stamp added to sheet and note. This change was recommended by VCAT– refer to paragraph 61 of the Order (highlighted).  <i>Refer to Local Planning Policy Framework for details.</i>



Page	Section	Proposed Changes - 2021
10	All	<p>Information outdated and not required under DPO1.</p> <p>Superseded stamp added to sheet and note. This change was recommended by VCAT– refer to paragraph 61 of the Order (highlighted).</p> <p><i>Refer to Local Planning Policy Framework for details.</i></p>
11	<p>Municipal Strategic statement</p> <p>Open Space and other characteristics</p> <p>Figure 8</p>	<p>Information outdated and not required under DPO1.</p> <p>Superseded stamp added (x3) to relevant outdated documents and note. This change was recommended by VCAT– refer to paragraph 61 of the Order (highlighted).</p> <p><i>Refer to Local Planning Policy Framework for details.</i></p>
12	<p>All text</p> <p>Figure 9</p>	<p>Amendment of text relating to Local Context. This change was recommended by VCAT– refer to paragraph 61 of the Order (highlighted).</p> <p><i>The Polaris Estate is broadly located between Plenty Road and Main Drive, is identified in the Darebin Planning Scheme as the Lancaster Gate Neighbourhood Activity Centre. Significant change has occurred since the PDP was prepared with a number of stages completed generally in accordance with the vision and objectives of the Polaris Development Plan. The Polaris Estate currently accommodates a mix of uses including residential, retail, restaurant and office tenancies. The surrounding land remains predominantly occupied by low - medium density residential development.</i></p> <p><i>Land to the east of the site has been developed with the residential development of Lancaster Gate. This residential development is characterised by a mixture of detached and medium density house lots with varying architecture styles and building heights. An oval is situated to the east of the Polaris Estate and connects the these two precincts.</i></p> <p><i>Main Drive is located to the south. It is a local road with a carriageway width of approximately 7.5 metres and a single trafficable lane in both directions. Further, the site has an interface to La Trobe University's Centre for Higher Learning.</i></p> <p><i>Plenty Road is to the west of the site. It is a state arterial road (Road Zone Category 1 Road) with a dual carriage way and three (3) lanes of traffic in each direction divided by a raised central median that accommodates the No. 86 Tramline. The road reserve has a width of 42 metres. There is a tram stop located in the median at the Plenty Road and Main Drive intersection. Further to the west of Plenty Road is Bundoora Park.</i></p> <p>Figure 9 is out of date and has been updated accordingly. Bubble added.</p>
13	<p>2.1.3 Local Context Continued</p> <p>Surrounding Use and Development</p>	<p>Amendment of first paragraph to remove reference to La Trobe University Wildlife Park &amp; Agriculture Reserve. This change was recommended by VCAT– refer to paragraph 60e of the Order. Bubble added.</p> <p>Noted added to acknowledge outdated images at Figure 10. This change was recommended by VCAT– refer to paragraph 61 of the Order. Bubble added.</p>

Page	Section	Proposed Changes - 2021
	Figure 10	<i>Note – images out of date. Refer to urban context reports prepared in supporting of planning permit applications for details of the current and emerging urban environment.</i>
13a	Section 2.1.3 Local Context Continued	<p>New page detailing the emerging built form context and La Trobe University Melbourne Campus Master Plan (MCMP).</p> <p>Amendment of the last paragraph to address VCAT recommend change – refer to paragraph 60a of the Order (highlighted).</p> <p><i>Both the PDP and MCMP recognise the functional and built form interlinkages between the two sites. Given this, and considering the <del>limited-existing</del> sensitive interfaces, it is appropriate that the southern edge of the Polaris Town Centre i.e. Building 1B-16 on Lot S3, considers the emerging and future character of the La Trobe University Town Centre, introduces with a similar podium and tower form and greater scale of 7 to 10 up to 5 storeys, up to a maximum building height of 16 metres. A building of this height, in this location, will be in keeping with the character of the area.</i></p>
14	2.1.4 Historical Context	<p>Notes added reference most recent conservation management plan prepared (highlighted).</p> <p><i>*This section to be read in conjunction with Conservation Management Plan Review prepared by Lovell Chen dated February 2013.</i></p> <p>Notes added to confirm status or heritage buildings (highlighted).</p> <p>Note added to refer to current Heritage Overlays (highlighted).</p> <p><i>*Refer to Darebin Planning Scheme for relevant planning policy and controls.</i></p>
15	All	<p>Note added reference most recent Conservation Management Plan prepared (highlighted).</p> <p><i>*This section to be read in conjunction with Conservation Management Plan Review prepared by Lovell Chen, dated February 2013.</i></p>
16	2.2.1 Land Details and Area	<p>Notes added regarding status of subdivision and feature and level survey (highlighted).</p> <p><i>*Appendix A - Subsequent subdivision of the original Polaris Development Plan area has been undertaken. Please obtain latest Certificate of Title for details.</i></p> <p><i>*Appendix B – Feature Survey Plan is out of date due to multiple stages of the Polaris Development Plan being constructed and completed.</i></p>
17	2.2 Site context Continued	<p>Note added regarding status of urban context (highlighted).</p> <p><i>*Significant change has occurred since this was written, with a number of stages completed generally in accordance with the vision and objectives of the development plan. Current information will be provided within the Urban Context Report at the planning permit application stage and should be read in conjunction with the above.</i></p>

Page	Section	Proposed Changes - 2021
18	2.2 Site context Continued  2.2.10 Archaeological and Aboriginal Matters	Note added regarding status of urban context (highlighted).  <i>*Significant change has occurred since this was written with a number of stages completed generally in accordance with the vision and objectives of the development plan. Current information will be provided at the planning permit application stage and should be read in conjunction with the above.</i>  Redundant heading removed (2.2.10 Archaeological and Aboriginal Matters). Bubble added.
19	2.2.10 Archaeological and Aboriginal Matters  2.2.11 Fauna	Note added regarding status of urban context (highlighted).  <i>*Significant change has occurred since this was written with a number of stages completed generally in accordance with the vision and objectives of the development plan. Current information will be provided at the planning permit application stage and should be read in conjunction with the above.</i>
20	2.3.2 Mixed Use Zone	Purpose of Mixed Use Zone deleted. Out of date and not required under DPO1. Bubble added.
23	2.3 Planning Scheme Policy and Statutory Context Continued	Superseded stamp applied and note added (highlighted).  <i>Refer to Darebin Planning Scheme for relevant planning policy and controls.</i>
24	2.3 Planning Scheme Policy and Statutory Context	Superseded stamp applied and note added (highlighted).  <i>Refer to Darebin Planning Scheme for relevant planning policy and controls.</i>
26	Figure 26	Figure 26 updated to delete reference to the 'La Trobe University agricultural reserve'. This change was recommended by VCAT– refer to paragraph 60e of the Order. Bubble added.  Note added regarding status of ITP (highlighted).  <i>*The Integrated Transport Plan (ITP) prepared by Ratio Consultants in July 2009, is intended to act as 'base' reference document and is to be used as a guide only for future development within Polaris.</i>
27	3.1.1 Pedestrian Access  3.1.4 Bicycle Access	3.1.4 Bicycle Access - Amendment of text (highlighted).  <i>Access to bicycle facilities and an informal bike route is located along Main Drive and an off-road bike path leading to central Melbourne (the Darebin Creek Trail) is approximately 1.5 kilometres to the south-west, off Plenty Road.</i>
28	3.1.5 Public Transport	Details relating to Public Transport updated (highlighted).  <i>Polaris has good access to public transport services, with tram route 86 and bus routes 566 and 563 operating on Plenty Road along the frontage of the Polaris 3083 development. Additional bus routes within convenient walking distance of the site have been summarised below.</i>

Page	Section	Proposed Changes - 2021
		<p><b>Bus services</b></p> <ul style="list-style-type: none"> <li>• Route 566 (Lalor-Northland via Plenty Road, Childs Road, Grimshaw Road)</li> <li>• Route 563 (Greensborough – Northland via Mill Park, Plenty Road, Diamond Creek Road)</li> <li>• Route 548 (Cotham Road, Kew to La Trobe University Bundoora)</li> <li>• Route 246 (Elsternwick- La Trobe University via Clifton Hill, St Kilda Junction)</li> <li>• Route 250 (Garden City – La Trobe University via City)</li> <li>• Route 340 (City-La Trobe University via Freeway (all stops))</li> <li>• Route 350 (City-La Trobe University via Freeway (limited pickup and set down))</li> <li>• Route 550 (Northland-La Trobe University via Waterdale Road)</li> <li>• Route 551 (Heidelberg – La Trobe University Interchange)</li> </ul> <p><b>Tram Services</b></p> <ul style="list-style-type: none"> <li>• Route 86 (Bundoora RMIT-Waterfront City Docklands)</li> </ul> <p>Tram Route 86 and Bus Routes 566 and 563 operate along Plenty Road while Bus Route 548 operates along Main Drive. The remaining bus services operate from the bus terminus near the Plenty Road and Kingsbury Drive intersection, 750 metres from the site.</p> <p>A privately operated and free 'Glider' bus service also connects the town centre with La Trobe University, stopping (Stop 10) on Copernicus Crescent before Main Drive. It operates Monday to Friday 8.30am to 9.10pm.</p> <p>Planning Scheme Amendment VC148 was gazetted on the 31 July 2018, which introduced an update to Clause 52.06–Car Parking. In instances where any part of the land is identified as being within the Principle Public Transport Network (PPTN), as is the case with Polaris, Column B of Table 1 is the applied parking rate.</p>
29	3.4 Building Height	<p>Amendment to last paragraph (highlighted).</p> <p>Other parts of Polaris could accommodate greater building heights where they are located at more distant or at lower elevations to the existing heritage buildings. The lower topographical locations of the site provide the opportunity for greater building height, for example, at the intersection of Plenty Road and Main Drive. On higher ground, dependent upon construction methods (to be determined at the planning permit stage for each precinct) three storey development will have a comparable height to the building heights of adjoining developments, as well as the existing heritage buildings which have provided a benchmark.</p>
30	3.5 Vegetation  3.7 Exposure and Interface  Figure 30	<p>3.5 Vegetation</p> <p>Amendment of last paragraph to reference correct Clause (highlighted).</p> <p>No vegetation Protection Overlay or Environmental Significance Overlay covers the site, however the provision of Clause 52.17 Native Vegetation of the Darebin Planning Scheme applies. Clause 52.17-6 7 contains a Table of Exceptions that may be applicable to the site.</p> <p>3.7 Exposure and Interface - Amendment of paragraph 4 to delete reference to agriculture reserve.</p>

Page	Section	Proposed Changes - 2021
		<p>The south-west interface abuts Main Drive, which forms part of the local street network. To the south-east of Main Drive is La Trobe University's, <b>agriculture reserve</b>. Careful management of the visual interface with the <b>agricultural reserve is required at the University</b> is required given it has been targeted for development.</p> <p>Amendment of paragraph 7 to delete reference to agriculture reserve.</p> <p>The south-east interface of the site is <b>the agricultural reserve of La Trobe University</b>. This requires careful treatment in relation to layout, built form and access.</p> <p>Figure 30 updated to delete reference to the 'La Trobe University agricultural reserve' and include a new reference to the LaTrobe University Master Plan. Bubble added.</p>
31	Figure 31 Figure 32	<p>Figure 31 and 32 updated to delete reference to the 'La Trobe University agricultural reserve'. Bubble added.</p> <p>Note added regarding interface with LaTrobe University (highlighted).</p> <p><i>Refer to page 13a for south-west interface details at La Trobe University.</i></p>
40		<p>Amendment of text to include reference to apartment in Precinct 1B (highlighted).</p> <p><i>Mixed use neighbourhood activity centre providing retail, commercial, office supermarket, food and drink premises (take away food premises, restaurant, cafes), residential (shop-top housing, student accommodation, <b>apartments</b>, residential hotel) and civic community centre, plaza land used.</i></p>
44	4.4.2 Precinct 1B Continued  Main Drive Frontage	<p>4.4.2 Precinct 1B Continued</p> <p>Changes recommended by VCAT– refer to paragraph 60b &amp; 60d of the Order.</p> <p>Figure 58 updated to show correct building envelope for 1B-01 and 1B-02 (highlighted and bubbled).</p> <p>Main Drive Frontage - Amendment of the second paragraph to address VCAT recommendation (changes highlighted).</p> <p><i>A building adjacent to Main Drive (located in the intervening area between Main Drive and the car park) (nominated as Building 1B-16 in Figure 49a, 59, <b>60 and 60a</b>), may possibly be used for either commercial or residential uses, to be determined at the planning permit stage. This building site presents an opportunity to demarcate the southern boundary of the Polaris Town Centre with a memorable building form and simultaneously provide a seamless transition to the La Trobe University Melbourne Campus. It has the potential to support a building height of up to seven (7) storeys <b>predominantly three (3) storey envelope with opportunity for a recessed fourth or fifth storey</b>, having regard to local context considerations and responding to the 7-10 storey, 'podium and tower' indicative future built form character anticipated at the northern edge of the university campus. At up to 7.5 storeys <b>and with a maximum building height of 16 metres</b>, this building will feature a high quality,</i></p>

Page	Section	Proposed Changes - 2021
		<p><i>contemporary architectural design, appropriate landscaping treatments to Main Drive and a robust palette of materials and finishes. Together these elements will significantly enhance the existing public realm amenity along Main Drive and provide facade activation whilst avoiding blank walls and minimising visual bulk perceptions.</i></p> <p>Figure 59 replaced and bubble added (note changes to building envelope and interfaces).</p> <p>Figure 60 updated with 7 5 storey building fronting Main Drive. Bubble added.</p> <p>Deletion of the 'benchmark' images for 1B-16 on page 44 that shows 'indicative contemporary building typology for student accommodation for building 1B-16'. Bubble added.</p>
44a	Figure 60a	<p>Changes recommended by VCAT– refer to paragraph 60b &amp; 60c of the Order.</p> <p>New indicative section diagrams to reflect amended building height (5 storeys). Figure 60a confirmed to be acceptable to Council in email correspondence of 24 November 2020. Bubble added.</p>
53	<p>Section 4.7.1 Development Envelope</p> <p>Front Setback</p> <p>Building Heights</p>	<p>Changes recommended by VCAT– refer to paragraph 60b &amp; 60d of the Order.</p> <p>Front Setback</p> <p>Amendment of second paragraph (highlighted).</p> <ul style="list-style-type: none"> <li>- Precinct 1B – Neighbourhood Activity Centre</li> </ul> <p><i>Generally 0 metre setback for retail and commercial buildings.</i></p> <ul style="list-style-type: none"> <li>- Main Street Frontage</li> </ul> <p><i>Generally 0 metre setback for retail and commercial buildings. Varies for residential. At least 2 metres for residential buildings (Building 1B-16) for the upper levels above the lower three (podium) levels with generous setbacks to Linakers Cottage.</i></p> <p>Building Heights - Amendment of fourth paragraph to address reduced building height (highlighted).</p> <p><i>Having regard to local context considerations and the future 7-10 storey, podium/tower built form character anticipated at the northern edge of La Trobe University under the MCMP, there is also opportunity for building height of up to 7 5 storeys for Building 1B-16 with generous setbacks to Linakers Cottage.</i></p>
59	High Retention Trees	<p>Text added to confirm agreement with Council regarding street tree removal in Main Drive (highlighted).</p> <p><i>*High Retention Trees continued...</i></p> <p><i>Lot S3 (1B-16): As per agreement with Council, Tree 199 is to be removed. A tree amenity off-set payment is to be paid to Council (using the city of Melbourne method) prior to first occupation of an approved building.</i></p>

Page	Section	Proposed Changes - 2021
		<i>Replacement trees are to be planted to off-set removal of Tree 199. A landscape plan showing the proposed location of the trees inclusive of complimentary vegetation is to be prepared in support of any planning permit application lodged with Council. Replacement tree species to be to Council requirements.</i>
60	Figure 80	Image updated to reflect agreement with Council regarding street tree removal in Main Drive (highlighted).  <i>Figure 80 'Landscape Concept Plan / Overall Master Plan (Indicative Only)'</i>  Note added and bubbled.  <i>Main Drive Replacement trees to be planted to off-set removal of Tree 199. Tree species to be to Council requirements.</i>  Amended landscaping treatments surrounding building envelope for Building 1B-16. Land use/tenant labels added to buildings.
62	Figure 82	Image updated to reflect agreement with Council regarding street tree removal in Main Drive.  Note added and highlighted - <i>Precinct 1B building envelopes updated. Refer to Figure 49a for current detail.</i>  Tree 199 deleted from Tree Retention Plan. Bubble added.
63	Figure 83	Image updated to reflect agreement with Council regarding street tree removal in Main Drive (highlighted)  Note added and highlighted - <i>Precinct 1B building envelopes updated. Refer to Figure 49a for current detail.</i>  Tree 199 classed as 'Recommended for removal by Arborist'. Bubble added.
71	4.13 Environmental Sustainable Development (ESD)	ESD statement outdated. Note added to refer to current Darebin Planning Scheme for details (highlighted).  <i>*Refer to Darebin Planning Scheme for current ESD requirements.</i>
81	5.3.3 Larundel Urban Village	Bullet point 3 amended and highlighted.  <i>- Allows opportunity for student housing to be located within the existing buildings within Precinct 2C and Precinct 1B (e.g. 1B-16).</i>
82	Section 5.3.4 Building Heights	Changes recommended by VCAT– refer to paragraph 60b of the Order  Figure 86 updated to reference amended building heights for Building 1B-16. Figure bubbled and amended heights highlighted.  Amendment of paragraph 5 (highlighted).  <i>The PDP provides the opportunity for a range of building heights on the site. A conceptual indication of these heights is provided in Figure 86. Although</i>

Page	Section	Proposed Changes - 2021
		<p>generally anticipated to be between one and three storeys (above ground), there is capacity for greater overall building height at the south- west of the site, along Plenty Road and Main Drive (Buildings 1B-0, 1B-02 and 1B-16). Three storey developments have been constructed in the adjoining Lancaster Gate development site which also varies the building height guidelines identified in the NECSP.</p> <p>New paragraph 13 regarding building height for Building 1B-16 (highlighted).</p> <p><i>There is also opportunity for greater building height, up to five (5) storeys for Building 1B-16 having regard to local context considerations and in response to the future 7-10 storey, podium/tower built form character anticipated at the northern edge of La Trobe University under the MCMP.</i></p>
86	Section 6.2 Yield	<p>Text amended and highlighted.</p> <p>Precinct 1B – Accommodation</p> <p><del>Student accommodation</del> <i>Dwellings within a residential building (200 dwellings).</i></p>
87	Section 6.4 Future Planning Approvals within Polaris	<p>Text added to confirm agreement with Council regarding street tree removal in Main Drive (highlighted)</p> <p><i>*6.4 Future Planning Approvals within Polaris continued...</i></p> <p><i>Lot S3 (1B-16): As per agreement with Council, Tree 199 is to be removed. A tree amenity off-set payment is to be paid to Council (using the City of Melbourne method) prior to first occupation of an approved building.</i></p> <p><i>Replacement trees are to be planted to off-set removal of Tree 199. A landscape plan showing the proposed location of the trees inclusive of complementary vegetation is to be prepared in support of any planning permit application lodged with Council. Replacement tree species to be to Council requirements.</i></p> <p>Below note added to provide link to previous note on status of documents in the appendices (highlighted).</p> <p><i>* These documents are intended to act as 'base' reference documents and are to be used as a guide only for the future development within Polaris.</i></p>



Consolidated PDP - March 2021  
Proposed Amendment for Lot S3 (1B-16)

DAREBIN CITY COUNCIL  
Approved plan pursuant to  
Development Plan Overlay - Schedule 1  
Ref: POD/1/2007/E  
Date: 12/11/2019

*[Signature]*  
Total Pages: 89  
Signature for the Responsible Authority

Pursuant to Clause 43.04, Schedule 1 of the Darebin Planning Scheme, this is a copy of the approved Addendum to the Development Plan (being the Development Plan approved on 27 November, 2001) for the land known as the former Larundel Psychiatric Hospital, Plenty Road, Bundoora.

*[Signature]*  
Peter Rollis  
CO-ORDINATOR STATUTORY PLANNING

Date Approved *23/9/09*  
Document 1 of 3

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# Polaris 3083 Development Plan



15 October 2008  
(Amended 31 July 2009)

Amended 18 October 2019  
Amended 04 March 2021



# Polaris 3083 Development Plan

## Acknowledgements

We would like to thank the following organisations for their contribution to the Polaris 3083 Development Plan:

### Owner

- Deal Corporation (LG) Pty Ltd

### Developer

- Deal Corporation (LG) Pty Ltd

### Project Manager

- Deal Corporation Pty Ltd

### Planners, Landscape Architects and Architects

- HASSELL

### Specialist Consultants

- Biosis Research
- Bosco Jonson
- Bryce Raworth
- Charter Keck Cramer
- Cundall
- Gallagher Jeffs
- Ratio Consultants
- Stephen Fitzgerald Arboriculture
- The O'Neill Group

Note - list only acknowledges consultants who contributed to the PDP of 2009 only.

Consultant team has subsequently changed each iteration thereafter.

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# Polaris 3083 Development Plan

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- Appendix H: ESD Statement, prepared by Cundall (3 July 2007)

**Note -**

\*Appendix A - Subsequent subdivision of the original Polaris Development Plan area has been undertaken. Please obtain latest Certificate of Title for details.  
 \*Appendix B - Feature Survey Plan is out of date due to multiple stages of the Polaris Development Plan being constructed and completed.  
 \*Appendix C-H. These documents are intended to act as 'base' reference documents and are to be used as a guide only for the future development within the Polaris Village Precinct.



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PLANNING COMMITTEE MEETING 12 JULY 2021

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1.0 Introduction



## Polaris 3083 Development Plan

### 1.1 Polaris 3083

The site is located at 1056-1140 and 1142 Plenty Road, Bundoora and forms part of the Lancaster Estate (formerly the Larundel Psychiatric Hospital site). The current approved land use and development strategic master planning document for the site is the *Lancaster Gate Development Plan (2001)*. An addendum was approved by the responsible authority on 20 July 2007 to amend the *Lancaster Gate Development Plan (2001)*. This addendum relocated the location of the multi-purpose community centre from within the Residential Precinct of the Lancaster Gate Estate (which is now indicated for Stage 5 residential subdivision), to one of the former Larundel Psychiatric Hospital buildings (located to the west of the oval, within the Mixed Use Precinct) to be the location of the proposed community centre.

This master plan contains two precincts: a mixed use precinct and a village precinct. Consistent with this approach, the *Polaris 2008 Development Plan (2008)* refines and provides additional detail and guidelines to the future land use and development envisaged to occur on the site.

A significant number of specialist consultants have prepared reports relating to specific aspects of the site during the past two decades which has also been supplemented by recent specialist input. Thus, a comprehensive information base is available regarding the site. This wealth of research and information has assisted with the preparation of the master plan, and has informed the holistic and integrated design response that has been adopted.

This is evident in the mix and location of land uses, vegetation retention and removal, building layout and configuration, road design and dwelling diversity. The design is a balanced response to the physical attributes, and opportunities and constraints of the site. In all, this master plan should be considered in its entirety whereby each precinct and sub-precinct interconnect and relate with each other and with the heritage of the site.

The original *Lancaster Gate Development Plan (2001, amended 2007)* encompasses an area of approximately 25 hectares. The *Polaris 3038 Development Plan (2008)* applies to the remaining undeveloped land in the western half of the site which totals approximately 10.59 hectares to be known hereafter as Polaris 3083 and referred to as 'PDP' or 'Polaris' in this document. Refer to Figure 1 for details.

The eastern half of the original Lancaster Gate Estate has been developed with residential development generally in accordance with the *Lancaster Gate Development Plan (2001, amended 2007)*.

Deal Corporation was introduced to this site by VicUrban through their Expression of Interest process, followed by an extensive and rigorous bid assessment process. VicUrban awarded the project to Deal Corporation after assessing the overall 'Master Plan' included herein.

Polaris, which is owned by Deal Corporation (LG) Pty Ltd and managed by Deal Corporation, will benefit from Deal Corporation's twenty years of experience in property development. Their combined experience in residential and commercial developments throughout Melbourne will benefit the Polaris 3083 Mixed Use project as it comes to fruition.

We are certain that this project will be one we are all proud to be associated with.

### 1.2 Scope, Purpose and Objectives of the Polaris 3083 Development Plan

The scope of the Polaris 3083 Development Plan (PDP) is to prepare a strategic plan that will guide the future land use and development of the remainder of the Lancaster Gate Estate.

The purpose of the PDP is to provide broad land use and development guidelines for the remaining undeveloped portion of Lancaster Gate. The proposed PDP ensures a holistic and integrated approach is adopted for the development of the remainder of the Lancaster Gate Estate.

The PDP maximises the opportunities of the site and resolves constraints to achieve an elevated and high net community benefit for the immediate area and for the Darebin municipality.

The PDP will not replace the current Schedule 1 to the Development Plan Overlay or the planning permit requirements of the relevant zone or the Development Plan Overlay of the Darebin Planning Scheme.

The objective of the PDP is to create a vibrant and high-quality environment for its future community, surrounding residents and the wider community. The PDP employs best practice urban design, planning, architecture and landscape architecture to achieve a mixed use 'live/work/shop/play' environment that is sensitively and sensibly arranged and integrated on the subject land with respect to its own intrinsic assets and characteristics, and also in response to the established patterns of adjacent existing urban fabric.

The key aims and objectives are:

- Urban design excellence
- Community and affordability initiatives
- Environmentally sustainable development

Except as shown in an amended development plan (endorsed by a responsible authority) the layout of Precinct 1A and Precinct 1B is to be generally in accordance with Figure 49b (page 40) of the PDP.

This amendment to the Lancaster Gate Development Plan is required under Schedule 1 to the Development Plan Overlay pursuant to Clause 43.04 of the Darebin Planning Scheme. Clause 43.04-3 states that the Development Plan may be amended to the satisfaction of the responsible authority. The new Development Plan must meet the requirements of Schedule 1 to the Development Plan Overlay.

### 1.3 Overview of the Polaris 3083 Development Plan

The PDP submission contains a total of six (6) chapters.

Chapter 1 provides a general introduction regarding the preparation, scope, purpose and objectives of the PDP.

Chapter 2 introduces the site in relation to its regional, municipal, local, historical and planning context. Characteristics and features of the site are outlined to provide the reader with a familiarity of the site. An overview of the planning scheme policy contained within the Darebin Planning Scheme and statutory planning context is provided.

Chapter 3 analyses the opportunities and constraints of the site relating to circulation and access, land uses, built form and height, vegetation and the public realm, main road exposure, heritage roads, landmarks and focal points, topography and orientation.

Chapter 4 outlines the vision and principles that underlie the PDP and outlines urban design guidelines for the future use and development of Polaris. Features of Precinct 1 and Precinct 2 (and their sub precincts) that comprise the PDP are introduced through text, conceptual images, plans and indicative benchmark images.

**Chapter 5 provides a response to the requirements of Schedule 1 to the Development Plan Overlay, to the City of Darebin's North East Corridor Strategic Plan (1998), and reference to the Lancaster Gate Development Plan (2001, amended 2007).**

Chapter 6 provides an overview of the potential staging of the development and residential yield. It also outlines the process for implementation and the development approach process, development contribution and open space contribution and conditional approval of the PDP.

The Appendices include copies of all consultants' reports which have contributed to the PDP, including an overall site plan, integrated transport plan, heritage assessment, arborist report, engineering services report and a retail economic assessment. Certificate of Title details are also included in the Appendices. **\*Refer to note regarding Appendices on page 3.**



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# Polaris 3083 Development Plan

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## 1.4 Preparation of the Polaris Development Plan (PDP)

The Polaris 3083 Development Plan (2008) has been prepared in consultation with Darebin City Council, VicUrban, key stakeholders and referral authorities over a three and a half year period. The key steps in this process have included:

- The engagement of a number of specialist sub consultants in collaboration with the project design team
- The preparation by HASSELL, on behalf of Deal Corporation, of the VicUrban Bid Document (2005). The Bid Document was subsequently approved by VicUrban and Deal Corporation was engaged to undertake an amendment to the PDP
- The development of specific aims and objectives for the PDP
- The creation of a vision statement for the PDP
- The undertaking of a thorough site analysis and identification of the opportunities and constraints of the physical and existing attributes of the site, as well as attributes afforded by its location within the Darebin municipality
- An assessment of the historical context of the site by a heritage consultant
- The review of statutory and strategic planning controls relevant to the site Darebin Planning Scheme, the North East Corridor Strategic Plan (1998)
- The input from various specialist consultants, such as traffic, environmental sustainable design, economic, heritage, arboricultural, survey and engineering to inform the design of the master plan and the PDP
- Consultation with various referral authorities (ie service utilities and VicRoads) and the City of Darebin
- The development of a contents page and structure of the report by the project team, in conjunction with Darebin City Council
- The undertaking of a number of workshops and presentations to Darebin City Council officers and councillors throughout the development of the PDP
- The construction of a scaled model (1:500) to assist with Council workshops and presentations. This model will be on display at Darebin City Council Offices during the exhibition of the amended PDP

The PDP is consistent with the Darebin Planning Scheme and satisfies the requirements of Schedule 1 to the Development Plan Overlay.

Under the provision contained within Clause 43.04 of the Darebin Planning Scheme, we formally lodge a request with the responsibility authority to consider and approve the Polaris 3083 Development Plan (2008).



Figure 1 Subject Site: 1056-1140 and 1142 Plenty Road, Bundoora (Source: HASSELL, 2008)

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2.0 Context





# Polaris 3083 Development Plan

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\*Refer to the Local Planning Policy Framework for details.

## 2.1 Locational Context

### 2.1.1 Regional Context

Polaris is located approximately 15 kilometres north east of Melbourne and is located within the Kingsbury/Bundoora Precinct of the City of Darebin. (Refer to Figure 2)

The City of Darebin borders the City of Whittlesea to the north, the City of Moreland to the west, and the City of Banyule to the east.

The age distribution (Figure 3) and household structure (Figure 4) of the study region supports a variety and diversity of residential land use types for future use and development on the site.

By 2016, it is envisaged that the population of the City of Darebin will increase to 132,650 persons and by 2021, the population is envisaged to reach a total of 133,660 persons (Charter Keck Cramer, July 2008).

The most significant cohort of the 2001 age distribution of the study region is persons aged between 35 and 49 years. The study region comprises a higher proportion of persons aged between 25 and 34 years and in excess of 75 years than the Melbourne Statistical District, but a lower proportion of children aged between 5 and 17 years. (Charter Keck Cramer, 2008).

Charter Keck Cramer (2008) expect that the trade area for Polaris would comprise a lower proportion of older persons (60+) than the remainder of the City of Darebin and a higher concentration of children aged less than 17 years and their parents.

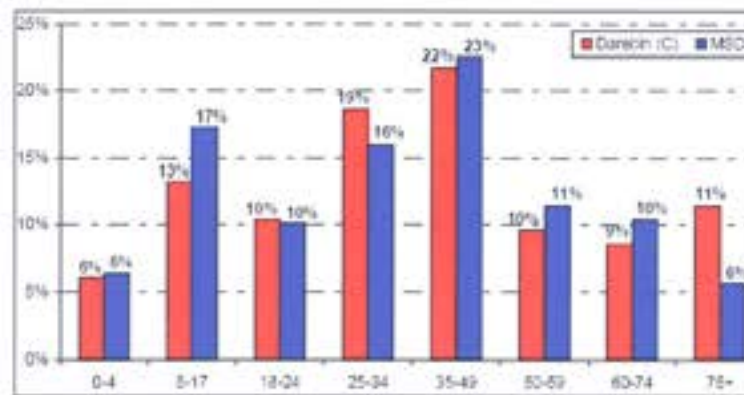


Figure 3 Age Distribution - Study Region (2001) (Source: ABS, Charter Keck Cramer)

	Darebin (C)	MSD
Couples without Children	24%	24%
Couples with Children	30%	37%
One Parent Families	12%	11%
Lone Person	29%	23%
Group Household	6%	4%

Figure 4 Household Structure - Study Region (2001) (Source: ABS, Charter Keck Cramer)



Polaris 3083 Development Plan

**SUPERSEDED**

\*Refer to the Local Planning Policy Framework for details.

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2.1 Locational Context Continued

2.1.2 Municipal Context

Kingsbury/Bundoora Precinct

Polaris is located in the north east corner of the City of Darebin, in the Kingsbury/Bundoora Precinct (Figure 5) which is approximately 900 hectares in size, and which constitutes 17% of the entire Darebin municipal area.

The majority of housing in the Kingsbury/Bundoora Precinct is freestanding houses on medium to large lots. There is a community need to increase the diversity of housing stock in this region. The area is acknowledged for its substantial amount of open space and parkland, including Bundoora Park.

La Trobe University provides employment, education, research and economic services within the City of Darebin. The proportion of young adults in the area is quite high, due to the high student population of La Trobe University.

In 2001, the City of Darebin had an enumerated resident population of 127,890 persons. Between 1996 and 2001, the population of the municipality increased marginally by 0.4%, which represents an average annual rate of growth of approximately 0.1%. This figure was considerably lower than that recorded across the Melbourne Statistical Division during the same period (+ 1.5% per annum) (Charter Keck Cramer, 2008).

Within the study region, the proportion of white collar employees exceeded the proportion of blue collar employees in 2001, accounting for 60% and 40% of employees respectively. Across the Melbourne Statistical Division, white collar workers accounted for 63% of employed persons while blue collar workers accounted for 37% of employed persons. It is expected that the trade area would contain a higher concentration of professionals and small business owners (managers) than the remainder of the City of Darebin, given the generally higher housing costs within the trade area (Charter Keck Cramer, 2008).

In 2001, the median weekly household income in the study region was in the \$700-\$799 income bracket, which was lower than that recorded across the Melbourne Statistical Division (\$800-\$899). The study region comprised a considerably higher proportion of households with incomes of less than \$500 (37%) than the Melbourne Statistical Division (28%), and a lower proportion of households with median incomes in excess of \$1,000 per week, accounting for 34% of households within the study region compared to 44% of households across the Melbourne Statistical Division (Charter Keck Cramer, 2008).

Charter Keck Cramer (2008) expect that the trade area would comprise a higher proportion of couples with children type families, and a lower proportion of lone person and group households.

The occupational structure and household income characteristics of the trade area support a variety and diversity of residential land use types for future use and development of the site.



Figure 5 Municipal Context of Site (Source: Darebin Planning Scheme, Clause 21.01, page 4)

	Darebin (C)	MSD
Managers & Administrators	6%	9%
Professionals	23%	21%
Associate Professionals	11%	12%
Trades Persons & Related	12%	12%
Advanced Clerical & Service	3%	4%
Intermediate Clerical, Sales & Service	17%	17%
Production & Transport	9%	8%
Elementary Clerical, Sales & Service	10%	10%
Labourers & Related	9%	8%

Figure 6 Occupational Structure - Study Region (2001) (Source: ABS, Charter Keck Cramer, 2008, page 15)



Figure 7 Median Household Income by Income Bracket - Study Region (2001) (Source: ABS, Charter Keck Cramer, 2008)

# Polaris 3083 Development Plan

## 2.1 Locational Context Continued

### 2.1.2 Municipal Context Continued

#### Municipal Strategic Statement **SUPERSEDED**

According to Darebin City Council's Municipal Strategic Statement, the City of Darebin:

- Comprises an area of approximately 53 square kilometres
- Includes the suburbs of Northcote, Alphington, Fairfield, Thornbury, Preston, Reservoir, Kingsbury and Bundoora
- Has a culturally diverse community
- Is well placed in terms of access with its close proximity to the airport and the western/northern ring roads
- Has a well developed and diverse retail structure
- Contains some significant environmental features and major open spaces including Gresswell Habitat Link, Gresswell Forest and Bundoora Regional Park
- Is predominately fully developed except for a number of major redevelopment opportunities in the Kingsbury/Bundoora Precinct
- Is well positioned due to the significant north-south transport link to take advantage of its relative proximity to employment, recreation, and entertainment opportunities in the CBD of Melbourne as well as the neighbouring inner and outer ring municipalities.

Darebin City Council's Framework Plan (Figure 8) identifies a future activity centre to be located on the site. The PDP is consistent with this vision, as it includes a Neighbourhood Activity Centre. **\*Refer to the Local Planning Policy Framework for details.**

#### North East Corridor Strategic Plan

The *North East Corridor Strategic Plan (1998) (NECSP)* is a strategic planning document for the Kingsbury/Bundoora Precinct of Darebin City Council. The NECSP is not an Incorporated Document in the Darebin Planning Scheme (pursuant to Clause 81.01 of the Scheme). As a reference document, it is a planning tool to assist in providing guidance to future developments located within the corridor.

Pursuant to Clause 43.04 of the Scheme, the provisions of Schedule 1 of the Development Plan Overlay states that "the responsible authority should consider whether the development or permit application is consistent with the North East Corridor Strategic Plan". The provision does not state that the development plan must be consistent with the land use and development guidelines contained within the NECSP.

Since the NECSP's inception and approval in 1998, significant time has elapsed. In particular, it is considered that the strategic vision of the *Larundel Urban Village Framework Plan* is a product of its time and its relevance today to Polaris has diminished.

State Planning Policy, during this time, has also changed and progressed.

*Melbourne 2030 - Planning for Sustainable Growth* identifies Polaris as a Neighbourhood Activity Centre, whereby urban consolidation and higher residential densities and a mix of uses within these centres is encouraged as well as new principles relating to good urban design for these centres. The site is not specifically identified in the NECSP as a Neighbourhood Activity Centre. (This is nominated in the *City of Darebin Retail Activity Centre Strategy, March 2005*).

The PDP is generally in accordance with the principles contained within The NECSP however it does include some variations to the guidelines and framework plan (particularly relating to front setbacks to Plenty Road and building heights), which achieve a better design and development outcome for the Polaris site than previously envisaged by the NECSP or the *Larundel Urban Village Framework Plan*.

These variations are not considered significant or to cause detriment. They are intrinsic and site responsive in context to the holistic and integrated design approach adopted for Polaris, and importantly contribute to the achievement of higher levels of net community benefit.

These variations are discussed in Section 5.3 of this document.

#### Open Space and other characteristics **SUPERSEDED**

The *Darebin Open Space Strategy (1999)* indicates that the Kingsbury/Bundoora Precinct contains 441.7 hectares of open space, which is approximately half the area of the Kingsbury/Bundoora Precinct and approximately 8.5% of the entire Darebin municipal area. This constitutes nearly half of the total area of open space in Darebin.

Bundoora Park is a significant area of public open space, and is located directly opposite and to the north-east of Polaris.

**\*Refer to the Local Planning Policy Framework for details.**

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Figure 8 Darebin City Council's Framework Plan (Source: Darebin Planning Scheme, Clause 21.04)

**\*Refer to the Local Planning Policy Framework for details.**



# Polaris 3083 Development Plan

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## 2.1 Locational Context Continued

### 2.1.3 Local Context

#### Adjoining Use and Development

The Polaris Estate is broadly located between Plenty Road and Main Drive, is identified in the Darebin Planning Scheme as the Lancaster Gate Neighbourhood Activity Centre. Significant change has occurred since the PDP was prepared with a number of stages completed generally in accordance with the vision and objectives of the Polaris Development Plan. The Polaris Estate currently accommodates a mix of uses including residential, retail, restaurant and office tenancies. The surrounding land remains predominantly occupied by low-medium density residential development.

Land to the east of the site has been developed with the residential development of Lancaster Gate. This residential development is characterised by a mixture of detached and medium density house lots with varying architecture styles and building heights. An oval is situated to the east of the Polaris Estate and connects the these two precincts.

Main Drive is located to the south. It is a local road with a carriageway width of approximately 7.5 metres and a single trafficable lane in both directions. Further, the site has an interface to La Trobe University's Centre for Higher Learning.

Plenty Road is to the west of the site. It is a state arterial road (Road Zone Category 1 Road) with a dual carriage way and three (3) lanes of traffic in each direction divided by a raised central median that accommodates the No. 85 Tramline. The road reserve has a width of 42 metres. There is a tram stop located in the median at the Plenty Road and Main Drive intersection. Further to the west of Plenty Road is Bundoora Park.

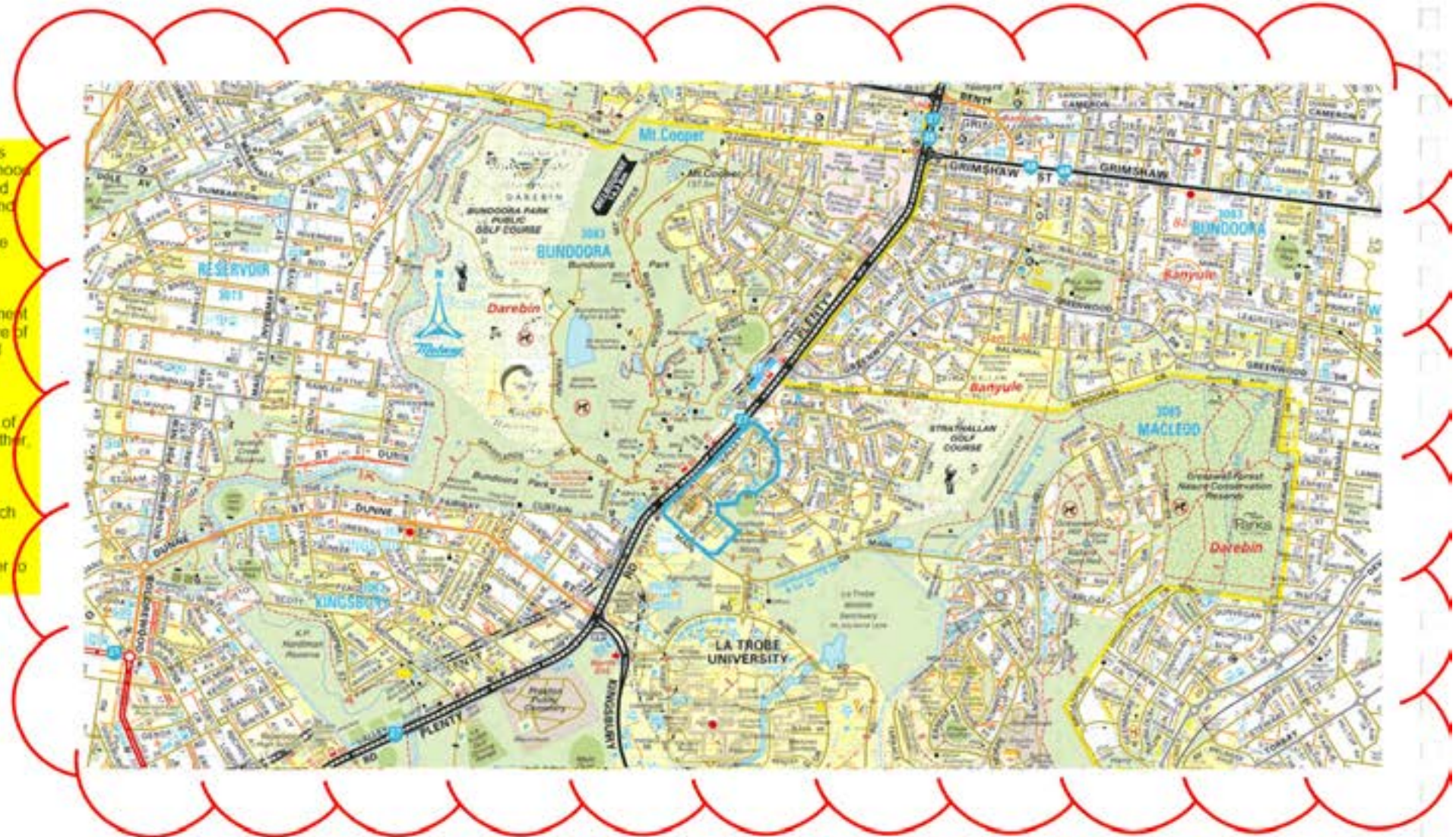


Figure 9 Local Context (Source: Melways, 2007, Map 19 G4)



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## 2.1.3 Local Context Continued

### Surrounding Use and Development

The site is surrounded by extensive open space, including Bundoora Park, La Trobe University, Strathallan Golf Course and Gresswell Forest. This creates the perception of Polaris as a somewhat separated residential community, as it is quite isolated from the surrounding estates and residential areas with the municipality and adjoining municipalities. A number of new residential communities have been developed within the region over the past 10 years or so, most of these on former institutional land. These include Springthorpe, Mount Cooper, Gresswell Grange and Lancaster Gate. The character of the locality is heavily influenced by new developments adjacent to older established suburbs, which contain a number of heritage buildings associated with former uses of the land.

The site is located between two sub-regional retail centres, Summerhill Village, 2 km to the south-west along Plenty Road, and Bundoora Square Shopping Centre, 1.5 km to the north-east on Plenty Road. These centres contain supermarkets as well as a number of specialty shops and department stores. Northland is a regional retail facility and is located 3.5 km to the south of the site. Access to retail is relatively easy via major arterial roads and trams, although it is too distant for pedestrian walkability. This provides the opportunity for the site to accommodate a neighbourhood scale retail facility serving the needs of the Polaris and Lancaster Gate communities, La Trobe University and other nearby residential areas.

A key attraction of the site for future residents will be access to education facilities. The full range of facilities is available from primary, secondary and tertiary campuses. Access to La Trobe University is perhaps the most significant asset for the site. Slightly further distant is RMIT Bundoora. All levels of education are available from the site within walking distance or via public transport.

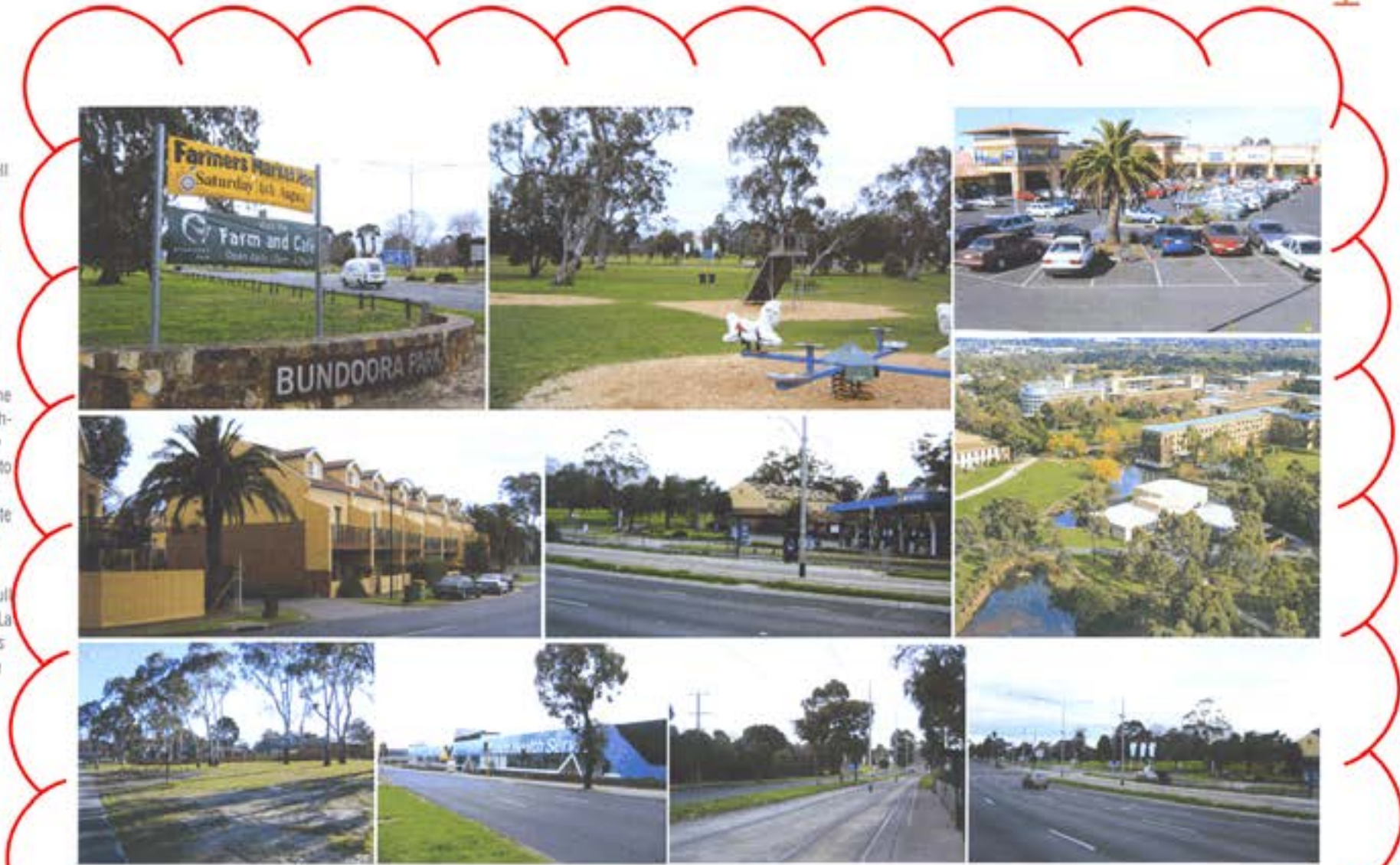


Figure 10 Existing land use and development within the surrounding community of Polaris (Source: HASSELL, 2007)

Note - images out of date. Refer to urban context reports prepared in supporting of planning permit applications for details of the current and emerging urban environment.

Polaris 3083 Development Plan

2.1.3 Local Context Continued

The Emerging Built Form Context & La Trobe University Melbourne Campus Master Plan

The PDP was originally prepared in consultation with key stakeholders over a three and a half year period in the mid 2000s. It involved thorough site opportunities and constraints analyses in respect of the physical, locational and existing site attributes.

La Trobe University is an important southern neighbour to the PDP site. Although predominantly zoned Public Use Zone 2, the PDP anticipates mutual benefits will be derived from the proximity of the PDP area to the university campus. In particular, the Polaris Town Centre was anticipated to service La Trobe University students and this, along with residential opportunities and the Glider private bus service, continues to this day. In a physical sense, the PDP seeks to better incorporate the northern edge of the La Trobe University campus.

At the time the PDP was first endorsed in 2009, the La Trobe University Melbourne Campus Master Plan had not been prepared and the PDP's future southern interface to the university was largely undefined.

The La Trobe University Melbourne Campus is now undergoing an unprecedented expansion. When the PDP was first endorsed in 2009, there were approximately 22,250 student enrolments at the Melbourne Campus. In 2017, enrolments grew to 25,235 students and the campus now aims to provide education facilities for over 40,000 students.

In 2014, La Trobe University launched its Melbourne Campus Master Plan (MCMP), providing a comprehensive guide to the future development of its Melbourne Campus. The Master Plan divides the campus into distinct neighbourhoods with land use, built form, urban design and transport expectations.

The northern campus edge, found immediately opposite the Polaris Town Centre to the south of Main Drive, falls within the 'Town Centre' neighbourhood and comprises a major development site in the short to medium term. It will create a very significant civic and commercial precinct servicing the northern suburbs of Melbourne.



Figure 10a: Approximate location of MCMP area relative to the PDP area

The MCMP advocates the blurring of the campus' edges, linking traditional academic uses with the community and commercial uses, and the creation of a seamless transition between the future Town Centre uses and the emerging Polaris Town Centre.

Indicatively, the MCMP proposes an iconic, 8 storey (approx. 36,000 GFA) gateway building opposite the PDP site. The building, known as TC2, will comprise a commercial built form and is identified for private sector delivered and operated housing. It is anticipated that this building's form and use will help bridge the existing colleges and the joint Polaris and University Town Centres. The building will have activated frontages to Main Drive and Science Drive which aligns with Copernicus Crescent. An indicative overall height range of 7 to 10 storeys is proposed with a podium height of 1-3 storeys at street level.

**Both the PDP and MCMP recognise the functional and built form interlinkages between the two sites. Given this, and considering the existing interfaces, it is appropriate that the southern edge of the Polaris Town Centre i.e. Building 1B-16 on Lot 53, introduces a scale of up to 5 storeys, up to a maximum building height of 16 metres. A building of this height, in this location, will be inkeeping with the character of the area.**



Figure 10b: Extract MCMP New Development



Figure 10c: Extract MCMP Built Form

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**\*This section to be read in conjunction with Conservation Management Plan Review prepared by Lovell Chen dated February 2013.**

2.1.4 Historical Context

The Polaris site was originally part of a large grazing property known as the Strathallen Estate. It was purchased, along with the adjacent Mont Park Estate, in 1909 by the state government as the site for a new psychiatric institution. The site was chosen largely because of a belief that clean air and a pleasant rural environment was beneficial to patients but also because it was much cheaper than land in built up areas closer to Melbourne. The existing psychiatric institution located at Kew dated back to 1872, and by 1935 was considered to be an overcrowded and outdated facility. A master plan for a new institution was drawn up by Percy Everett, Chief Architect of the Public Works Department. Construction began in 1938 and by 1942 most of the buildings on the site were completed. However, wartime shortage of materials and manpower left the site without basic services, including electricity, sewerage, hot water and access paths. The site was considered unfit for use as a psychiatric institution and was taken over by the Royal Australian Air Force as a training venue for the Women's Auxiliary Air Force. After the war, Larundel accommodated families left homeless by the chronic housing shortages of the period. The opening of Larundel for its intended use as a psychiatric institution did not take place until early 1949 and required the renovation of many of the buildings. Larundel was proclaimed as a separate mental hospital on 15 June 1953, as opposed to being a ward of the broader Mont Park complex. By then it housed 310 male and 77 female patients. In 1966, a modern brick building was constructed for industrial therapy work. In more recent times, changing attitudes to mental healthcare shifted the emphasis towards community based housing. As part of the state government's program of deinstitutionalisation, Larundel was closed in 1999.

**Hugh Linaker's cottage**

Hugh Linaker was appointed Superintendent of Parks and Gardens at the Mont Park mental health complex in 1912, with the task of overseeing the landscape design, planting and gardening works. Linaker was also responsible for landscaping a number of psychiatric institutions throughout the state as well other important commissions for public and private gardens. A cottage constructed in 1912 in the south-east corner of the Mont Park site served as Linaker's residence up until 1937. The cottage was later used by the Kingsbury Training Centre for the intellectually handicapped during the 1970s and 1980s.

The former Larundel site is located on the east side of Plenty Road, to the north of the former Mont Park complex. The principal buildings on the site are arranged symmetrically along an east-west axis and comprise: the administration building, hospital, dining room and male and female receiving and sub-receiving wards. A curvilinear driveway off Plenty Road provides the main point of access through the site. Secondary roads curve around the north and south sides of the Hospital Building (Building 2) leading to roundabouts which provide access to the wards. The building numbering adopted from this point onward in this report is a combination of that of the current PDP and the various previous reports relating to this site as specified on page 15.

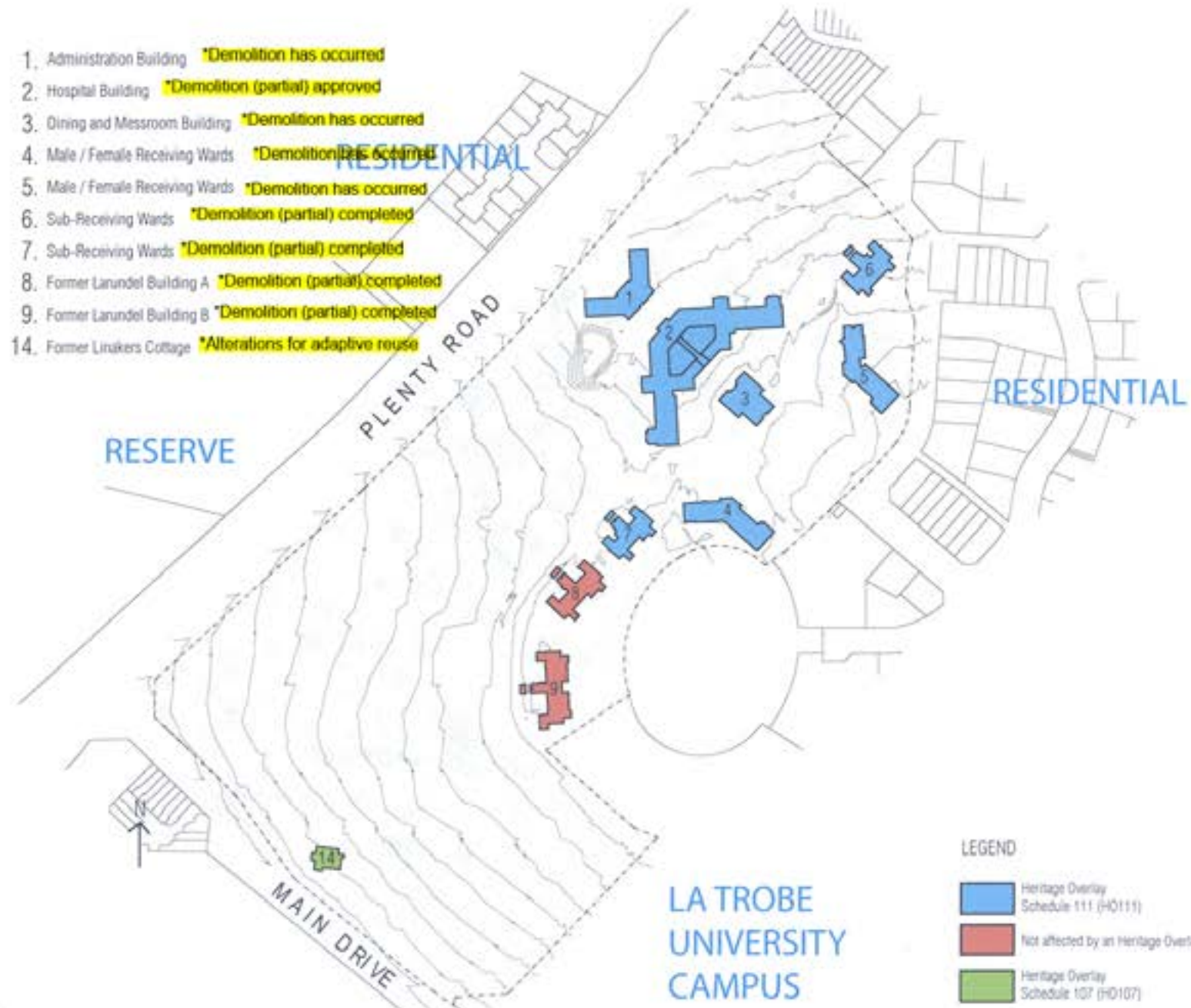


Figure 12 Heritage Building Reference Numbering System (Source: HASSELL, 2008 and the Larundel and Linaker's Cottage Plenty Road, Bundoora Conservation Management Plan, prepared by Atom Lovell & Associates, 2000)

LEGEND  
 Blue square: Heritage Overlay Schedule 111 (HO111)  
 Red square: Not affected by an Heritage Overlay  
 Green square: Heritage Overlay Schedule 107 (HO107)

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\*Refer to Darebin Planning Scheme for relevant planning policy and controls. 14

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**\*This section to be read in conjunction with Conservation Management Plan Review prepared by Lovell Chen dated February 2013.**

### Administration Building (Building 1)

A single-storey building with a central entry block and flanking wings set on an angle creating a boomerang plan form. Constructed of red-brick, it adopts the Tudor revival style of the other main Larundel buildings. The building was extensively damaged by a fire in 2005. Part of the central block remains.

### Hospital Building (Building 2)

A double-storey red-brick building, symmetrically planned with two diagonal wings extending from a central block. Each of the angled wings is bisected by a smaller wing at its centre. As per the other main buildings on the site, the hospital is designed in a Tudor revival style. Constructed of red-brick, it has terracotta tiled roofs with half timbered gable ends. A recent fire damaged parts of the interior.

### Dining and Messroom Building (Building 3)

The former dining and messroom building is a large Tudor revival style single-storey building with a symmetrical plan form. Constructed of red-brick, it has a steeply pitched roof with terracotta tile cladding and half-timbered gable ends. The building is substantially intact externally. A kitchen wing to the rear has been demolished.

### Male and Female Receiving Wards (Buildings 4 and 5)

Two red-brick single-storey buildings with identical, cranked plan forms. They have steeply pitched terracotta tiled roofs with half-timbered gables and flat roofed verandahs supported on brick piers to one side. Windows have multi-pane, timber-framed double-hung sashes.

### Sub-Receiving Wards (Buildings 6 and 7)

The former sub-receiving wards comprise a pair of identical double-storey red and clinker brick buildings, constructed in the Tudor revival style consistent with the other original Larundel buildings. The wards remain substantially intact externally. Internally, the buildings have undergone alterations and refurbishment. The sub-receiving wards were originally constructed as a matching group of four (to either side of the Hospital and Administration Buildings). The ward to the north of Building 6 has been demolished. The ward to the south of Building 7 still stands but is located outside the Heritage Overlay that covers the site.

### Former Linaker's Cottage (Building 14)

Linaker's Cottage is a single-storey late-Edwardian bungalow with roughcast rendered walls and a gambrel form corrugated iron roof. The asymmetrical front elevation has projecting bays flanked by a verandah with timber posts resting on tapered roughcast rendered piers. The building is in good condition.

Refer to Appendix D for additional detail and Figure 12 on the following page for a plan identifying the heritage buildings and their reference number.

The numbering reference system adopted as shown on Figure 12 is consistent with the numbering reference system contained within the *Larundel and Linaker's Cottage Plenty Road, Bundora Conservation Management Plan*, prepared by Allom Lovell & Associates (September 2000) and the *City of Darebin Heritage Citation (1999)* for the Former Larundel Mental Hospital and Former Kingsbury Training Centre.



Figure 11 Views of Larundel Mental Hospital, c.1946 (Source: State Library of Victoria Picture Collection)



# Polaris 3083 Development Plan

## 2.2 Site Context

### 2.2.1 Land Details and Area\*

The subject site comprises two individual allotments and has a total site area of approximately 10.59 hectares.

The main southern allotment of the site is shown in Figure 13. This allotment has a frontage to Plenty Road of 512.88 metres, a frontage to Main Drive of 231.55 metres and a total site area of 10.59 hectares.

The allotment located to the north of the site has a frontage to Plenty Road of 49.60 metres, and a total site area of 3,227 square metres, as shown in Figure 14. This land is affected by the Development Plan Overlay Schedule 1, but is not currently included in the development plan for the site (i.e. the Lancaster Gate Development Plan, 2001, amended 2007). It is proposed to include this land within the Polaris Development Plan (2008). This lot was formally used as a road.

For the purpose of the PDP the 'site' refers to both parcels of land. Refer to Appendix A for Certificate of Title details.

The northern and eastern boundaries of the subject land are irregular in shape. These boundaries are defined by the adjoining uses and development.

Refer to the Feature Survey Plan, prepared by Bosco Jonson (8 December 2006) located in Appendix B for further details.



Figure 13 Southern Allotment (Volume 10885 Folio 335) of Polaris. (Source: HASSELL, 2008)



Figure 14 Northern Allotment (Volume 10891 Folio 332) of Polaris (Source: HASSELL, 2008)

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\*Appendix A - Subsequent subdivision of the original Polaris Development Plan area has been undertaken. Please obtain latest Certificate of Title for details.

\*Appendix B - Feature Survey Plan is out of date due to multiple stages of the Polaris Development Plan being constructed and completed.

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## 2.2 Site Context Continued \*

### 2.2.2 Buildings and Structures

The remaining former Larundel Hospital buildings on the site (Figure 15) are the most significant elements that define the site's character. A total of 10 buildings remain on site, eight of which are affected by an Heritage Overlay under the Darebin Planning Scheme. Most of these are two-storey red-brick construction dating from 1938. There are three single-storey former hospital buildings as well as Linaker's Cottage. A significant characteristic of the existing buildings and roads on the subject land is their symmetrical arrangement centred on the largest of the remaining buildings. While the remaining two buildings (including the proposed community centre building) are not affected by the Heritage Overlay, they nonetheless contribute to the site's sense of place and provide the opportunity for adaptive reuse, as do the other heritage buildings.

The majority of the administration building located adjacent to the Plenty Road frontage has been damaged by fire, and is in poor appearance.

### 2.2.3 Roads, Circulation and Access

Vehicle access to the subject land from Plenty Road is provided at the north-west corner of the subject land. Vehicles may enter and exit the site at this point. An exit point is located in the centre of the Plenty Road frontage, where vehicles may exit the site only. These entry and exit points are infrequently used as the northern half of the site is not currently used for any purpose.

The main entrance to the site is from Main Drive to the south. The Plenty Road and Main Drive intersection is signalised with traffic lights. There is currently a left-hand turn slip lane from Plenty Road into Main Drive. Please refer to the *Integrated Transport Plan (2008)*, located in Appendix C for more detail regarding the existing road, circulation and access conditions of the site and adjoining the site.

There is limited formal car parking located on the subject land. Indented car parking bays are located in the road which runs adjacent to Linaker's Cottage.

The layout of the internal road network situated in the northern half of the site was originally designed to support the former use of the site, ie the Larundel Hospital. The roads are aligned in an axial arrangement to complement the symmetrical layout of the buildings. The roads are predominately constructed of bitumen and are in need of repair (refer to Figure 16).

A large stockpile of soil is located to the south of the former administration building. This stockpile currently blocks access through one of the bitumen roads.

### 2.2.4 Views

Due to the large size of the site, a number of different views can be obtained. There are minimal views of the existing buildings on the site from Plenty Road due to the existing vegetation on the site. Certain buildings have landmark status and are easily visible when viewed from close proximity within the site.

The symmetrical organisation of the existing buildings cannot be determined from within the site, but only from an aerial perspective.

Figure 17 illustrates the view of the site at the Plenty Road and Main Drive intersection, taken from the entrance of Bundoora Park.

### 2.2.5 Existing Land Use

The subject land is not currently used for any purpose other than the Lancaster Gate VicUrban Land Sales Office. The VicUrban Land Sales Office is located in Linaker's Cottage, which is identified under the Heritage Overlay and is located near the southern boundary of the site adjacent to Main Drive.

The existing buildings located in the northern half of the subject land are currently vacant and will require significant repair prior to use. As part of the PDP these buildings will be repaired and their contribution to the area significantly improved. This will not only maintain, but enhance, the quality, and heritage significance of these buildings.



Figure 15 Views of existing buildings and structures on the site (Source: HASSELL, 2007)



Figure 16 Views of the existing curvilinear road on the site (Source: HASSELL, 2007)



Figure 17 View of the site: Plenty Road and Main Drive intersection, taken from the entrance to Bundoora Park (Source: HASSELL, 2007)

**\*Significant change has occurred since this document was prepared with a number of stages completed generally in accordance with the vision and objectives of the development plan. Current information will be provided within the Urban Context Plan at the planning permit application stage and should be read in conjunction with the above.**

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2.2 Site Context Continued \*

2.2.6 Vegetation

The existing vegetation on the site provides a setting for the existing heritage buildings and creates a park-like character for most of the site. Trees located on the site are predominantly native eucalyptus species and were rated higher under the Stephen Fitzgerald Arboriculture Report 2007 due to their maturity, health and the visual amenity they provide. Many of the lower rated trees are considered weed species, such as the large number of *Fraxinus* (Ash), or have poor health and structure, or low habitat value.

The majority of the trees on the subject land are located in an ad hoc manner and there is no structure which could justify their location (refer to Figure 18).

Five hundred and thirty-one trees inspected as 408 individual specimens and 123 tree groups. Approximately 81 species or cultivated varieties were represented (a few require further identification). Forty-five species are native to Australia but no specimens were considered to be indigenous to the local area (see Table 2.2.6a for most common species represented on site).

Table 2.2.6a - Most Prevalent Species (represented by more than 10 specimens)

Species	No. of trees
Cupressus torulosa	10
Melaleuca amillarlis	11
Fraxinus 'Raywood'	12
Casuarina cunninghamiana	13
Eucalyptus sp.	13
Eucalyptus botryoides	13
Corymbia citriodora	13
Mixed species	14
Lophostemon confertus	18
Cupressus macrocarpa	24
Eucalyptus camaldulensis	29
Corymbia maculata	32
Fraxinus angustifolia subsp. angustifolia	54

(Source: Stephen Fitzgerald Arboriculture, June 2007)

At the time of the arborist assessment carried out by Stephen Fitzgerald Arboriculture in 2007, none of the trees on the site were considered to have 'very high' retention value, just less than 10% of the trees were considered to be of 'high' retention value, approximately 30% as 'moderate' retention value and 61% as 'low' retention value. Of the 'high' and 'moderate' retention value trees, approximately 58% are mature and only 11% young.

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The peak tree planting period for the site has long past and many mature trees are beginning to enter their over-mature/senescent phase. Trees at this stage of their lives are much more costly to maintain. For the site to have a reasonable future mature tree cover, significant numbers of suitable young trees will need to be planted in appropriate areas. Good landscape management will plan for a 'healthy' tree population mix of young to over-mature trees.

Many of the trees located to the north of the site, and certain trees located in the southern half of the site, which are identified as 'high' value by Stephen Fitzgerald Arboriculture (2007) are proposed to be retained, and their retention and survival enhanced by the application of tree protection zone guidelines during and post construction (refer to Section 4.9 of this document for further details).

It should be noted however that the retention of trees with a high to medium retention value must be balanced with the nature of medium density development anticipated on the site under the PDP, and having regard to the appropriateness of the tree species, existing site levels, and the location of trees in relation to services and infrastructure.

Please refer to the Lancaster Precinct Arboricultural Report, prepared by



Figure 18 View of existing vegetation on site (Source: HASSELL, 2007)



Figure 19 View of sloping topography of site, built form and building heights of existing

Stephen Fitzgerald Arboriculture (June 2007).

2.2.7 Topography

The site falls from a high point at the north, gently towards the south and west. The site has a 19 metre fall along the 500 metre long Plenty Road frontage. The south-west corner of the site at the intersection of Plenty Road and Main Drive is the lowest point of the site. This corner is also the main visual gateway to the site when approaching on Plenty Road from the south.

Due to the topography of the site, the heritage buildings have high finished floor levels from natural ground level and the visual height of these buildings increases with the topography (refer to Figure 19).

Please refer to the Feature Survey Plan prepared by Bosco Jonson (8 December 2006) (refer Appendix B) for further topographical details.

2.2.8 Drainage and Infrastructure

A three metre wide easement runs along the front boundary to Plenty Road. This easement benefits the Crown Grant. It appears from the Feature Survey Plan (Appendix B) that there are no underground services located within this easement. A number of services exist in the northern section of the site that were associated with the previous use of the site.

2.2.9 Contamination

The site is generally clear of contaminants. An Environmental Audit Overlay previously affected the entire Lancaster Gate Estate site, which includes the portion of Lancaster Gate that is now developed with residential dwellings. The Lancaster Gate Development Plan (2001, amended 2007) recommended that the former Urban and Regional Land Corporation (URLC) (now VicUrban) remove the Environmental Audit Overlay from Lancaster once 'an accredited environmental auditor has confirmed the proper and successful removal of contamination and remediation of each area, thus giving the 'all clear' for 'sensitive' land uses'. This was confirmed by a previous report prepared by Golder & Associates.

The Environmental Audit Overlay has since been removed from the site as contamination matters pertaining to the entire Lancaster Gate site have been satisfactorily addressed.



\*Significant change has occurred since this was written with a number of stages completed generally in accordance with the vision and objectives of the development plan. Current information will be provided at the planning permit application stage and should be read in conjunction with the above.

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**2.2.10 Archaeological and Aboriginal Matters\***

A previous report by Biosis Research Pty Ltd (1999) investigated the archaeological features of the site, and concluded:

- There are no Aboriginal sites or scarred trees
- There are no areas of potential archaeological sensitivity
- Other historical sites which potentially might have existed are likely to have been disturbed by subsequent construction activity on the land and is therefore unlikely that these remain in good condition, at the present

This report found a historical site along the eastern boundary of the original Lancaster Gate Development Plan site. This historical site is of local significance with low scientific value (classified as H7922-20 9 on the Heritage Victoria Inventory) but is not located within the site boundaries of the PDP

**2.2.11 Fauna\***

A Flora and Fauna Assessment was undertaken for the site by Biosis Research (April 2003). An Addendum to this report entitled *Fauna Habitat Assessment – Lancaster, 1056-1140 Plenty Road, Bundoora* was prepared by Biosis Research in 2007, which considers the land use and development of the PDP (2008).

The study area has been highly modified through past development of the site and the introduction and planting of non-indigenous and exotic vegetation. As a result of this disturbance and lack of suitable cover, fauna habitats present are generally of low value to native species. In particular, the exotic grassland areas are mostly depleted of native fauna. The planted native trees present are unlikely to produce hollows and therefore reduce their value as habitat. They do, however, offer some value in the form of foraging resources for a limited number of native birds and mammals (including possums and bats) and provide connectivity throughout the surrounding landscape.

The current Landscape Master Plan (Figure 80) within the PDP (2008), prepared by HASSELL, proposes to include plantings of indigenous, native and exotic flora species throughout the site. The planting of indigenous flora species has the capacity to improve the poor habitat areas of the site and it is recommended that where possible, indigenous species be planted in preference to exotics. The retention of several native trees currently on site, as proposed in the Landscape Master Plan, will further maintain their habitat values and connectivity while the newly planted vegetation establishes over time.

**\*Significant change has occurred since this was written with a number of stages completed generally in accordance with the vision and objectives of the development plan. Current information will be provided at the planning permit application stage and should be read in conjunction with the above.**

# Polaris 3083 Development Plan

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## 2.3 Planning Scheme Policy and Statutory Context

### 2.3.1 Introduction

Polaris is affected by a Mixed Use Zone (MUZ), Development Plan Overlay (DPO), Heritage Overlay (HO), and a Development Contribution Plan Overlay (DCPO) of the Darebin Planning Scheme.

### 2.3.2 Mixed Use Zone

The entire site is affected by a MUZ under Clause 32.04 of the Darebin Planning Scheme as shown in Figure 20.



The Polaris Development Plan is consistent with the objective of the MUZ as it provides for a range of residential, commercial/retail, community and business uses.

A planning permit is required for buildings and works associated with land uses such as accommodation (other than a single dwelling or dependant person's unit), leisure and recreation, office, place of assembly and retail premises (other than trade supplies) and a shop.

Advertising sign requirements are at Clause 52.05. This zone is in Category 3.

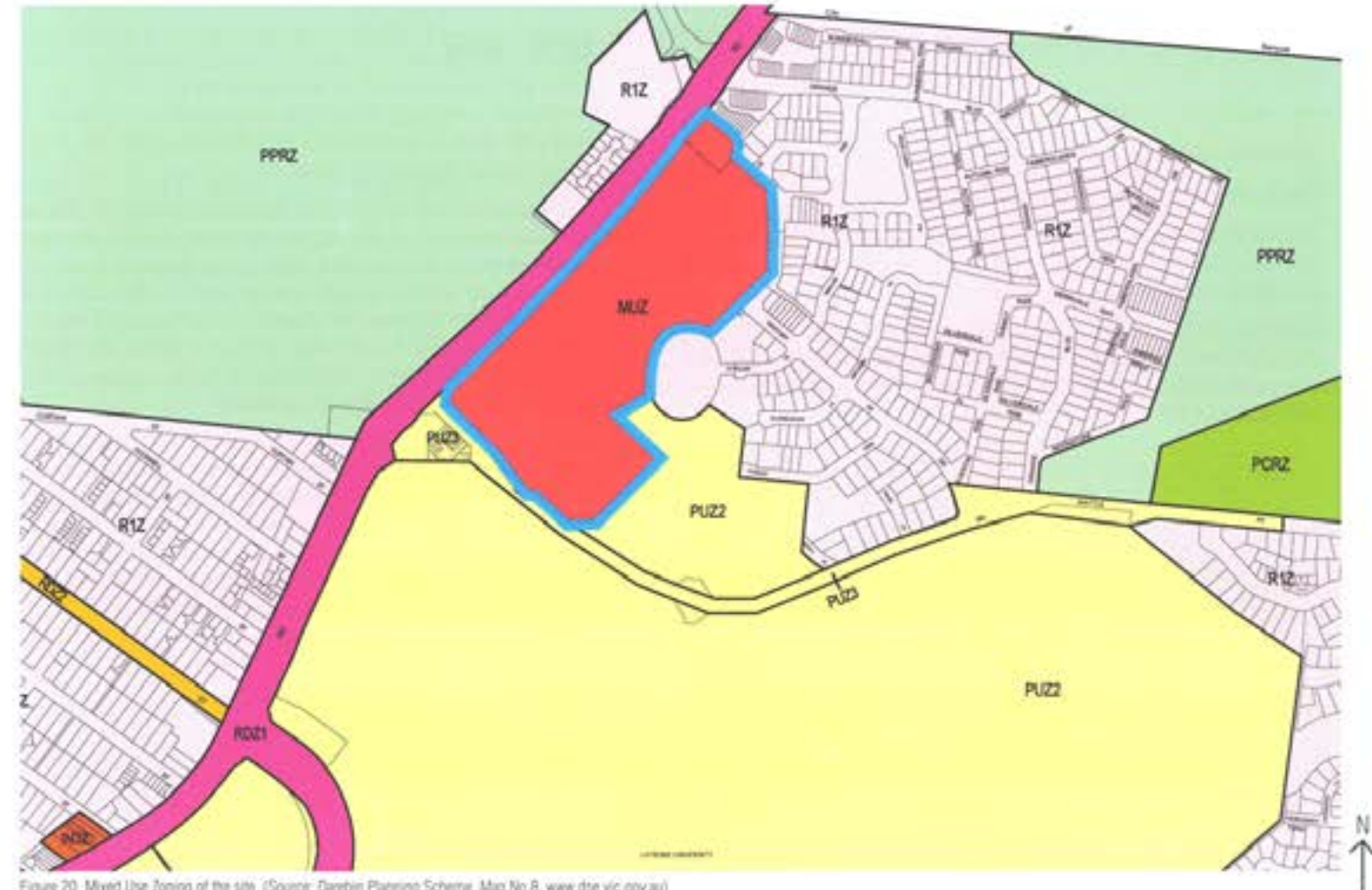


Figure 20 Mixed Use Zoning of the site (Source: Darebin Planning Scheme, Map No 6, www.dse.vic.gov.au)

#### LEGEND

- MUZ Mixed Use Zone
- R1Z Residential Zone
- PPRZ Public Park & Recreation Zone
- PUZ2 Public Use Zone
- IN37 Industrial Zone
- RDZ1 Road Zone
- PCRZ Public Conservation & Resource Zone



Polaris Site Boundary



# Polaris 3083 Development Plan

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## 2.3 Planning Scheme Policy and Statutory Context Continued

### 2.3.3 Development Plan Overlay

The site is affected by Schedule 1 (Former Larundel Psychiatric Hospital) of the Development Plan Overlay under Clause 43.04 of the Darebin Planning Scheme as shown in Figure 21. The purpose of the Development Plan Overlay is:

- To implement the State Planning Policy Framework and the Local Planning Policy Framework, including the Municipal Strategic Statement and local planning policies
- To identify areas which require the form and conditions of future use and development to be shown on a development plan before a permit can be granted to use or develop the land
- To exempt an application from notice and review if it is generally in accordance with a development plan

A permit must not be granted to use or subdivide land, construct a building or construct or carry out works until a development plan has been prepared to the satisfaction of the responsible authority. This does not apply if a schedule to this overlay specifically states that a permit may be granted before a development plan has been prepared to the satisfaction of the responsible authority.

A permit granted must:

- Be generally in accordance with the development plan
- Include any conditions or requirements specified in a schedule to this overlay

A development plan may be amended to the satisfaction of the responsible authority.

#### Compliance with the Requirements of the Development Plan

The PDP complies with the requirements outlined within the Development Plan Overlay, and Schedule 1 to this Overlay as it:

- Comprises both documents and plans
- Describes the land to which it applies
- Outlines the use and development proposed for each part of the land
- Is generally consistent with the underlying urban design principles contained within the *North East Corridor Strategic Plan (1998)*

- Does not propose industrial land uses
- Has incorporated the review of the *Larundel and Linaker's Cottage Plenty Road, Bundoora Conservation Management Plan (2000)*. An additional heritage assessment has also been prepared as part of this submission
- Makes a positive contribution to the character of the area and the Plenty Road streetscape in relation to height, bulk, and location of buildings, removal of vegetation and proposed replanting/landscaping. In accordance with Clause 56 of the Scheme, residential subdivisions with more than 60 lots are encouraged to create a new identity for the future community. The PDP has been thoughtfully designed to result in a high degree of net community benefit for the new community and surrounding local catchments
- Retains identified high retention value trees on the site where practical.
- Identifies the location, layout and appearance of areas set aside for car parking access (ingress and egress), loading and unloading; and
- Respects the character of the area, adjacent buildings and the Plenty Road and Main Drive streetscape

In accordance with the specified requirements of Schedule 1 to the Development Plan Overlay, the PDP includes various plans that shows those items required under Section 1.0 of the Schedule to the Development Plan Overlay, including but not limited to the layout of existing and proposed buildings and works, all heritage buildings, the location of all passive and active open space, details of treatment of all internal roads and road connections to the adjoining road network, and proposed uses on all parts of the land.

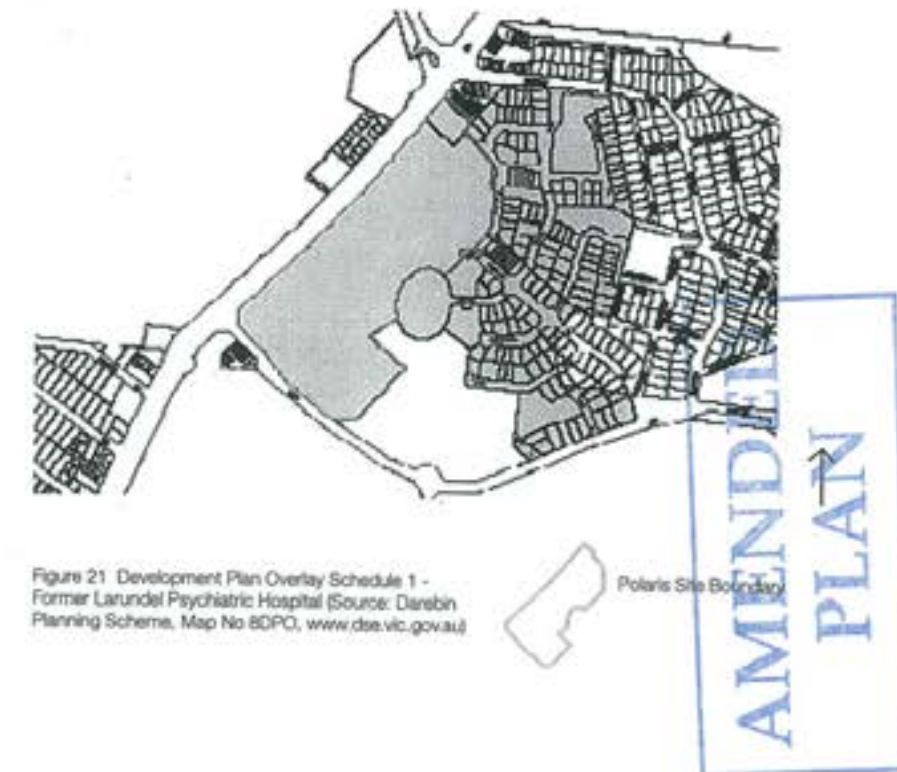


Figure 21 Development Plan Overlay Schedule 1 - Former Larundel Psychiatric Hospital (Source: Darebin Planning Scheme, Map No 8DPO, www.dse.vic.gov.au)

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2.3 Planning Scheme Policy and Statutory Context Continued

2.3.4 Lancaster Gate Development Plan (2001, amended 2007)

The Lancaster Gate Development Plan (2001, amended 2007) as shown in Figure 22, generally identifies the eastern portion of the site (to the east of the oval) to be developed with residential development. The western portion of the site (to the west of the oval) is designated to be developed with a Village Precinct and a Mixed Use Precinct.

The Village Precinct is nominated to be developed with possible uses such as student housing, medium density town housing, apartment housing, educational, community, open space, recreational, offices and ancillary facilities.

Future development and use of the Village Precinct and the Mixed Use Precinct is subject to an amendment to the Lancaster Gate Development Plan (2001, amended 2007) based on the Lancaster Gate Urban Village Design and Development Guidelines (2001).

The two existing heritage buildings on the site located to the west of the oval were not nominated for retention under the Lancaster Gate Development Plan (September 2001, amended 2007).

Conditional secondary approval was given by the responsible authority for the existing Lancaster Gate Development Plan (2001, amended 2007).

The Polaris Development Plan seeks to amend the existing Lancaster Gate Development Plan (2001, amended 2007), and provides an integrated development plan for the Village and Mixed-Use Precincts as required by the Lancaster Gate Development Plan (2007, amended 2007).

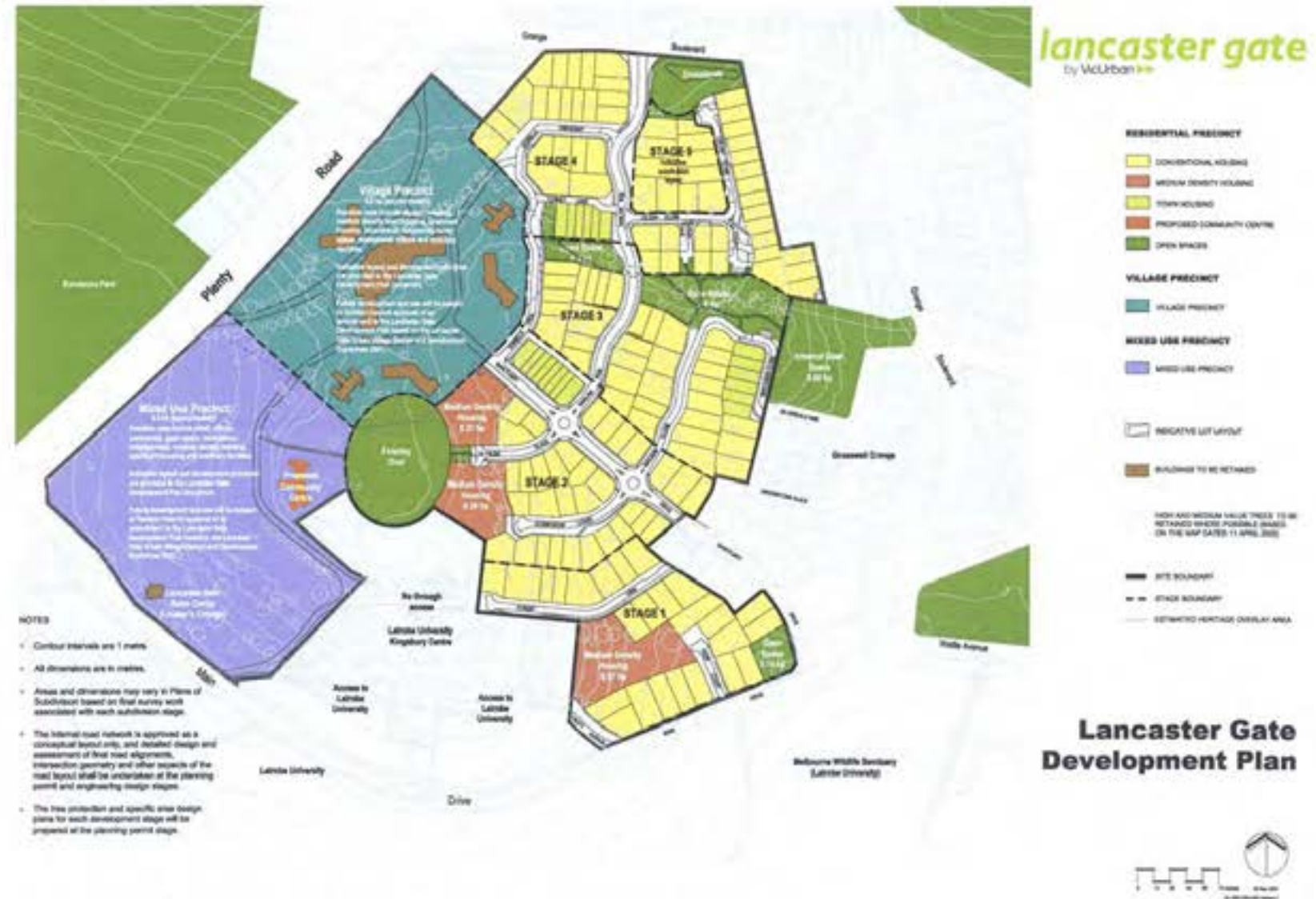


Figure 22. (Existing) Lancaster Gate Development Plan (2001, amended 2007) (Source: Lancaster Gate Development Plan, 2001, amended 2007)

Polaris 3083 Development Plan

2.3 Planning Scheme Policy and Statutory Context Continued\*

2.3.5 Heritage Overlay

The northern half and southern section of the site are affected by a Heritage Overlay, as shown in Figure 23. The PDP meets the objectives of the Heritage Overlay as the characteristics of the site that contribute to its heritage significance will be retained and their contribution to the community will be significantly enhanced. In addition to meeting the minimal requirements of the Heritage Overlay, the PDP goes further and retains two buildings that are not affected by the Heritage Overlay, located to the west of the oval. One of these buildings is designated to be used as a community facility and this portion of the site is likely to be transferred into the ownership of Darebin City Council as a municipal reserve.

2.3.6 Other Overlays

Road Closure Overlay

The northern most allotment which forms part of the site is affected by a Road Closure Overlay (RXO) as shown in Figure 24. The purpose of this overlay is to identify a road that is closed by an amendment to the Darebin Planning Scheme.

A road included in this overlay is closed on the date notice of approval of the amendment is published in the Government Gazette.

It appears that the status of this road has been removed as it is not identified as a road on the current Certificate of Title for this portion of Polaris.

Development Contribution Overlay Plan Schedule 1

The Development Contribution Plan Overlay (DCPO) as shown on Figure 25 applies to all land within the City of Darebin. The purpose of this overlay is to identify areas which require the preparation of a Development Contribution Plan for the purpose of levying contributions for the provision of works, services and facilities before development can commence.

Schedule 1 of the DCPO is the City of Darebin Development Contribution Plan. This Development Contribution Plan relates to all development types, however non-residential developments are exempt from community and parkland infrastructure charges.

The PDP is located within Charge Area 2331009 under Schedule 1, Section 4 of the DCPO. The developer contribution is calculated from the floor areas of non-residential uses and dwelling numbers.

The development contribution required for Polaris will be assessed at the planning permit application stage of future use and development.

Refer to Section 6.3 for more detail.

**SUPERSEDED**

\*Refer to Darebin Planning Scheme for relevant planning policy and controls.

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Figure 23 Development Contributions Overlay  
(Source: Darebin Planning Scheme, Map No 8180, www.dse.vic.gov.au)



Figure 24 Road Closure Overlay  
(Source: Darebin Planning Scheme, Map No 8180, www.dse.vic.gov.au)



Figure 25 Development Contributions Plan Overlay  
(Source: Darebin Planning Scheme, Map No 80CPD, www.dse.vic.gov.au)

2.3.7 Local Planning Policy

The proposal is consistent with the Framework Plan contained within Clause 21.04 Darebin Tomorrow – Our Vision of Darebin City Council’s Municipal Strategic Statement. The Framework Plan identifies a new activity centre to be developed in Polaris.

Abutting land uses to the proposed development site are compatible with the PDP and the heritage significance of the site will be maintained and enhanced. The PDP makes efficient use of existing infrastructure in the area. The economic well being of Polaris and Lancaster Gate and the surrounding area will be improved with the development of a new neighbourhood activity centre which is identified to be created in Darebin City Council’s Municipal Strategic Statement.

A number of local planning policies apply to the site and the PDP as follows:

- Activity Centres Policy (Clause 22.03)
- Urban Character (Clause 22.04)
- Outdoor Advertising Policy (Clause 22.06)

These controls essentially seek to (amongst other things):

- Implement the Retail Activity Centres Strategy (2005) that identifies Polaris as a future neighbourhood activity centre
- Ensure that developments retain and enhance the identified elements that contribute to the urban character of the area
- Seek to revitalise the appearance of retail and business activity centres by promotion of high quality economic development within the City of Darebin

The PDP is consistent with the local policy contained within the Darebin Planning Scheme.





Polaris 3083 Development Plan

**SUPERSEDED**

**\*Refer to Darebin Planning Scheme for relevant planning policy and controls.**

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2.3 Planning Scheme Policy and Statutory Context Continued\*

2.3.8 State Planning Policy – Melbourne 2030

A number of State Planning Policies are applicable to the Polaris Development Plan. The relevant policies pertaining to this development area as follows:

- Metropolitan Development (Clause 12)
- A more compact city (Clause 12.01)
- Better Management of Metropolitan Growth (Clause 12.02)
- Networks within regional cities (Clause 12.03)
- A more prosperous city (Clause 12.04)
- A great place to be (Clause 12.05)
- A fairer city (Clause 12.06)
- A greener city (Clause 12.07)
- Better transport links (Clause 12.08)
- Noise Abatement (Clause 15.05)
- Conservation of native flora and fauna (Clause 15.09)
- Open Space (Clause 15.10)
- Heritage (Clause 15.11)
- Energy Efficiency (Clause 15.12)
- Residential Development for single dwellings (Clause 16.01)
- Medium Density Housing (Clause 16.02)
- Affordable Housing (Clause 16.05)
- Economic Development – Activity Centres (Clause 17.01)
- Economic Development – Business (Clause 17.02)
- Declared highways, railways and tramways (Clause 18.01)
- Car parking and public transport access to development (Clause 18.02)
- Bicycle transport (Clause 18.03)
- Health Facilities (Clause 18.06)
- Water supply, sewerage and drainage (Clause 18.09)
- Developer contributions to infrastructure (Clause 18.12)
- Subdivision (Clause 19.01)
- Design and Built Form (Clause 19.03)

These controls essentially seek to (amongst other things):

- Facilitate orderly development of urban areas
- Assist achievement of a functioning economically viable metropolis with an enhanced environmental quality and livability and best practice management of its infrastructure;
- Assist control of noise effects on sensitive land use
- Provide for a diverse and integrated network of public open space commensurate with the needs of urban communities
- Encourage efficient use of energy
- Encourage the location of new activity centres near current infrastructure and growth areas and in areas best able to cope with change
- Encourage neighbourhood activity centres with a mix of uses that meet local convenience needs and are accessible by local pedestrian and cycle routes and are an important community focal point
- Ensure access is provided to developments by all available modes of transport, including bicycle travel
- Achieve high quality urban design and architecture

At the 'macro' level, the proposed Polaris Development Plan will satisfactorily reflect the objectives and goals of the relevant state planning policy framework.

**Melbourne 2030**

The PDP is consistent with the Melbourne Metropolitan Strategy 'Melbourne 2030' (October 2002) and contained within Clause 12 of the Darebin Planning Scheme.

Melbourne 2030 – Planning for sustainable growth aims for Melbourne to be a compact and sustainable city by encouraging new residential and commercial development in close proximity to existing activity centres, and for new activity centres to be developed on strategic earmarked sites. Polaris is identified as a strategic site which is zoned for a mixed use development, and earmarked for a new neighbourhood activity centre. Redevelopment of selected sites must enable the more effective use of existing physical and community infrastructure, and reduce pressure for inappropriate development on the fringes of Melbourne.

The PDP is consistent with the objectives of Melbourne 2030 in that it facilitates the development of a variety of residential types with a mixed use neighbourhood activity centre, in an identified strategic location accessible by public transport, and provides employment opportunities, recreation and social infrastructure, without imposing detrimental impacts on the locality and surrounding natural environment.

The City of Darebin Retail Activity Centres Strategy (March 2005) nominates the core role/function of Lancaster Gate as the 'Lancaster Gate Neighbourhood Activity Centre'.

Melbourne 2030 states that the key features of a neighbourhood centre is to:

- Contain a limited mix of uses meeting local convenience needs
- Contain generally less than 10,000 square metres of retail floor space
- Be accessible to a viable user population by walking/cycling
- Be accessible by local bus services, and public transport links to one or more principal or major activity centres
- Be an important community focal point, ideally located close to schools, libraries, child care, health services, police stations and other facilities that benefit from good public transport

The PDP contributes to the goal of encouraging walking, cycling and local public transport use. Polaris is well served by a number of public transport routes located on Plenty Road as follows:

- Tram (route 86) providing access south to Melbourne City or north to RMIT Bundoora Campus
- Buses (routes 563 and 568) providing access north and south with numerous opportunities to connect with buses travelling east or west to significant locations such as railway stations, schools and/or commercial shopping centres
- Main Drive provides access to the south and/or connections with buses travelling east, west and north, via bus routes 547 and 548 extensions

The PDP has incorporated higher-density housing (eg townhouses) which is encouraged by Melbourne 2030 within and around neighbourhood activity centres. The townhouses enhance the character of the area while providing a variety of housing options for different types of households. The PDP will improve access to local services and accommodate the changing housing needs of Darebin residents who do not want to sever their links with their local community.

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### 3.0 Site Analysis – Opportunities and Constraints



# Polaris 3083 Development Plan

## 3.1 Circulation and Access

### 3.1.1 Introduction - Heritage Roads

The axial arrangement of the existing road network on the site (as shown in Figure 26) is linked to the spatial arrangement of the existing heritage buildings and the symmetrical master planning of the former Larundel Estate.

The *Larundel and Linaker's Cottage Conservation Management Plan (2000)* states that the principal curved drive, off Plenty Road, provides access to the main group of buildings which are arranged symmetrically around a north-west/south-east oriented axis. The Administration Building is flanked by roundabouts on the north-east and south-west sides of this building. Additional roads branch off from these roundabouts to the north-east, south-east and south-west. The layout of the road network on the site is moderately intact as per Percy Everett's original 1938-42 master plan for the estate. The *Larundel and Linaker's Cottage Conservation Management Plan (2000)* identifies the layout of the former Larundel Hospital as being of primary significance. The formal qualities of the site are expressed by the surviving road layout and axial arrangement of the buildings.

The policies contained within the *Conservation Management Plan (2000)* are to retain the curved entrance drive and to retain the axial arrangement of roads around the principal core of buildings. The site provides an opportunity to enhance the heritage significance of the site, as the original location of the road network is able to be retained and repaired. The axial arrangement can feasibly be retained and has significantly influenced the design response of the proposed PDP.

The existing road network provides opportunities for re-use and the retention of established entry points.



Figure 26 Existing Heritage Roads. (Source: HASSELL, 2008)

### 3.1.2 Vehicular Access

Ratio Consultants have prepared an *Integrated Transport Plan (2008)* for the PDP (refer to Appendix C), and they advised the following regarding the existing road network that services the site:

- Plenty Road is a six lane divided primary arterial road, running essentially north-south between the Whittlesea township and High Street in Preston. Adjacent to the subject site, Plenty Road has an 80 km/h speed limit and a tram reservation within the central median. It currently carries in the order of 45,000 vehicles per day (vpd). The Plenty Road/Main Drive intersection is signalised.
- Main Drive is a local road along the southern boundary of the site. It essentially runs east-west between Plenty Road and Springthorpe Boulevard/Gresswell Road, where it links into the Springthorpe Estate. It has a carriageway width of about 7.5 metres and caters for one traffic lane in each direction and has a 50 km/h speed limit.
- Sanctuary Drive is a local road located within the existing Lancaster Gate residential precinct that provides a collector road function in the precinct and has a carriageway width of about 8.1 metres and caters for one traffic lane in each direction with unrestricted parking on both sides and has a 50 km/h speed limit. It will provide a future direct connection to the Lancaster Gate development.

As part of the PDP, vehicle access and connectivity will be enhanced from both within the site and to/from the adjacent land uses and developments.

Allowance has been made for a number of road connections into the existing residential development located to the east of the site. A seamless integration between the precincts is achievable.

There is the opportunity for the main entrance to the site to be relocated in context to the internal configuration and landmark setting. The location of the main entrance to the site is proposed to be located at the centre of the Plenty Road frontage to enhance the visibility of the existing heritage landmark buildings within the boundaries of the site and also from the surrounding area (refer to Figure 27).

There is the opportunity to organise the on site car parking in a logical manner and, to provide for a permeable, well-connected road system. There is opportunity to re-use and incorporate into the configuration of the development, the existing road network associated with the former Larundel Hospital use. On-street car parking can be provided on all internal streets within Polaris.

\*The Integrated Transport Plan (ITP) prepared by Ratio Consultants in July 2009, is intended to act as 'base' reference document and is to be used as a guide only for the future development within the Polaris.



DEAL CORPORATION

# Polaris 3083 Development Plan

## 3.1 Circulation and Access Continued

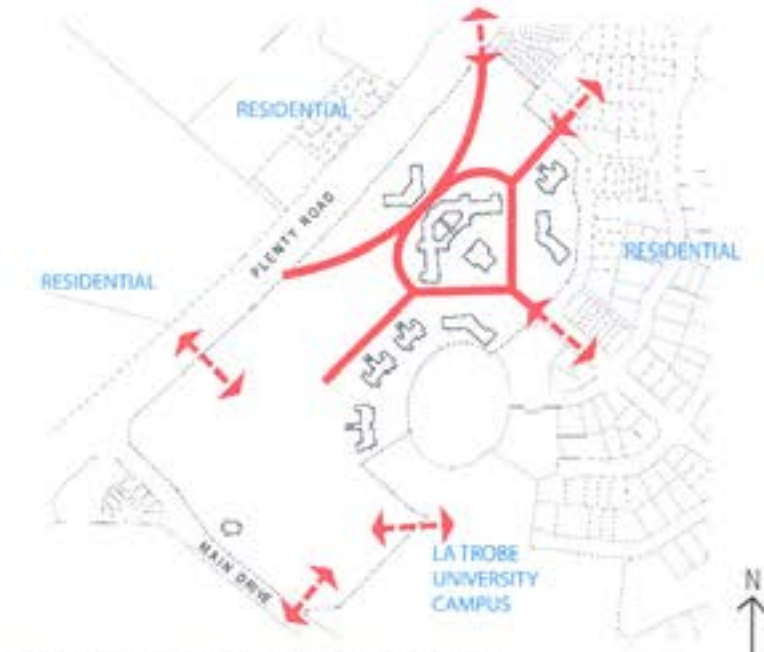


Figure 27 Opportunities for Vehicular Access (Source: HASSELL, 2008)



Figure 28 Opportunities for Pedestrian Access (Source: HASSELL, 2008)

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### 3.1.3 Pedestrian Access

Ratio Consultants (July 2009) advise that there are limited direct shared path connections linking the site to surrounding facilities at present, with opportunities to enhance pedestrian and bicycle access between the site and key land uses such as La Trobe University to the south, Bundoora Park to the west and adjoining nearby residential estates, including Lancaster Gate, Springthorpe, Gresswell Grange, and Mt Cooper.

Pedestrian access, connectivity and permeability are design elements promoted in the PDP. There are opportunities to connect pedestrian paths to the adjoining land uses and development, such as La Trobe University, public open spaces such as the oval, and the adjoining street network to the residential development located to the north and east of the site (refer to Figure 28).

There is the opportunity to provide an additional layer of permeability dedicated for pedestrians in addition to the internal road network. A circuit path is able to be provided around the oval. Pedestrian 'walking' friendly precincts and sub-precincts are promoted and access to focal points has been enhanced.

Pedestrian access to Bundoora Park from the site is currently hindered by the need of pedestrians to traverse six lanes of 80 km/h speed traffic across Plenty Road. Two sets of traffic lights do allow for safer crossing of Plenty Road, however the location of the pedestrian-only signalisation light may need to be reviewed as part of future development on the site, at the planning permit application stage.

The natural topography of the site is a constraint which has been addressed by the sensitive location of roads and pedestrian paths.

### 3.1.4 Bicycle Access

Access to bicycle facilities and an informal bike route is located along Main Drive and an off-road bike path leading to central Melbourne (the Darebin Creek Trail) is approximately 1.5 kilometres to the south-west, off Plenty Road.



Figure 29 Opportunities for Bicycle Access (Source: HASSELL, 2008)

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### 3.1 Circulation and Access Continued

#### 3.1.5 Public Transport

The site has good access to public transport services, with tram route 86 and bus routes 566 and 563 operating on Plenty Road along the frontage of the Polaris 3083 development. Additional bus routes within convenient walking distance of the site have been summarised below.

##### Bus services

- Route 566 (Lalor-Northland via Plenty Road, Childs Road, Grimshaw Road)
- Route 563 (Greensborough – Northland via Mill Park, Plenty Road, Diamond Creek Road)
- Route 548 (Cotham Road, Kew to La Trobe University Bundoora)
- Route 246 (Elsternwick- La Trobe University via Clifton Hill, St Kilda Junction)
- Route 250 (Garden City – La Trobe University via City)
- Route 340 (City-La Trobe University via Freeway (all stops))
- Route 350 (City-La Trobe University via Freeway (limited pickup and set down))
- Route 550 (Northland-La Trobe University via Waterdale Road)
- Route 551 (Heidelberg – La Trobe University Interchange)

##### Tram Services

- Route 86 (Bundoora RMIT-Waterfront City Docklands)

Tram Route 86 and Bus Routes 566 and 563 operate along Plenty Road while Bus Route 548 operates along Main Drive. The remaining bus services operate from the bus terminus near the Plenty Road and Kingsbury Drive intersection, 750 metres from the site.

A privately operated and free "Glödor" bus service also connects the town centre with La Trobe University, stopping (Stop 10) on Copernicus Crescent before Main Drive. It operates Monday to Friday 8.30am to 9.10pm.

Planning Scheme Amendment VC148 was gazetted on the 31 July 2018, which introduced an update to Clause 52.06-Car Parking. In instances where any part of the land is identified as being within the Principle Public Transport Network (PPTN), as is the case with Polaris, Column B of Table 1 is the applied parking rate.

### 3.2 Land Use

Identified as an opportunity in the existing *Lancaster Gate Development Plan (2001, amended 2007)*, the development of the site will allow presently under-utilised land to become more sustainable and thus offer the community, environmental, economic and social benefits through its redevelopment and re-use as a mixed use community.

The existing buildings lend themselves to be adapted and re-used for a variety of uses, primarily residential and community uses. Residential uses could include but not limited to, student accommodation, a range of aged care accommodation (including a retirement village) and prestige apartments. The buildings on the site have previously been used for emergency housing following the Second World War. A report prepared by the Department of Planning and Development (1995) states that after the Second World War, during a period of severe housing shortages in Melbourne, Larundel provided emergency housing for homeless families. This 'emergency housing' use commenced in April 1946 and 109 families (486 people) moved into the buildings. Larundel State School was opened for the children of these families on 4 February 1947 and operated until 15 March 1949. Thus, the existing buildings have previously been used as residential accommodation, and the site has a history of mixed use development.

The retention and re-use of the existing heritage buildings on the site is an opportunity to increase the sustainability of the development. Re-using existing buildings on the land is an environmentally sustainable design initiative which minimises the amount of potential waste created from future development on the land. These buildings contain embodied energy from their original construction. Re-use of existing buildings for new uses will be maximised through the PDP.

The site has a frontage to Plenty Road approximately half a kilometre in length. Plenty Road presents both opportunities and constraints for future development on the site. Plenty Road is a main arterial road that is a Road Zone 1 within the Darebin Planning Scheme.

The site has great opportunities to accommodate a mix of uses as the site is affected by a Mixed Use Zone. In addition to the zoning of the land, the state planning policies, including Melbourne 2030 and the Darebin Planning Scheme earmark this site for development with a new Neighbourhood Activity Centre. Neighbourhood Activity Centres are characterised as containing a variety of uses.

It appears that the oval currently does not cater for much organised sport, however there is an opportunity to redefine its role (in the future) and to also physically integrate the oval with the adjoining Polaris development by sensitively considering the details of paths, tree plantings and seating adjacent to the oval.

### 3.3 Built Form

The *Lancaster Gate Development Plan (2001, amended 2007)* identifies a total of nine buildings to be retained on site due to their heritage value, including seven buildings located in a cluster along Plenty Road, Hugh Linaker's Cottage and a building in the far north-east corner of the original Lancaster Gate site. A total of eight of these buildings are located within the site boundaries for the Polaris Development Plan.

Previous reports undertaken which recommend the retention of these eight buildings, are:

- A study undertaken by Darebin City Council's Heritage Advisor, Willy Keeble, prepared for Darebin City Council (1999)
- *Larundel and Linaker's Cottage Conservation Management Plan (2000)* prepared for the Urban Land Corporation by Allom Lovell & Associates

As part of the proposed amended PDP a heritage assessment has been prepared by Bryce Raworth (2007), which concurs with Allom Lovell's assessment of the existing built form on the site, as follows:

- **Administration Building (Building 1)**  
Of primary significance. Constructed as part of the first phase of Everett's Master Plan for the site, the Administration Building is integral to the former mental hospital complex. It is a good example of inter-war institutional design, distinguished by its unusual modern/Tudor revival style. However the building has been extensively damaged by fire.
- **Hospital Building (Building 2)**  
Of primary significance. Constructed 1940 as part of the first building phase of Everett's Master Plan for the site, the Hospital Building is integral to the former mental hospital complex. Substantially intact externally, it is a good example of inter-war institutional design, distinguished by its unusual modern/Tudor revival style which is characterised by the combination of half-timbered gables, multi-paned double-hung sash windows, glass block work and decorative red and clinker brickwork. The interior is of no significance.
- **Dining and Messroom Building (Building 3)**  
Of primary significance. Constructed in 1940 as part of the first phase of Everett's Master Plan for the site, the dining section of Building 3 is integral to the former mental hospital complex. Substantially intact externally, it is a good example of inter-war institutional design, distinguished by its unusual modern/Tudor revival style which is characterised by the combination of half-timbered gables, multi-paned double-hung sash windows, glass block work and decorative red and clinker brickwork. The

## Polaris 3083 Development Plan



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### 3.3 Built Form Continued

interior is of no significance. The kitchen section, storage area and the clerestory are of utilitarian nature and are not considered to be significant. The interiors are of no significance.

- Male and Female Receiving Wards (Buildings 4 and 5)

Of primary significance. Constructed as part of the first phase of Everett's Master Plan for the site, the male and female receiving wards are integral to the former mental hospital complex. Substantially intact externally, they are good examples of inter-war institutional design, distinguished by the unusual modern/Tudor revival style which is characterised by the combination of half-timbered gables, glass block work and decorative red and clinker brickwork.

- Sub-Receiving Wards (Buildings 6 and 7)

Of primary significance. Constructed as part of the first phase of Everett's Master Plan for the site, the male and female sub-receiving wards are integral to the former mental hospital complex. Substantially intact externally, they are good examples of inter-war institutional design, distinguished by the unusual modern/Tudor revival style which is characterised by the combination of half-timbered gables, glass block work and decorative red and clinker brickwork. The interiors are of no significance.

- Former Linaker's Cottage (Building 14)

Of primary significance. The former residence of Hugh Linaker is among the initial group of buildings constructed for the Mont Park Hospital during the period 1911-1925. Moderately intact externally, the design of the cottage relates to the principal hospital buildings of the complex, having roughcast rendering and arts and crafts style details. Historically, the building is associated with the noted landscape designer, Hugh Linaker, who was responsible for many of Melbourne's significant institutional, civic and municipal gardens and landscapes in the early twentieth century.

- Site Planning

The layout of the former Larundel mental hospital is of primary significance. It is a good and moderately intact example of inter-war institutional landscape design. Designed according to Everett's 1938-42 Master Plan, the formal qualities of the site are expressed by the surviving road layout and axial arrangement of the Tudor style buildings.

The existing buildings on the site present both opportunities and constraints for future use and development. There is opportunity as part of the POP to retain and reuse the existing buildings. All of the buildings on the site affected by the Heritage Overlay of the Darebin Planning Scheme are retained and incorporated into the PDP. In addition, the two buildings associated with the former Larundel Hospital located to the south-west of the oval are proposed to be retained and incorporated into future use and

development of the site. These buildings are not affected by an Heritage Overlay. However, as they were constructed as part of Everett's Master Plan for the Larundel Psychiatric Hospital, their retention will have a positive affect on the character of the area and enhance the heritage significance of the site.

Proposed buildings must be designed in response to the existing buildings on the site to protect the heritage significance of these buildings. Thus, a constraint (particular to the northern half of Polaris) is evident, whereby new development within this area is limited and requires greater development sensitivity, as this half of the site contains the majority of the heritage buildings.

The southern section of Polaris contains greater opportunity for increased density of new buildings as the majority of this half of the site does not contain many existing buildings and/or established built form.

### 3.4 Building Height

Many of the existing heritage buildings, especially those surrounding the oval, have been artificially benched so that they sit higher than the surrounding land and roads. The finished floor level of the existing heritage buildings are also raised above the natural ground level. This gives these buildings a greater presence on the site. During detailed design phases (at the planning permit stage of each precinct), careful thought must be given to DDA requirements providing equal access for all, not only for these heritage buildings, but all proposed new buildings and public realm areas.

Though most of the existing buildings are two storeys, they have high floor to ceiling heights as well as large pitched rooves. The existing buildings provide a benchmark building height for the site as they have a scale, height and symmetry that is comparable, under current construction standards, to the scale, height and symmetry of a contemporary three or four storey building.

Other parts of Polaris could accommodate greater building heights where they are located at more distant or at lower elevations to the existing heritage buildings. The lower topographical locations of the site provide the opportunity for greater building height, for example, at the intersection of Plenty Road and Main Drive. On higher ground, dependent upon construction methods (to be determined at the planning permit stage for each precinct) three storey development will have a comparable height to the building heights of adjoining developments, as well as the existing heritage buildings which have provided a benchmark.

# Polaris 3083 Development Plan

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### 3.5 Vegetation

The schedule to the Heritage Overlay affecting the site does not contain any requirements or controls relating to tree removal. However, the contribution the existing trees make to the amenity of the site and their association with the former Larundel Hospital use is acknowledged. The PDP takes this into account and aims to retain, where practical, trees which have been independently assessed to have high retention value. In some cases where existing tree groupings are not appropriate for retention having regard to the nature of development anticipated on the site, appropriate revegetation will be undertaken to further contribute to the creation of a unique environment for the Polaris development.

There is the opportunity to maintain and enhance the distinctive landscape setting of the site which softens the existing built form and will assist in integrating new built form on the site.

..... of arborist advice (refer to Appendix E), it is apparent that a number of trees are recommended for removal due to poor health, structure, etc.

No Vegetation Protection Overlay or Environmental Significance Overlay covers the site, however the provision of Clause 52.17 Native Vegetation of the Darebin Planning Scheme applies. Clause 52.17-7 contains a Table of Exemptions that may be applicable to the site.

### 3.6 Public Open Space

The new residential community of Polaris will have direct access to public open space areas located within the public realm. To the north of the site, Bundoora Regional Park, Bundoora Park Public Golf Course, Bundoora Children's Park are located across Plenty Road within walking and cycling distance.

A public oval is located adjacent to the centre of the eastern boundary of the site. Further east of the original Lancaster Gate Development Plan (2001, amended 2007) site boundary lies the Strathallan Golf Course, Gresswell Habitat Link, and Gresswell Forest.



Figure 30 Location of Existing Public Open Space (Source: HASSELL, 2008)

### 3.7 Exposure and Interface

Land use and development located adjacent to the boundaries of the site vary in their land use sensitivity.

The northern boundary of the site abuts the main arterial road of Plenty Road. Consideration of noise, visual and access issues are required in the future design and development of the Plenty Road interface. Sensitive uses are not appropriate to locate adjacent to this boundary without proper interface treatment and consideration, such as landscaping. This also presents the opportunity for a mixed use development along Plenty Road to develop a 'more urban' built form with a reduced setback and greater density along the Plenty Road (particularly towards the Plenty Road and Main Road intersection) frontage as opposed to the remainder of the site. This can provide a suitable buffer between Plenty Road and the new residential areas on the site to prevent possible amenity impacts from traffic noise.

Plenty Road presents the opportunity of high exposure for future tenants of any future development along Plenty Road.

The south-west interface abuts Main Drive, which forms part of the local street network. To the south-east of Main Drive is La Trobe University. Careful management of the visual interface with the University is required given it has been targeted for development.

The north-east boundary of the site abuts the residential development approved and developed under the Lancaster Gate Development Plan (2001, amended 2007). Future land use and development along this interface must consider the nature of adjoining residential development in terms of use and scale.

The site abuts public open space (ie the oval) as shown in Figure 30. This interface provides the opportunity for an integrated design response to maximise the potential of an integrated development with the oval.

The south-east interface of the site is La Trobe University. This requires careful treatment in relation to layout, built form and access.

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Figure 31 Exposure and Interface (Source: HASSELL, 2008)

\*Refer to page 13a for south-west interface details at La Trobe University.



3.8 Focal Points/Landmarks

A number of existing landmarks and focal points are present that have the opportunity to be incorporated into the future land use and development of Polaris such as:

- Linaker's Cottage. This building has prominence and visibility along Main Drive, and provides the context of the former Larundel Hospital use of the site
- The Administration Building associated with the former use of the land. This is located in the centre of the symmetrical arrangement of the existing buildings on the site which are affected by a Heritage Overlay

The western corner of the site adjacent to the Plenty Road and Main Drive intersection has high exposure and visibility from Plenty Road. There is opportunity to create a new landmark feature at this location which will provide a sense of arrival to Polaris and this intersection.

There is opportunity to create an additional entrance/focal point to the site from Plenty Road as nominated in Figure 32. This new entrance has the opportunity to enhance the visibility of existing buildings located on the east boundary of the site, by creating a viewing corridor from Plenty Road to the existing buildings located on higher land levels within the site. A new entrance to Polaris at this point will create a sense of arrival and sense of place for the Polaris Neighbourhood Activity Centre.

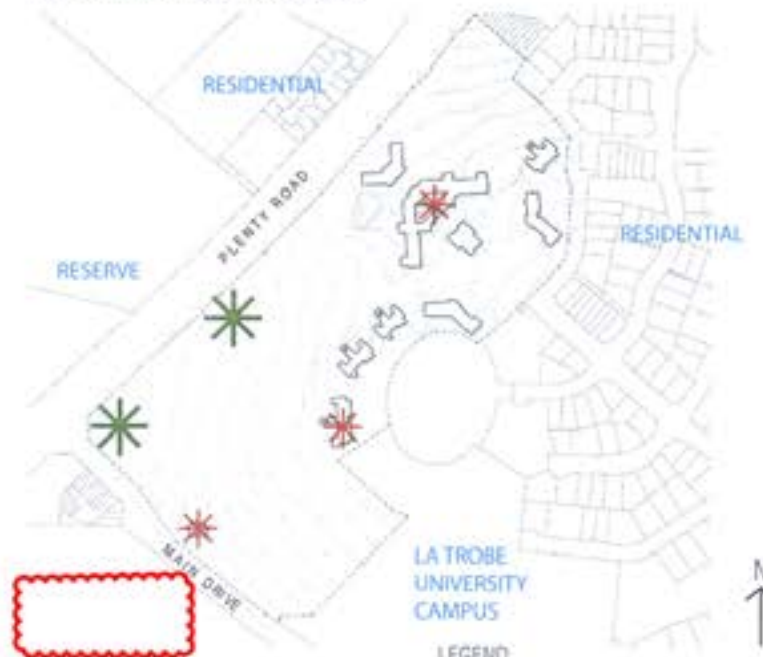


Figure 32 Landmarks/Focal Points (Source: HASSELL, 2008)



3.9 Topography and Orientation

The topography of the site presents both constraints and opportunities for future development on the land (refer to Figure 33). There is the opportunity for greater building height to be located at the corner of Plenty Road and Main Drive. This corner of the site has the potential to be the location for a new landmark and focal point for Polaris.

Careful siting of new buildings must be considered to take advantage of some opportunities that the topography provides. For example, larger building footprints associated with uses such as supermarkets and commercial offices, if sited correctly, provide the possibility for semi-basement car parking that can be naturally ventilated.



Figure 33 Topography and Orientation (Source: HASSELL, 2008)

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4.0 Polaris 3083 Development Plan (PDP)



## Polaris 3083 Development Plan

### 4.1 Overview of Vision/Principles

The principles contained within the *Darebin North East Corridor Strategy Plan (1998, pages 29-33)* acknowledge and build on the *Kingsbury – Larundel Village Master Plan (Urban Land Authority, 1995)* prepared for the Minister of Major Projects in 1995, presents a Framework Plan for the redevelopment of Lancaster Gate.

Refer to Section 5.3 for details of the *Larundel Urban Village Framework Plan (2001)*.

The fundamental principles of the *Darebin North East Corridor Strategy Plan (1998)* are the same principles that underpin the design response of the PDP.

The vision for the PDP is:

- A place that is well located, easily accessed and well served by public transport
- A place that is well integrated with its surroundings in both use and form
- A vibrant and mixed community where the public realm encourages day-to-day interaction, as well as providing opportunities for organised civic events
- A place where high levels of public safety is achieved through good design
- A place where pedestrians, cyclists and car drivers are given equal consideration
- A place that becomes an exemplar for best practice environmentally sustainable design
- A place that values its existing built form heritage – by refurbishing and wholly integrating existing heritage buildings into the Master Plan
- A place where new built form is contemporary in nature
- A place that values the landscape and employs a mix of new exotic and native species to reflect the natural and cultural influences on the site.
- A place where the activities of living, working, playing and shopping happen together in a well designed whole
- A place with a distinct sense of place and a true village nature

The developer will aim to provide a total of 5% affordable housing within the Polaris development to the satisfaction of the responsible authority. The developer will consider designing the affordable housing to achieve a higher level of accessibility and design for persons with limited mobility.

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## Polaris 3083 Development Plan

### 4.2 Design Response

#### 4.2.1 Heritage

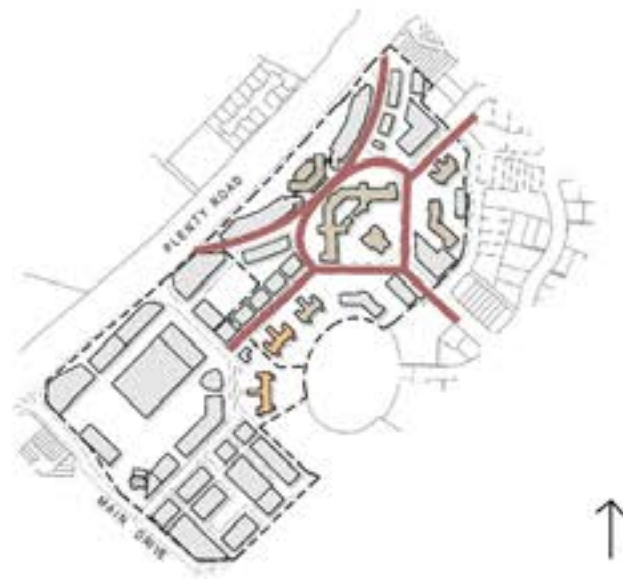


Figure 34 Heritage (Source: HASSELL, 2008)

The PDP proposes the retention and adaptive re-use of all heritage buildings on site, as well as retention of the heritage road alignments. The buildings are to be retained within their open landscape setting with some new buildings sensitively added. Proposed new road alignments extend the existing heritage alignments and place heritage buildings at key landmark locations. It is proposed that the southern most existing two-storey heritage building will be used by Darebin City Council as a community centre. The other remaining buildings will be mostly used for residential purposes, including aged care, student accommodation and apartments.

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#### 4.2.2 Exposure and Traffic Noise



Figure 35 Exposure and Traffic Noise (Source: HASSELL, 2008)

The PDP responds to the high exposure and traffic noise from Plenty Road by locating uses along that edge that are appropriate in this situation. Commercial office and retail showroom uses are proposed to take advantage of the high exposure offered, but not be sensitive to traffic noise and volumes. The location of residential uses and buildings along the Plenty Road frontage will have landscaping to ameliorate traffic noise. The character along the Plenty Road frontage will respond to the difference between Precinct 1 and Precinct 2, with the latter having buildings set back and within a park-like setting, and the former having buildings in a more urban format, set closer to the road reserve. Built form will be contemporary and where appropriate, iconic in appearance, befitting the gateway status of Plenty Road.

Precinct 1B building envelopes updated  
Refer to Figure 49a for current detail.

#### 4.2.3 Landmarks

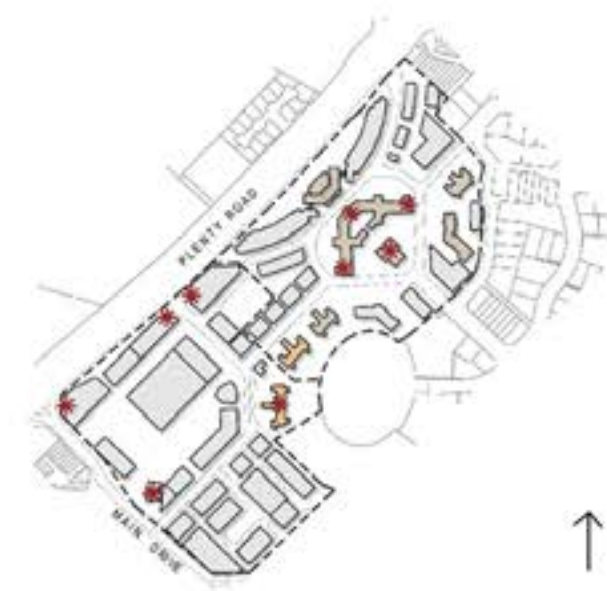


Figure 36 Landmarks (Source: HASSELL, 2008)

Many of the existing heritage buildings already have landmark status. The PDP responds to this by ensuring roads align with as many of these buildings as possible, thereby providing clear views to them as a way finding and orienting technique. The proposed community centre building forms the focal point between two proposed road alignments, enhancing its role as a public building. Linaker's Cottage is placed on a new corner site with Main Drive and a new main street, thereby acting as a landmark at this important corner.

A number of new landmarks will be created within the proposed buildings. The site at the corner of Main Drive and Plenty Road is a significant gateway site. Built form at this location must be iconic to signify this gateway. Similarly the entry to the new main street off Plenty Road will be an important gateway. Buildings flanking this new intersection must also act as landmarks, important especially for retail/commercial uses. New and existing landmarks essentially improve the legibility of a 'place'.

## Polaris 3083 Development Plan

### 4.2 Design Response Continued

#### 4.2.4 Community Focal Point



Figure 37 Community Focal Point (Source: HASSELL, 2008)

The future community centre within the building located to the south of the site, is a main driver in the proposed road pattern for the area outside the Heritage Overlay. The new main street is aligned so that the community centre, the most important civic building on the site, terminates the vista along the new main street from both Plenty Road and Main Drive. The new main street bends past the community centre and creates a new public realm forecourt to this building. The interactive community destination is proposed to be supported within a community garden adjacent the oval.

#### 4.2.5 Entry and Main Street



Figure 38 Entry and Main Street (Source: HASSELL, 2008)

One new entry road off Plenty Road is proposed in the PDP. Flanked by proposed new landmark buildings, this new retail street will form the vibrant heart of the village. Importantly for retail uses, the street will have visual exposure to Plenty Road, a key economic sustainability driver. Dependant on more detailed traffic analysis, access to this street may be limited to a left turn out onto Plenty Road.

Precinct 1B building envelopes updated  
Refer to Figure 49a for current detail.

#### 4.2.6 Access and Connectivity



Figure 39 Access and Connectivity (Source: HASSELL, 2008)

Access to and from the site will be provided from a number of places. Two road connections to Polaris on the northern and eastern boundaries will link these two communities together and provide Polaris and Lancaster Gate residents access to the proposed retail and commercial uses within Polaris. Three access points from Plenty Road are provided, two of which are existent via the heritage road and one new one for the main street. The latter will also provide a connection to a public transport node and Bundoora Park. Connections to Main Drive and La Trobe University complete the connectivity on all sides of the Village Hub Precinct.

## Polaris 3083 Development Plan

### 4.2 Design Response Continued

#### 4.2.7 Vehicular Permeability and Car Parking

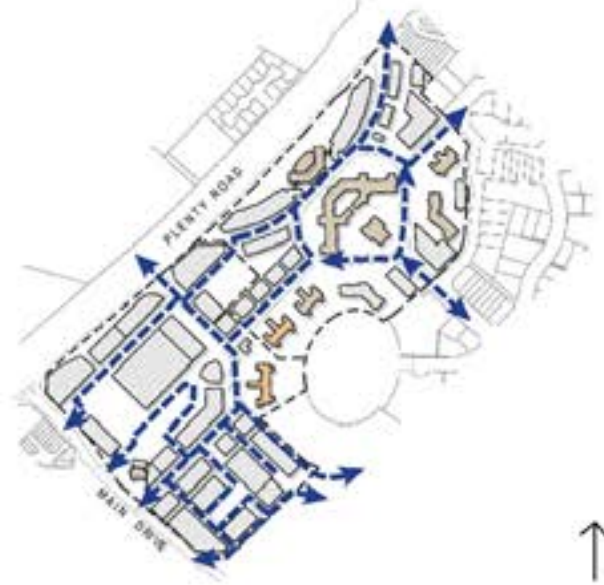


Figure 40 Vehicular Permeability and Car Parking (Source: HASSELL, 2008)

The PDP provides for a permeable, well-connected road system. The heritage roads are all utilised for vehicular traffic and new roads connect into this existing structure and create logical development parcels in between. On-street parking is allowed for on all streets, with additional parking provided behind buildings, where required. These larger on-grade car parks at the rear of buildings have been designed to accommodate future building development over, when it becomes economically feasible to provide basement or decked parking. Any structure associated with a future decked car park will be integrated into the built form of the activity centre.

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#### 4.2.8 Pedestrian Network



Figure 41 Pedestrian Network (Source: HASSELL, 2008)

An additional layer of permeability is provided for pedestrian and cycle transport. As well as being able to use all the same routes as vehicles, there are additional dedicated cycle and pedestrian linkages. A number of connections to the oval are provided in the development plan, linking back to other pedestrian and cycle routes. It is suggested that a circuit path be provided around the oval although this will require liaison with La Trobe University to allow access through their land. The retail area is primarily designed for pedestrian comfort and interest, through lane way connections between shops to the supermarket, wider footpaths, pedestrian crossings to the community centre and pathways through the car parks to make travel safer. The possibility of relocating the tram stop to the entrance to the new Main Street from Plenty Road will be investigated further with the relevant authorities. The detailed design of the Development should provide for safe and convenient access by pedestrians within and through the site.

Precinct 1B building envelopes updated  
Refer to Figure 49a for current detail.

#### 4.2.9 Cycle Routes



Figure 42 Cycle Routes (Source: HASSELL, 2008)

Designated cycle lane along Main Drive to be investigated with the relevant authorities during detailed design. The Principal Bicycle Network along Plenty Road should be considered in the design of pedestrian paths and bicycle paths. The development of the site should maximise opportunities to link with Latrobe University, the signalised crossings along Plenty Road, to bus and tram services and the parklands opposite Plenty Road.

# Polaris 3083 Development Plan

## 4.2 Design Response Continued

### 4.2.10 Aspect and Main Street



Figure 43 Aspect and Main Street (Source: HASSELL, 2008)

The new main Street takes advantage of its alignment and aspect, allowing for wide footpaths alongside the shops, providing opportunities for sunny outdoor dining.

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### 4.2.11 Active Frontages

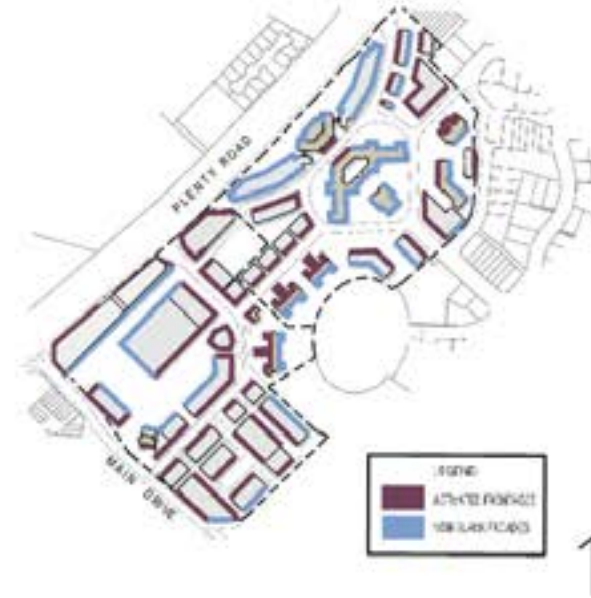


Figure 44 Active Frontages (Source: HASSELL, 2008)

Within the retail/commercial area, the PDP provides for active edges abutting the public realm. This means that shop fronts, office entries etc. are all accessed via the public front with private access for loading, unloading and staff parking primarily available from the rear of those buildings. At-grade car parking areas are located at the rear of shops and offices so that they do not detract from the public street frontage and so that a continuous active edge along this frontage is ensured.

Within the residential areas, a similar technique is employed. Dwellings front onto public streets and where required, private lane ways provide access to garages. Internally, the dwelling design also responds to the active edge concept by placing those rooms at the street front that can contribute life to the public realm.

While recognising that buildings should have a back and front, the backs of buildings shall be designed to avoid blank facades being presented, ie for these elevations to appear as visually active. This can be achieved through the use of glazing to provide passive surveillance, as well as articulated treatments using varying materials, textures and colours, provided this is done with restraint.

### 4.2.12 Public Realm



Figure 45 Public Realm (Source: HASSELL, 2008)

The intention of the PDP is that most public realm areas would be managed by the local Council, including street reserves, parkland areas and the civic forecourt to the community centre. Areas that may be considered for management by private organisations, although remaining accessible to the public, include retail plazas, off-street retail parking areas and land surrounding the student housing.

Future management and ownership of the public realm will be considered and determined in consultation with the City of Darebin during the planning permit process.

## Polaris 3083 Development Plan

### 4.2 Design Response Continued

#### 4.2.13 Integrated Urban Art Proposals

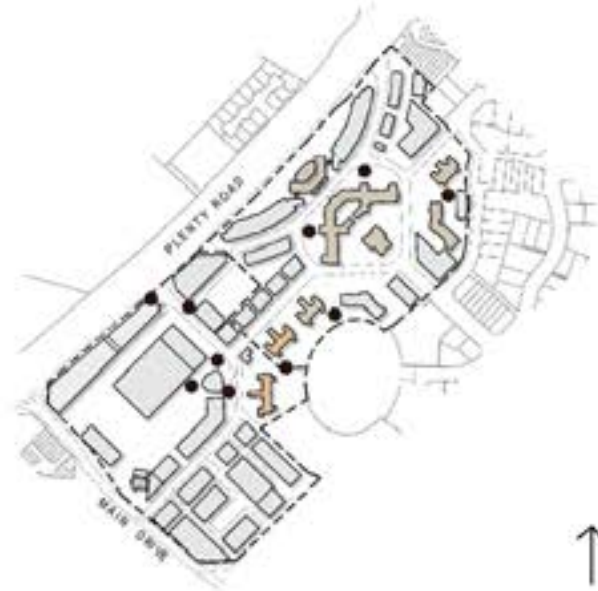


Figure 46 Integrated Urban Art Proposals (Source: HASSELL, 2008)

Provision of public art warrants an integrated process whereby community, Darebin City Council, retailers, artists and other stakeholders are consulted and engaged in the process of selection, siting and determining appropriate artwork. No specific public art components are identified at this development plan stage due to the varied nature of what this may be, other than indicating a commitment to incorporating public art in the detailed design of public realm. The provision of a new main street and community centre forecourt present two particularly likely locations for public art.

#### 4.2.14 Landform and Heights

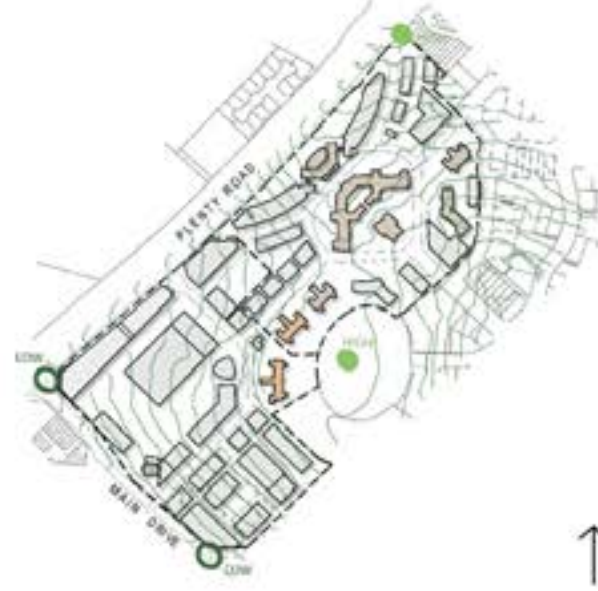


Figure 47 Landform and Heights (Source: HASSELL, 2008)

Buildings have been generally kept to a maximum of three-storeys so as not to exceed the heights of the existing heritage buildings, and to allow for lower cost domestic construction techniques to be used. Most buildings are between two and three storeys in height, creating a comfortable humanscale. The site's landform allows for some buildings to have basement parking or undercroft parking in semi-basement areas that will be naturally ventilated. These include the proposed supermarket and commercial office/residential buildings within Precinct 1.

#### 4.2.15 Developable Parcels

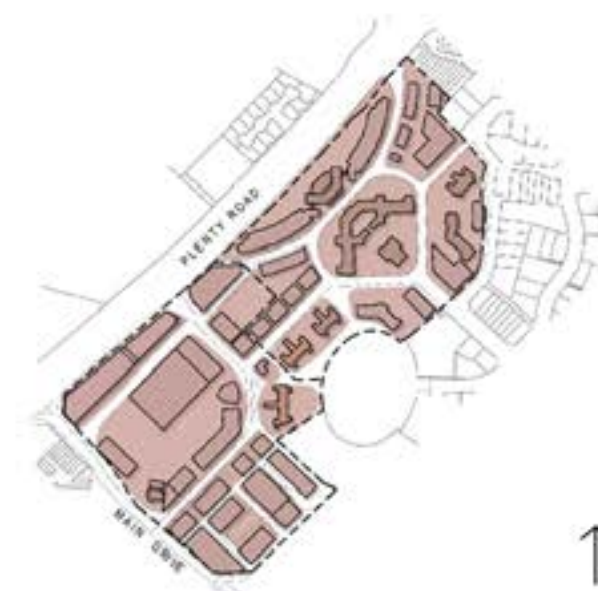


Figure 48 Developable Parcels (Source: HASSELL, 2008)

The configuration of the road alignments (both existing and proposed) results in the creation of parcels of developable land that allow the site to be constructed in a logical and staged manner. Keeping to basically rectilinear forms allows for ease of building and car park siting, in a legible and permeable urban grid.

## Polaris 3083 Development Plan

### 4.3 Overview of Development Plan

The PDP (Figure 49a) is described in terms of two precincts, Precinct 1 and Precinct 2. The original Lancaster Gate Development Plan (2001, amended 2007) envisages Polaris to be an urban village which contains integrated residential and commercial development that aims to meet the local communities retail and service needs, as well as those of a broader community, while creating an innovative mix of residential and development opportunities.

The PDP achieves this vision.

Both of these precincts contain two sub-precincts. Precinct 1 contains sub-precincts A and B, and Precinct 2 contains sub-precincts C and D.

The design response focuses on keeping existing high retention value trees where practical, while creating a layout that encourages permeability for both vehicles and pedestrians, that maximises the views of the existing buildings on the site, and that creates a viable Neighbourhood Activity Centre on the corner of Plenty Road and Main Drive.

The building footprints indicated on the PDP are indicative only and will be refined during detailed design of future development at the planning permit stage.

#### 4.3.1 Polaris Overall Master Plan

The Overall Master Plan for Polaris is located in Figure 49b. This plan provides an illustrative overview of proposed land uses and development opportunities with the various precincts of the Polaris site area.

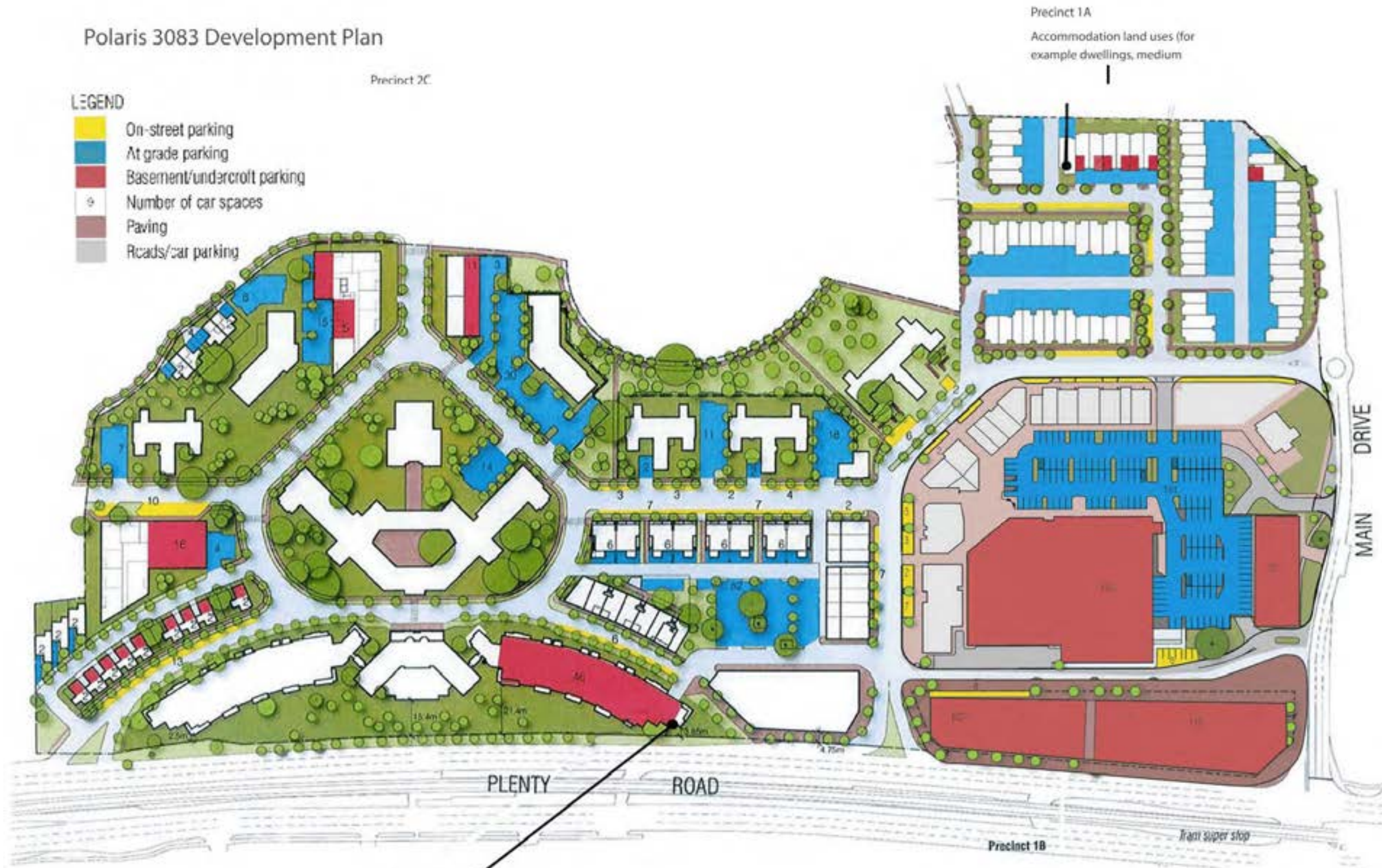
Note: The number and location of car parking spaces as shown on Figure 49b Polaris Overall Master Plan, is indicative only at this stage and is subject to change.



Figure 49a Polaris Development Plan  
Not to scale



# Polaris 3083 Development Plan



- LEGEND**
- On-street parking
  - At grade parking
  - Basement/undercroft parking
  - + Number of car spaces
  - Paving
  - Roads/car parking

Not to scale  
 Figure 49b Polaris Overall Master Plan  
 PMP0719

Precinct 2D  
 Accommodation land use (for example aged care, retirement village or residential)

Precinct 1B  
 Transit super stop  
 Mixed use neighbourhood activity centre providing retail, commercial, office, supermarket, food and drink premises (take away food premises, restaurant, cafes), residential (shop-top housing, student accommodation, **apartments**, residential hotel) and civiccommunity centre, plaza land uses



# Polaris 3083 Development Plan

## 4.4 Precinct 1



Precinct 1B building envelopes updated Refer to Figure 49a for current detail.

Figure 50 Location of Figure 51 and 52 (Source: HASSELL, 2008)

Precinct 1 provides for a mixed use Neighbourhood Activity Centre, a community centre and residential accommodation with a focus on creating a vibrant and mixed use community that is well integrated with the surrounding use and built form.

Precinct 1 provides for various different land uses, including retail, commercial, office, and food and drink premises, residential and civic. A major supermarket will be the main tenant and anchor store of the Lancaster Neighbourhood Activity Centre.

Precinct 1 comprises two sub-precincts, Precinct 1A and Precinct 1B.

### 4.4.1 Precinct 1A

Precinct 1A is a contemporary residential subdivision catering for a wide variety of households including students, couples without children, and families alike.

A continuous terrace house model may be used to create a medium-density precinct; combining single, double and three-storey massing to provide visual variation, to assist in allowing light penetration into private courtyards, and to maximise cross ventilation. The building mass at street corners will be carefully considered to ensure an appropriate urban scale. The building and street layout will work with the site's natural topography to minimise excavation and to ensure at-grade street addresses.

Low fencing (maximum 1.2m high) and minimal to no paving treatments except for pedestrian paths from the footpath directly to the front door in the frontage of all dwellings in Precinct 1A.

The verge abutting the western side the access street that traverses through the precinct is to have a minimum width of 4 metres. The front setbacks to the adjoining dwellings can be reduced by a minimum of 1.5m to accommodate the verge, with no reduction to the width of the access street or on-street parking spaces.

A linear park has been provided to act as a focal point for adjoining residential

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uses within Precinct 1A, and to enable retention and/or relocation of existing high value vegetation located within Precinct 1A, where practical.

The design of the dwellings will be contemporary in nature and utilise a varied palette of materials to maximise visual interest and individual dwelling identity.

The subdivision has been designed to retain high value vegetation where practical. Revegetation along internal streets and the surrounding road network is encouraged in order to create a leafy green, treed environment.

Small deciduous trees may be used as a street tree in this precinct. These trees will be appropriate for the scale of the precinct and will provide some shade in summer and allow solar penetration in winter and will also act as an interesting contrast to the established trees in this precinct.

The adjacent cross section, plan and images provide a conceptual indication of the form, character, mass and materials of future residential development within this precinct.

Detailed design and the assessment of the subdivision, road layouts and intersection geometry shall be undertaken at the planning permit and engineering design stage.

Schedule 1 of the Development Plan Overlay does not require the PDP to specify the number of housing lots to be created. This will be resolved in the design response at planning permit stage for Precinct 1A.

The proposed residential use at this location is compatible with the adjacent education use of La Trobe University, located to the east of Precinct 1A. Access connections for vehicles, pedestrians and bicycles will be provided which will integrate Precinct 1A with the surrounding uses.

The interface with the agricultural reserve of La Trobe University will be sensitively treated in respect of siting, scale and form and appearance of development and appropriate landscaping. Provision of dwelling fencing is to be a minimum 1.8m high on the South East boundary abutting La Trobe University.

Compliance with ResCode (Clause 55 of the Darebin Planning Scheme) will be required for the future medium density housing development for Precinct 1A. Compliance with ResCode will ensure that future development will be site responsive and have an appropriate design response. Refer to benchmark images A, B and C for indication regarding the future form, character, mass and materials of development within Precinct 1A.

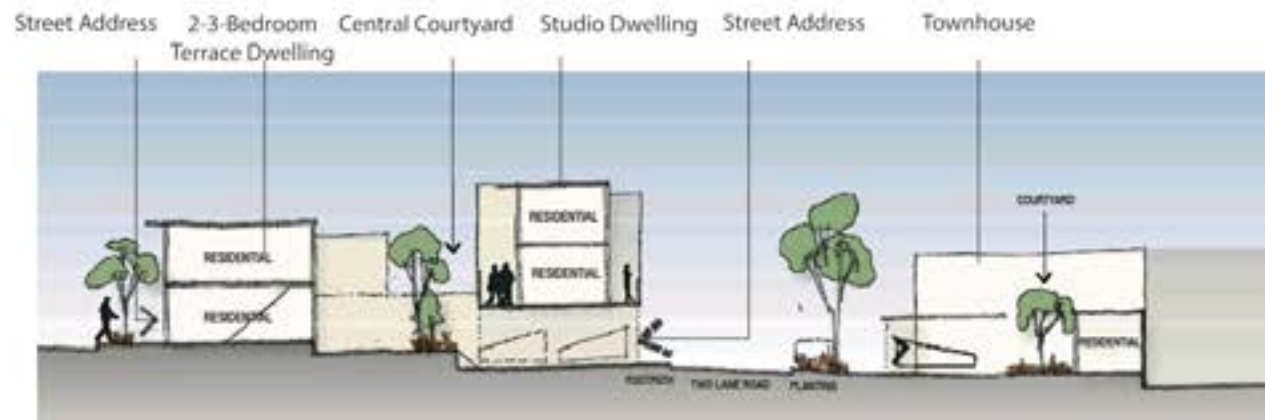


Figure 51 Indicative Townhouse Section (Source: HASSELL, 2008)

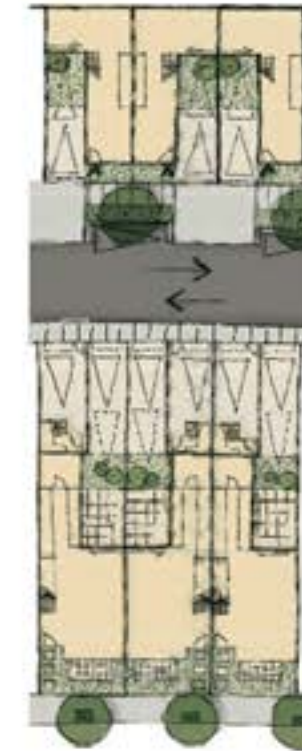


Figure 52 Indicative Townhouse Plan (Source: HASSELL, 2008)



Benchmark Image A Contemporary design with clearly expressed street addresses



Benchmark Image B Adaptable, year round courtyards



Benchmark Image C Variety of materials to create visual richness

HASSELL



## Polaris 3083 Development Plan

### 4.4 Precinct 1 Continued

#### 4.4.2 Precinct 1B

Precinct 1B is a mixed use precinct which encompasses the Polaris Neighbourhood Activity Centre, a new main street, shop-top housing, food and drink premises, retail, commercial, office use and residential hotel use and development of a civic plaza. These uses will be supported by non-retail uses such as the new community centre also to be located within Precinct 1B. The existing heritage buildings within this precinct are to be retained and either repaired or reconfigured to accommodate a suitable use.

#### Neighbourhood Activity Centre

The 'Mixed Use Precinct' identified under the Lancaster Gate Development Plan (2001, amended 2007) is referred to as Precinct 1B under the PDP. Precinct 1B is to be a vibrant mixed use precinct containing the new Polaris Neighbourhood Activity Centre (NAC). The NAC will serve the needs of the existing and future residential community of Polaris and surrounding residential communities.

The estimated Area Schedule for Precinct 1B is indicated in the following Table. These floor areas are estimates only and are provided to indicate the possible intensity of development that may occur within the Polaris NAC.

LAND USE	EST. GROSS FLOOR AREA (GFA)	EST. NET LEASABLE AREA (NLA)
Specialty Shops	4,300	3,700
Supermarket	3,700	3,700
Showroom	3,200	3,200
Large Format Specialty Shop	1,400	1,400
Residential Hotel	12,900	8,000
Tavern	950	950
Office	10,000	6,000
<b>TOTAL</b>	<b>36,450</b>	<b>26,950</b>

Figure 53 Estimated Area Schedule for Precinct 1B of the Polaris Development Plan

A retail economic and commercial assessment has been undertaken by Charter Keck Cramer (July 2008) as part of the investigations undertaken for the preparation of the PDP. This report assesses the proposed NAC against the objectives and policy directions contained within Darebin City Council's Retail Activity 'Centres Strategy' (Essential Economics, March 2005). The 'Retail Economic Assessment' (Charter Keck Cramer, July 2008) for the proposed Lancaster Neighbourhood Activity Centre supports the area of retail and commercial (ie office) floorspace proposed the Polaris Development Plan.

#### Linaker's Cottage

Linaker's Cottage is located adjacent to the southern boundary of Precinct 1B and may possibly be developed with a tavern. Bryce Raworth has prepared a Heritage Report (2007) which assesses the proposed PDP against the heritage significance of the site. Linaker's Cottage is of primary heritage significance and is proposed to be retained as part of the PDP. The significance of this building should be respected by retaining the entry porch and the general bungalow character of the building as viewed from Main Drive. However, it is possible to make substantial alterations and additions to the east and west elevations of the building on the proviso that there is some indent or point of transition indicated between the addition and the extant outline of the cottage as shown in Figure 54 below (Bryce Raworth, Proposed Development Plan for Lancaster – Heritage Issues, 2007).

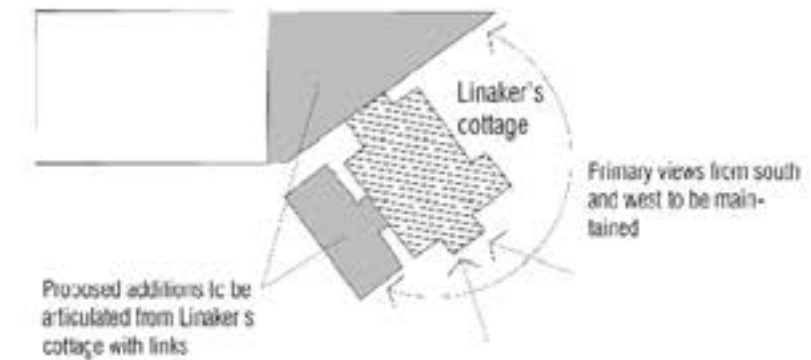


Figure 54 Indicative Future Additions to Linaker's Cottage (Source: HASSELL, 2008)

# Polaris 3083 Development Plan

## 4.4.2 Precinct 1B Continued



Figure S5 Location of Figure S6, S7 and S7a

### Plenty Road Frontage

It is envisaged that the Plenty Road frontage of Precinct 1B may be developed with mixed use commercial/retail use and development. The interface of Precinct 1B with Plenty Road will be appropriately treated to create a distinctive character for the new Polaris NAC. Minimal setbacks of Precinct 1B will be encouraged due to the adjacent main arterial road and to achieve an active frontage, where possible. Where active frontages at ground level cannot be achieved, blank walls/projecting basement are to be articulated. Sensitive uses, such as residential but excluding a residential hotel and student accommodation, are not considered to be appropriate along this frontage and are discouraged.

The Plenty Road frontage has the potential to support a preferred maximum building height of nine (9) storeys, having regard to local context considerations, including the site's relationship to surrounding development.

The functionality of buildings located within Precinct 1B, which forms part of the Polaris NAC, and the layout of the Precinct, have both significantly influenced the front setback of buildings to Plenty Road. A varied ground level setback of between 0 metres and approximately 5.5 metres from the title boundary along Plenty Road is proposed. The varied setback will create a sense of place and a sense of arrival to the Polaris Town Centre, particularly at the corner intersection of Main Street and Main Drive. The title boundary is setback 4 metres from the Plenty Road kerb.

A strong built edge that defines the urban block and that punctuates the corners at landmark opportunities is a proper and valid urban design response, that promotes better activation of the street (where possible), passive surveillance, increased commercial exposure and more efficient use of land (ie avoidance of 'no man's land').

The Plenty Road setback frontage (along Precincts 1B and 2D) has been designed with the view to creating a setback rhythm of buildings coming forward to mark urban corners and buildings setback in between to further accentuate these landmarks. The Plenty Road setback of Precinct 1B varies between 0 metres and approximately 5.5 metres from the title boundary. Refer to Figure 49b for details. This will prevent a hard, continuous edge along Plenty Road. Buildings sit proudly forward (ie in Precinct 1B) and others are recessed (ie in Precinct 2D) with appropriate landscaping treatment to accentuate their attractiveness along this frontage.

Native street trees are proposed for the Plenty Road site frontage wherever practical. This treatment is a continuation of the existing Plenty Road treatment and complements the native planting environment created by Bundoora Park. It is intended that street trees be planted within the footpath on Plenty Road and wherever practical incorporated into rain gardens that harvest surface run-off from the footpath. Trees to the car park on the east side of the commercial buildings will be native species. Refer to the Landscape Plan (Figure 80) for a list of indicative species. Rain gardens are proposed in order to capture street run-off in this area.

The adjacent conceptual plan (Figure S6) and cross section (Figure S7 & S7a) provide a conceptual indication of the form, character, mass and materials of future development as do benchmark images D, E and F.



Benchmark Image D  
Corner buildings should have greater height to define the corner. Buildings should be articulated and provide active edges (where possible).

Benchmark Image E  
Indicative landscaping treatment along the Plenty Road frontage of Precinct 1B (where possible)

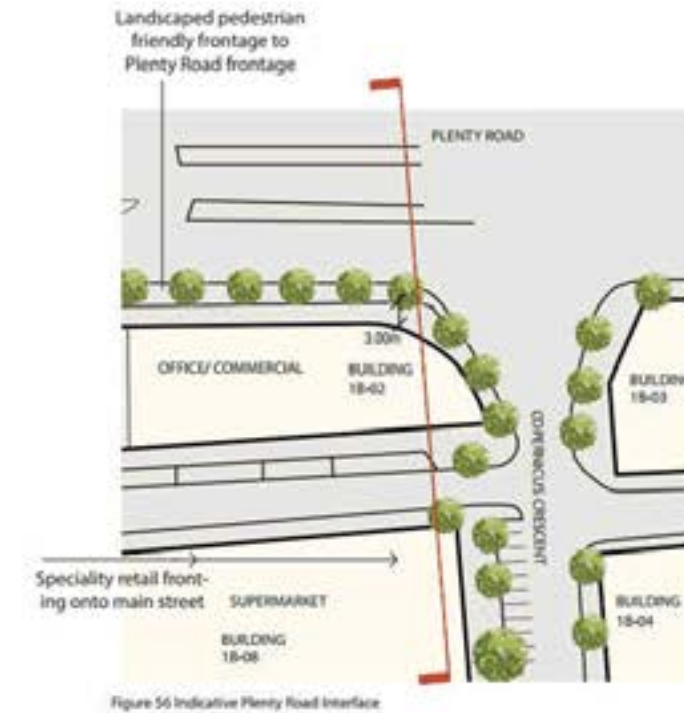


Figure S6 Indicative Plenty Road Interface

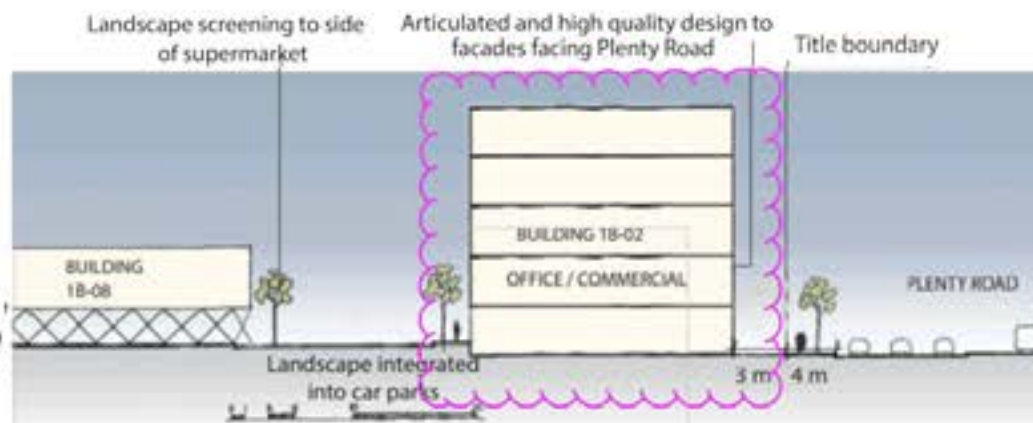


Figure S7 Indicative Section through buildings fronting onto Plenty Road

Appropriate screening to understorey of supermarket to avoid an open sided and open ended sub-basement car park below

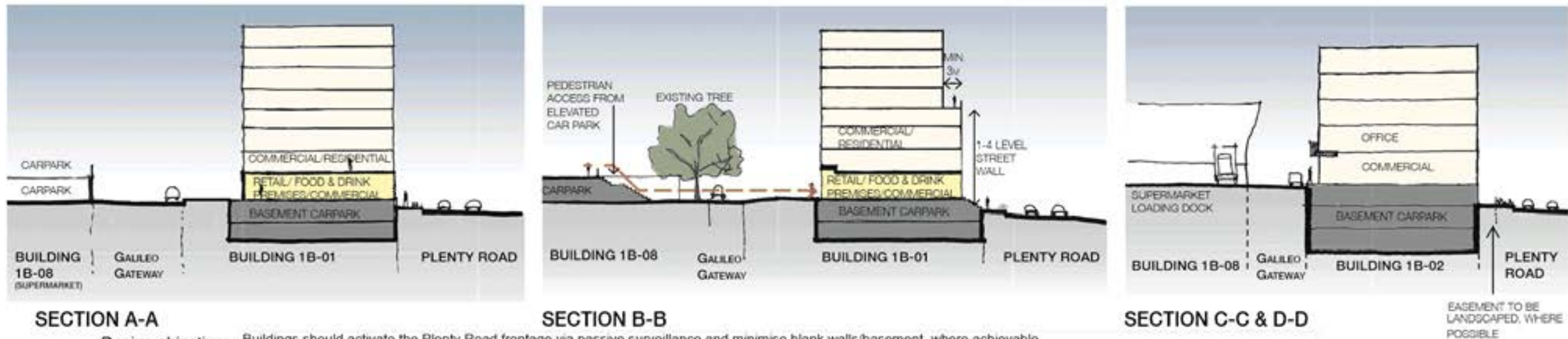


Benchmark Image F  
Level changes are well handled, simple and consistent ground plane treatment and provision of shade with trees and canopies create attraction and comfortable pedestrian amenity



Polaris Precinct 1B - Indicative Section Diagrams

FIGURE 57a

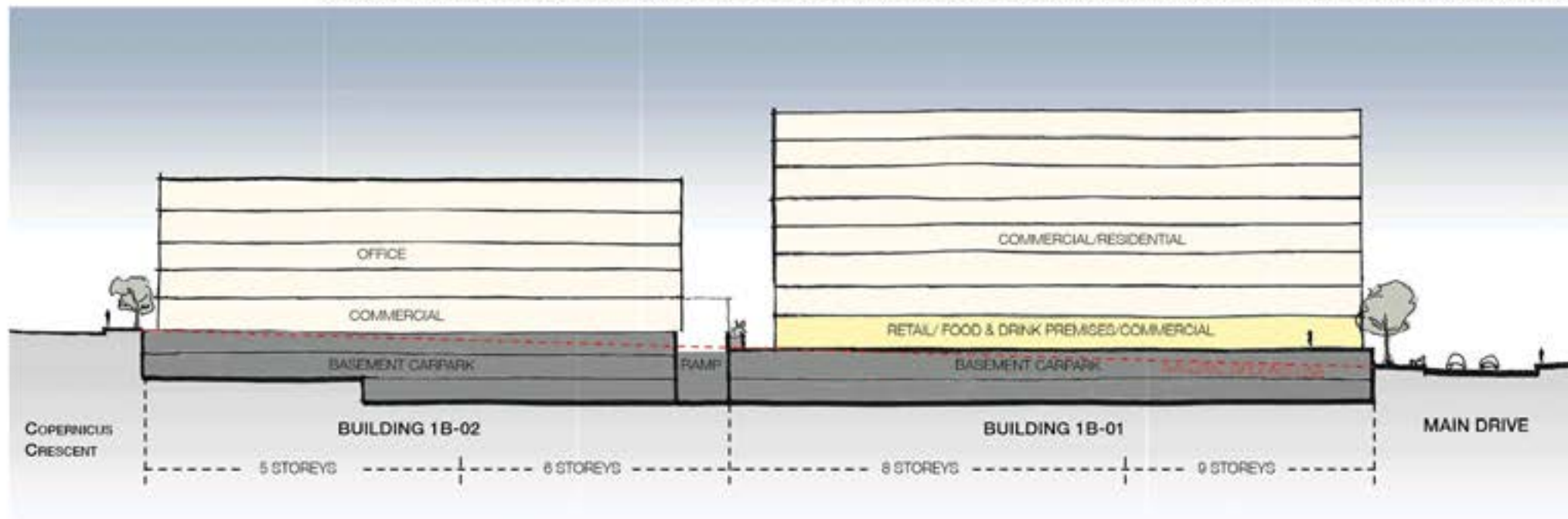


**Design objectives:** Buildings should activate the Plenty Road frontage via passive surveillance and minimise blank walls/basement, where achievable.

Blank walls/basement are to be articulated where activation of the ground level may not be achievable. The colour texture or finish of the wall/basement should be designed to provide visual interest to passing pedestrians.

Where dual frontage are proposed to Plenty Road and Galileo Gateway, buildings should activate both frontage and minimum blank walls, where achievable.

EASEMENT TO BE LANDSCAPED, WHERE POSSIBLE



SECTION E-E



Polaris Precinct 1B

# Polaris 3083 Development Plan

## 4.4.2 Precinct 1B Continued



Figure 58 Location of Figure 59, 60 and 60a

### Main Drive Frontage

The Main Drive frontage of Precinct 1B will have a continuous landscape treatment along the length of the boundary to create a sensitive interface with the adjoining use and development across Main Drive.

A building adjacent to Main Drive (located in the intervening area between Main Drive and the car park) (nominated as Building 1B-16 in Figure 49a, 59, 60 and 60a), may possibly be used for either commercial or residential uses, to be determined at the planning permit stage. This building site presents an opportunity to demarcate the southern boundary of the Polaris Town Centre with a memorable building form and simultaneously provide a seamless transition to the La Trobe University Melbourne Campus. It has the potential to support a building height of **predominantly three (3) storey envelope with opportunity for a recessed fourth or fifth storey**, having regard to local context considerations and responding to the 7-10 storey, 'podium and tower' indicative future built form character anticipated at the northern edge of the university campus. At up to **5 storeys and with a maximum building height of 16 metres**, this building will feature a high quality, contemporary architectural design, appropriate landscaping treatments to Main Drive and a robust palette of materials and finishes. Together these elements will significantly enhance the existing public realm amenity along Main Drive and provide facade activation whilst avoiding blank walls and minimising visual bulk perceptions.

The rear of the supermarket will be treated in a visually interesting manner to positively contribute to the presentation of the Polaris Neighbourhood Activity Centre. Contemporary built form will be encouraged where appropriate within this precinct. The materials and finishes of the rear of the supermarket will be chosen to maximise the visual interest of this elevation.

There is opportunity for at-grade car parking to be supplemented with some deck, basement and/or semi-basement car parking within Precinct 1B, to minimise the expanse and visibility of at grade parking. An appropriate landscape response to the design of car parking areas in Precinct 1B will satisfactorily mitigate any potential adverse amenity impacts from adjoining roads and uses.

Good tree cover will be achieved in the supermarket car park. Kerb and channel around the tree planting areas will be avoided, and car park surface run-off will be captured and directed into rain gardens. In addition to the tree planting, massed shrubs and ground covers will be used to provide another layer of visual interest and pedestrian scale to the space.

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Trees with understorey planting will also be used to delineate the private vehicular entries to shop-top dwellings. Refer to benchmark images H and G for a conceptual indication of the form, character, mass and materials of development within Precinct 1B.



Figure 59 Indicative Part Plan of Precinct 1B

Figure 59 Indicative Part Plan of Precinct 1B



Benchmark Image G  
Front facade to supermarket with external canopies for pedestrian comfort

Benchmark Image H  
Signage to be integrated into the architecture

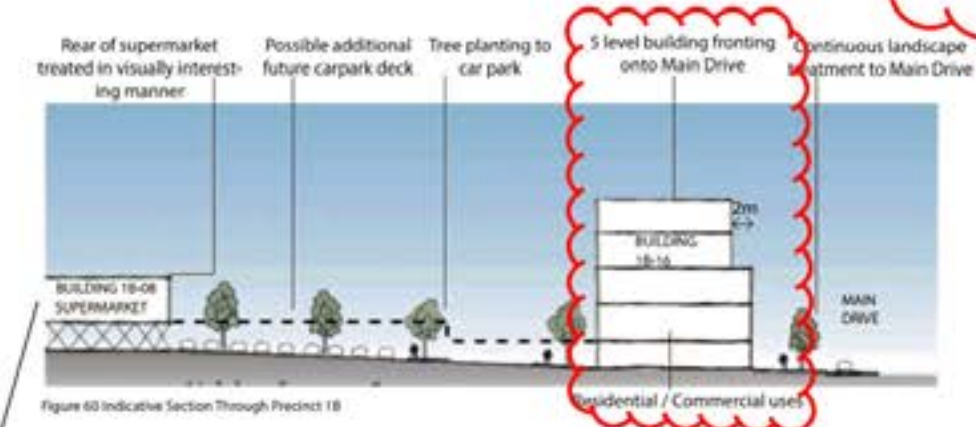


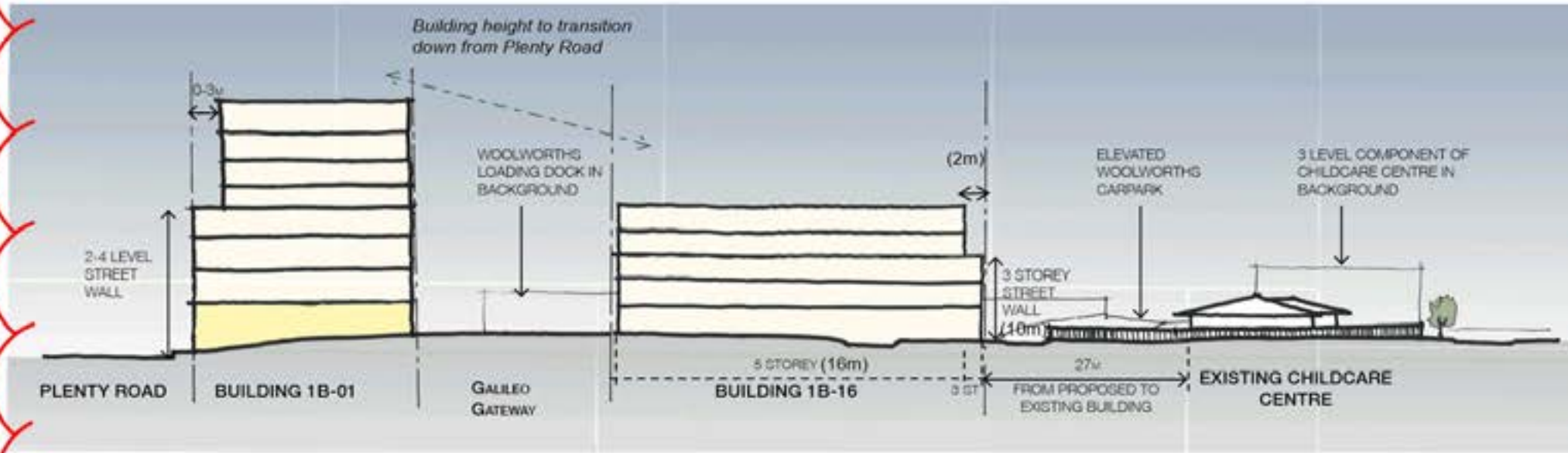
Figure 60 Indicative Section Through Precinct 1B

Appropriate screening to the understorey of the supermarket to avoid an open sided/open ended sub-basement car park below

Figure 60 Indicative Section Through Precinct 1B

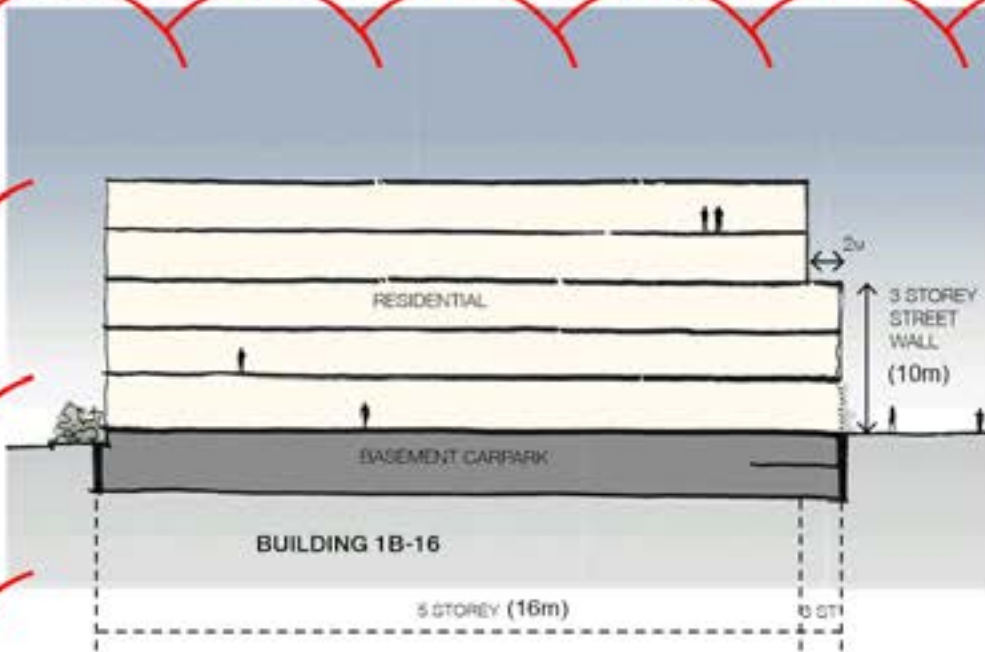
### Polaris Precinct 1B - Indicative Section Diagrams

FIGURE 60a

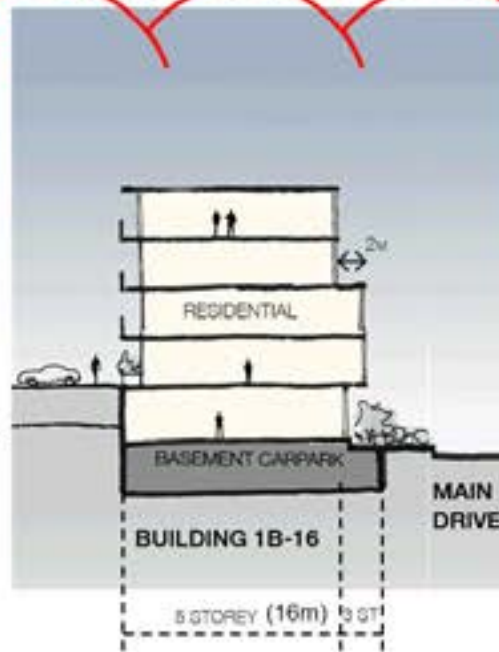


SECTION F-F (MAIN DRIVE ELEVATION)

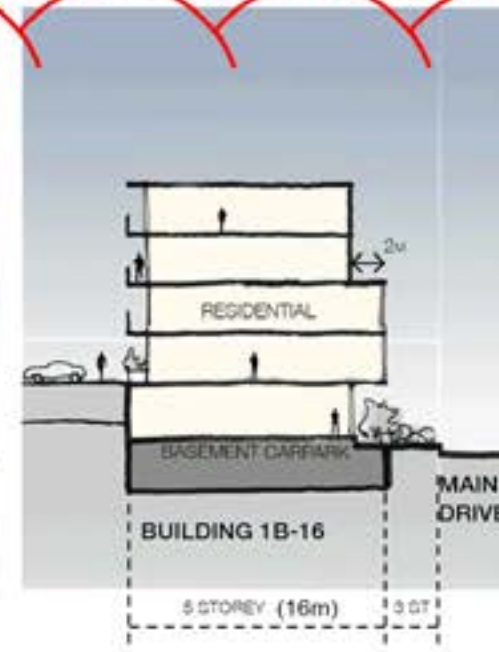
Note: All maximum building height and setback dimensions expressed in metres are not mandatory and exclude roof top plant and equipment.



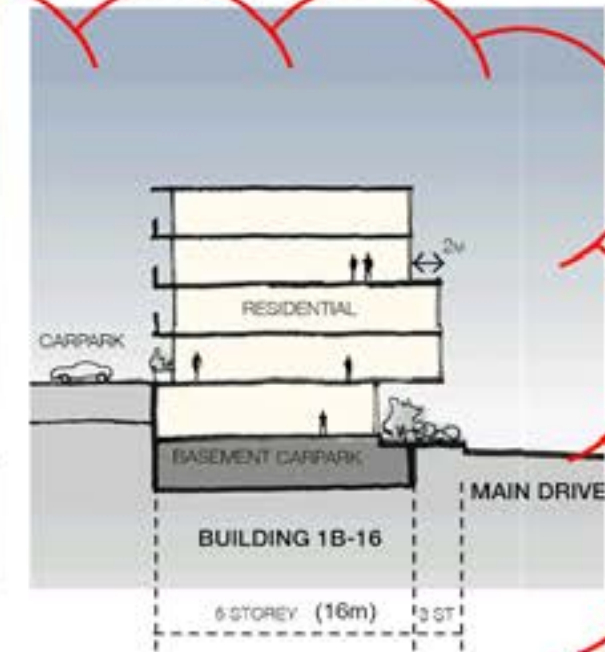
SECTION G-G



SECTION H-H



SECTION I-I



SECTION J-J

### Polaris Precinct 1B

44a

NOVEMBER 2020

# Polaris 3083 Development Plan

## 4.4.2 Precinct 1B Continued



Figure 61 Location of Figure 62 and 63 (Source: HASSELL, 2008)

Precinct 1B building envelopes updated  
Refer to Figure 49a for current detail.

### Main Street South

The southern part of Main Street (between the community centre and Main Drive) will be comprised of home/office units on the east (abutting the medium density townhouses) and shop-top dwellings over ground floor retail on the west. The home/offices incorporate a ground floor office with one or two level dwellings over.

Adaptable floor areas will be encouraged to allow for a variety of uses, including student housing, work studios, family homes with a small business office, or separate offices for professionals. Balconies to the dwellings will provide private open space for residents, articulation of the architecture, and additional interaction with the street. Balconies will be screened to ensure privacy and solar control.

Pedestrian access to ground level retail and offices is via a continuous active frontage to Main Street. Shared pedestrian/vehicle lanes to the rear provide access to secure off-street parking and potential separate street level entries to the dwellings, further animating these frontages. Private 'front doors' are provided at ground level for access to each shop-top dwelling.

The building design is to be contemporary in nature, while also ensuring variety of form, material and detail to provide a rich main street environment. Canopies will be incorporated to ensure pedestrian comfort, contribute to solar control and visual privacy.

Tree planting to Main Street will be medium sized deciduous trees. These will provide autumn colour as a signature element of this streetscape and will also provide some shade in summer and allow solar penetration in winter - supporting the proposed use of Main Street footpaths for outdoor dining/kerb-side cafes. Planting is used to separate residential entries to shop-top apartments from the main body of the car park. Native species will be incorporated into rain gardens that include shrub and ground cover planting.

The adjacent conceptual cross section (Figure 63), plan (Figure 62) and Benchmark Images I, J and K provide an indication of the form, character, mass and materials of future development of Main Street South.

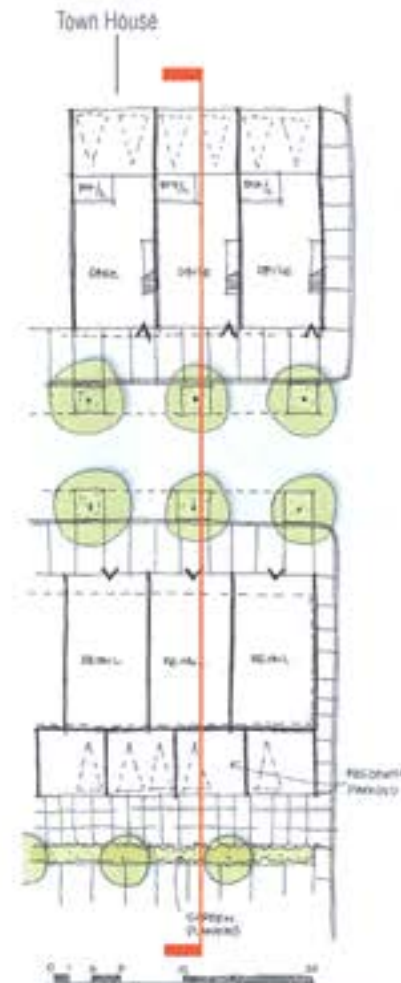


Figure 62 Indicative part-plan, Precinct 1B (Source: HASSELL, 2008)

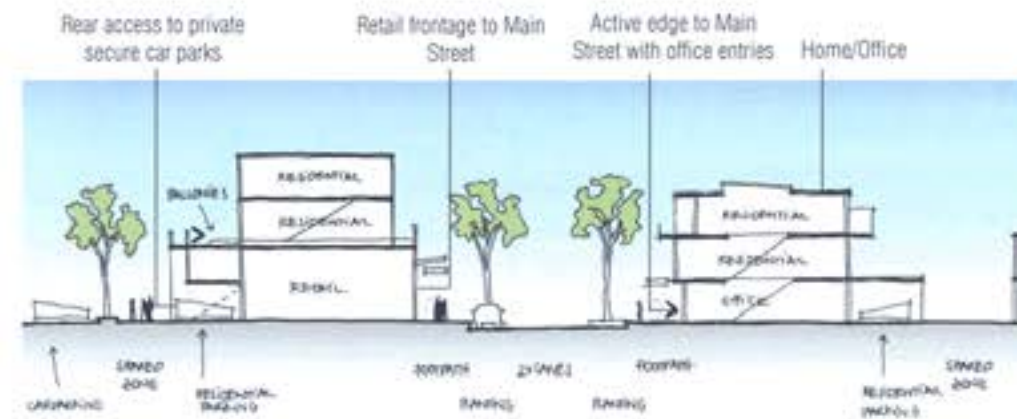


Figure 63 Indicative Section through Main Street, Precinct 1B (Source: HASSELL, 2008)

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Benchmark Image I  
Pergolas and screens to provide sun protection and privacy



Benchmark Image J  
Balconies to animate facade and encourage interaction with the street



Benchmark Image K  
Screening for privacy and solar control





Polaris 3083 Development Plan

4.4.2 Precinct 1B Continued



Precinct 1B building envelopes updated  
Refer to Figure 49a for current detail.

Figure 64 Location of Figure 65 and 66 (Source: HASSELL, 2009)

Main Street North

The development along Main Street between Plenty Road and the proposed community centre is envisaged to consist of retail development at ground level with shop-top dwellings above the retail tenancies located on the north-eastern side of Main Street.

A continuous active frontage to Main Street at ground level provides access to the retail tenancies. Access to the shop-top dwellings will be achieved via a common shared lobby or a number of private 'front-doors' at the rear adjacent to the secure off-street parking.

The building design is to be contemporary in nature, while also ensuring variety of form, material and detail to provide a rich main street environment. Canopies will be incorporated to ensure pedestrian comfort, contribute to solar control and visual privacy.

Tree planting to Main Street will be medium sized deciduous trees. These will provide autumn colour as a signature element of this streetscape and will also provide some shade in summer and allow solar penetration in winter - supporting the proposed use of Main Street footpaths for outdoor dining/kerb-side cafes. Planting is used to separate residential entries to shop top apartments from the main body of the car park. Native species will be incorporated into rain gardens that include shrub and ground cover planting. Refer to Figure 80 for details.

The adjacent conceptual cross section (Figure 66), plan (Figure 65) and Benchmark Images L, M and N provide an indication of the form, character, mass and materials of Main Street North.

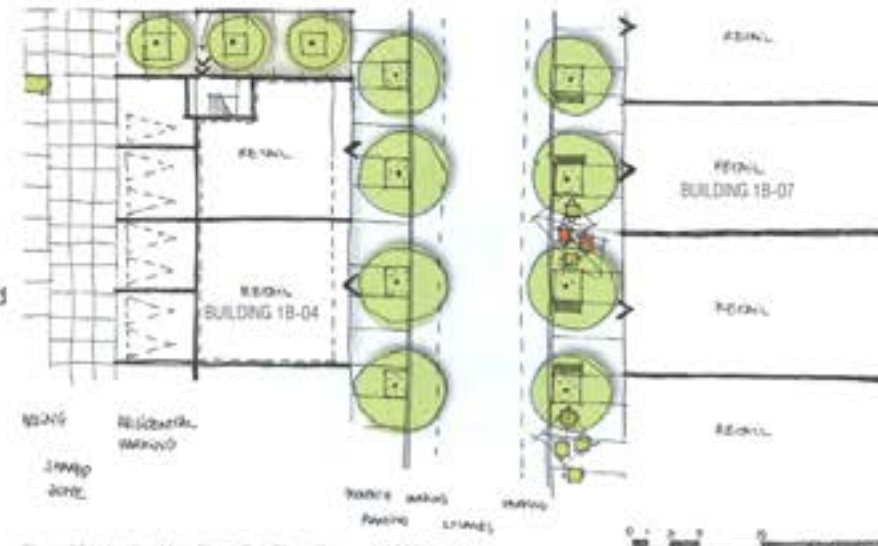


Figure 65 Indicative Main Street Part-Plan (Source: HASSELL, 2008)

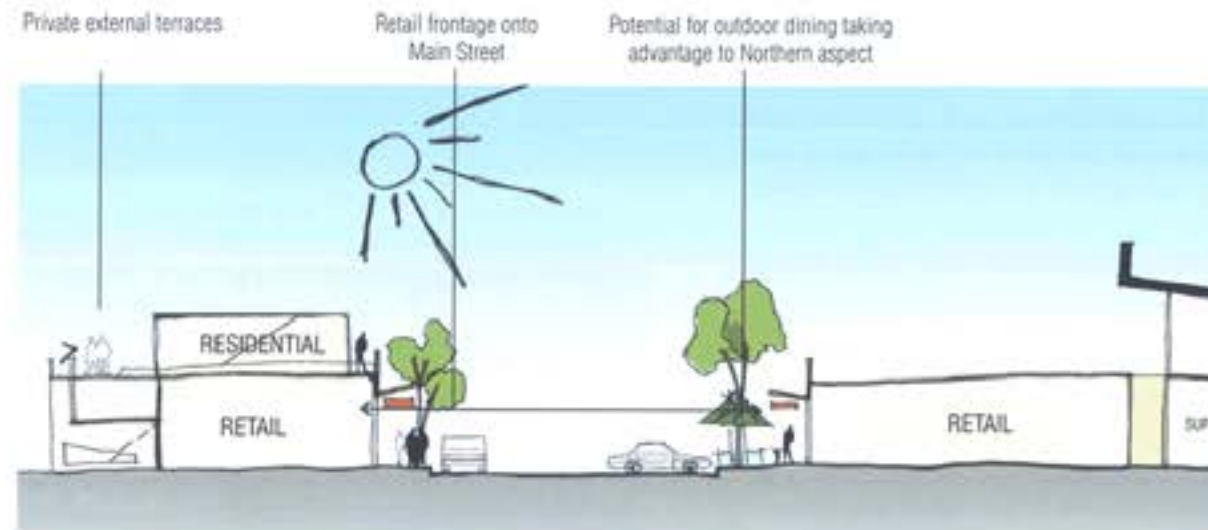


Figure 66 Indicative section through Main Street retail and shop-top dwellings (Source: HASSELL, 2008)



Benchmark Images L  
New main street to emulate the scale and detail of traditional Main Streets



Benchmark Images M  
Contemporary shopfront at a scale appropriate to the main street location



Benchmark Images N  
Canopies and street trees to ensure pedestrian comfort. Generous footpaths to Main Street to allow for landscaping and outdoor dining areas.

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# Polaris 3083 Development Plan

## 4.4.2 Precinct 1B Continued



Precinct 1B building envelopes updated Refer to Figure 49a for current detail.

Figure 67 Location of Figure 68 and 69 (Source: HASSELL, 2009)

### Main Street Civic Plaza/Community Centre

It is proposed that one of the existing buildings on the site be converted into a community centre and transferred into the ownership of Darebin City Council. A new civic plaza will be created at the front of the community centre building to create a public forecourt to this building. The forecourt is to be designed to be an interactive community destination and the centre piece of the Polaris Neighbourhood Activity Centre.

Main Street is purposefully aligned with the view lines of the future community centre building to maximise views of the building from Plenty Road and Main Drive. The future community centre building is located on a higher land level than Plenty Road and Main Drive, and will be a destination and a focal point of the PDP and Precinct 1B.

A 'pavilion' building is proposed to be located within the civic plaza, to encourage the activation of the space and contribute to the importance of this location within the entire site. A conceptual building footprint envelope (Figure 70) and a number of 'pavilion' design guidelines have been developed to direct the future detailed design of the pavilion building.

The proposed community centre and pedestrian plaza are located at an important nodal point on Main Street. Here, Main Street tree planting appears within the central median as well as within the footpaths to create a bold triple row, reinforced by massed understorey planting within the median itself. Main Street trees will be medium sized deciduous species. Refer to Figure 80 for detail.

The adjacent conceptual cross section (Figure 68), plan (Figure 69) and Benchmark Images Q and P provide an indication of the form, character, mass and materials.

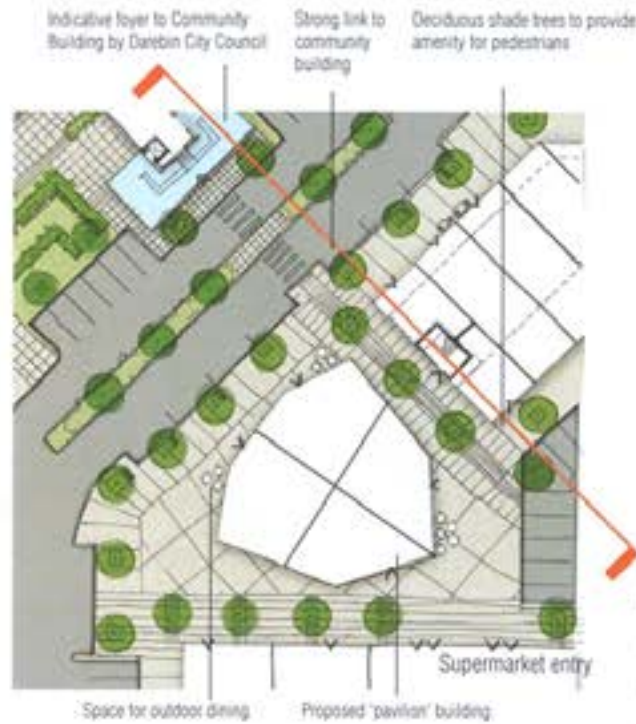


Figure 68 Indicative Plan of Main Street Civic Plaza (Source: HASSELL, 2008)



Figure 70 Indicative 'pavilion' footprint envelope (Source: HASSELL, 2008)

### 'Pavilion' Design Guidelines

- To be taller than immediate neighbours
- To be a positive contribution to the public space
- Ground floor façade to be a minimum 70% glazed
- Design to be expressive and of a high quality commensurate with the importance of this building as the centre piece of the Neighbourhood Activity Centre
- Outdoor dining is encouraged
- Plan footprint to be within the adjacent envelope
- Canopies are encouraged to provide solar and weather protection to pedestrians/diners
- Service areas to be well planned and concealed

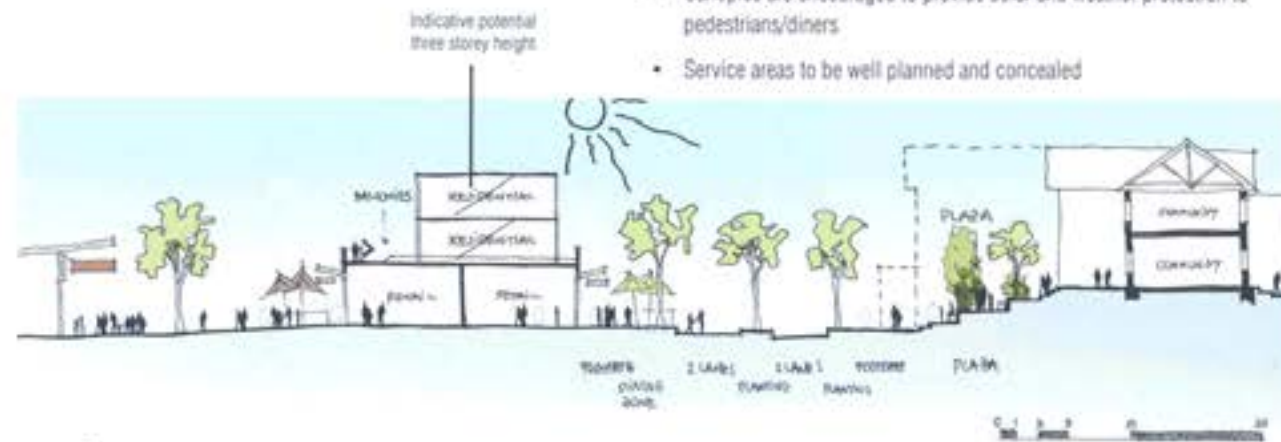
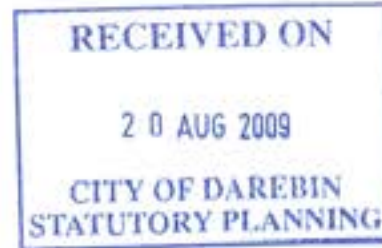


Figure 69 Indicative Section through Community Centre, Main Street and adjacent retail frontage (Source: HASSELL, 2008)



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Benchmark Images Q Outdoor dining activating public space (Note: Image not indicative of architecture or materials)



Benchmark Images P Facade transparency and permeability (Note: Image not indicative of architecture or materials)



Polaris 3083 Development Plan

4.5 Precinct 2



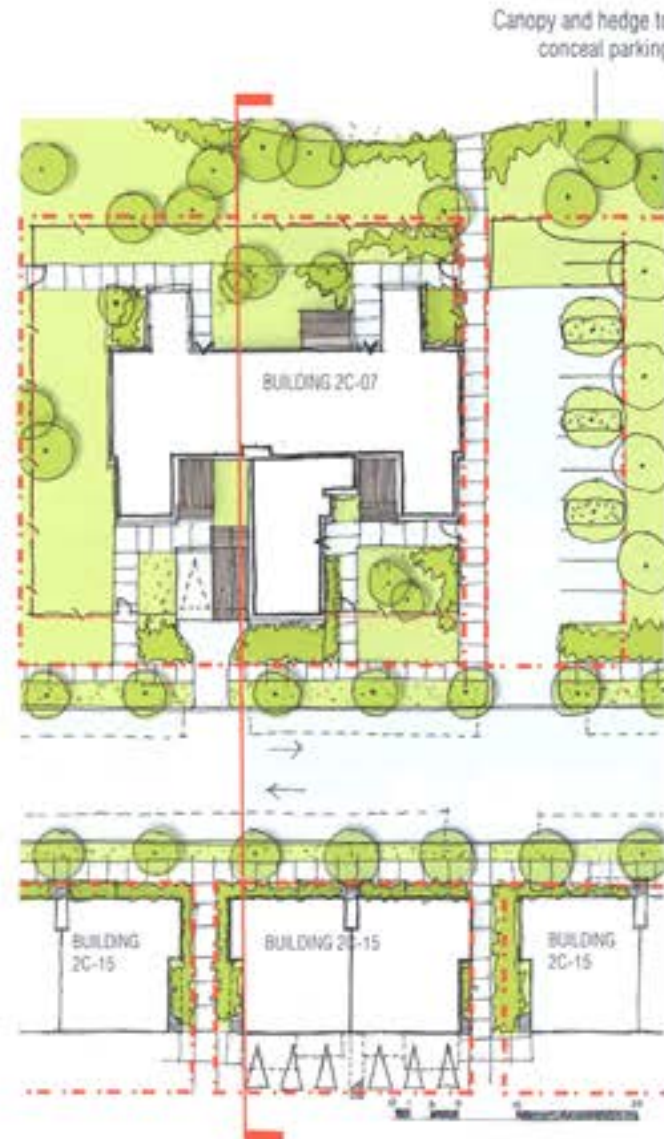
Precinct 1B building envelopes updated Refer to Figure 49a for current detail.

Figure 71 Location of Figure 72 and 73 (Source: HASSELL, 2009)

Precinct 2 predominately provides for accommodation uses. Precinct 2 comprises two sub-precincts, Precinct 2C and Precinct 2D. The future use of Precinct 2D is envisaged to provide for aged care accommodation. The future use of Precinct 2D will be compatible to be located adjacent to the Plenty Road interface. The existing heritage buildings located within Precinct 2 will be retained and incorporated into the future use and development.

The existing tree planting around heritage buildings creates a unique character for this part of the site. These trees are retained wherever possible, and where additional planting is required around buildings these existing trees are augmented with randomly placed native trees. In contrast to this, exotic trees will be used as street trees and as the feature avenue planting for the crescent shaped roadway.

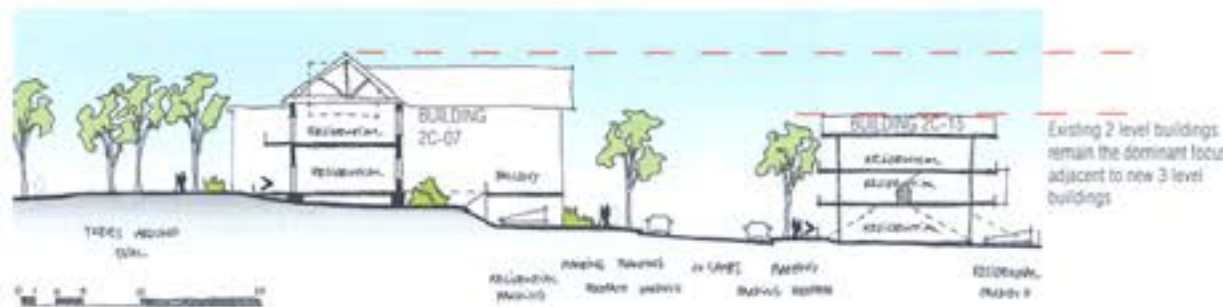
Benchmark Images Q, R and S provide an indication of the form, character, mass and materials within Precinct 2C.



Canopy and hedge to conceal parking

NOTE: The location and type of all fencing will be to the satisfaction of the responsible authority. The location of fencing shown is indicative only.

Figure 72 Indicative Plan of 'Garden Apartments' (Source: HASSELL, 2008)



Existing 2 level buildings remain the dominant focus adjacent to new 3 level buildings

Figure 73 Indicative 'Garden Apartments' Section (Source: HASSELL, 2008)

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Benchmark Image Q  
New buildings sensitively integrated with public space and recreated garden settings



Benchmark Image R  
Indicative scale and materials of new buildings to complement existing heritage buildings



Benchmark Image S  
Balconies and courtyards to provide private open space



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## 4.5.1 Precinct 2C



Precinct 1B building envelopes updated  
Refer to Figure 49a for current detail.

Figure 74 Location of Figure 75 and 76 (Source: HASSELL, 2009)

Precinct 2C provides for a range of accommodation (ie residential) land use and development. A variety of accommodation types will be integrated within Precinct 2C, which could possibly include independent and high care aged care facilities, student housing and garden apartments.

Future development within this precinct will respect the built form and significance of the existing heritage buildings on the site. Views of these buildings will be maximised and the buildings will be repaired and made good. The overall contribution the heritage buildings make to the site will be significantly increased. Due to the large floor to floor levels, high pitched roof, and raised ground floor level of the existing buildings, new buildings can be designed to avoid dominating views of the existing buildings. The configuration and layout of new buildings within Precinct 2C will be respectful of all existing heritage buildings within the precinct.

The garden-setting character of this precinct will be enhanced and recreated. Pathways between buildings will connect to shared parking area, to public footpaths and to the existing oval.

The road layout of Precinct 2C will utilise the existing road networks associated with the former Larundel Hospital use of the site. The former road layout should be extended and the symmetrical layout of development on the site replicated and maintained with future development of each precinct. There is opportunity for at-grade car parking to be supplemented with basement and/or semi-basement car parking within Precinct 2C, to minimise the expanse and visibility of at grade car parking.

The proposed street trees within this precinct are exotic species will provide an interesting visual contrast to the established trees, which are predominantly natives. Randomly placed native tree species are proposed as an interface to the oval. It is proposed that the oval itself be fringed by a double row of native trees with a pathway between. Refer to Figure 80 Landscape Plan for further detail.

The adjacent conceptual cross section (Figure 76), plan (Figure 75) and Benchmark Images T, U and V provide an indication of the form, character, mass and materials.



Figure 75 Indicative part plan of residential units (Source: HASSELL, 2008)



Figure 76 Indicative section through residential units (Source: HASSELL, 2008)



Benchmark Image T  
Some use of autumn plantings to provide warmth and depth of colour



Benchmark Image U  
Hedging and climber plantings assist in privacy and provide a natural aesthetic to combine with the architectural materials



Benchmark Image V  
Enclosed courtyards for a variety of external experiences

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## 4.5 Precinct 2

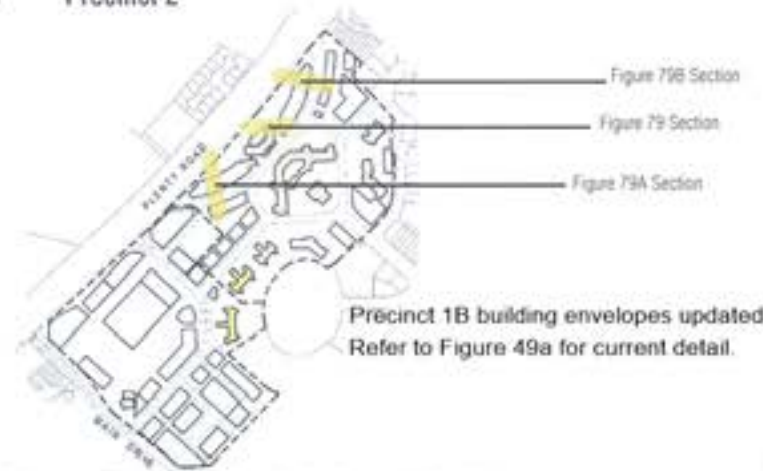


Figure 77 Location of Figures 78, 79, 79A and 79B (Source: HASSELL, 2009)

### 4.5.2 Precinct 2D

Precinct 2D is located along the Plenty Road frontage of Precinct 2 and provides for an accommodation (potentially retirement village or residential building) use.

The existing former Administration Building located on the site is a part of a suite of buildings that form a symmetrical and radiating plan of building and road elements that have a significant heritage value to the site. The building fronts onto the crescent road that runs between the Administration Building and the Hospital Building. The development proposal for this precinct will see the Administration Building re-used as the important central entry block servicing communal facilities for the new accommodation buildings as flanking wings.

The Administration Building has been extensively damaged by fire. The proposal within this precinct is to reconstruct the external shell and roof of the Administration Building as part of the proposed use, with an alternative internal configuration to accommodate the adaptive re-use. The adaptive re-use will also be executed to a greater success with the increase of the buildings size on the western side facing Plenty Road. This increase in size will be constructed in a more modern, interpretive manner to juxtapose between the old and new as identified in the Heritage Report prepared by Bryce Raworth. The new proposed buildings will generally be a maximum of three storeys in height above ground.

It is estimated that Precinct 2D could possibly accommodate approximately 11,000 square metres of gross floor area (GFA) (including the floor area of the existing heritage building which will be reconstructed and re-used for accommodation land use).

The total number of units within Precinct 2D is estimated to be 127, with a breakdown over three levels as shown in Table 77a. Car parking for the facility is anticipated to be self sufficient and accommodated at basement level. This will be reviewed at the planning permit stage of the precinct.

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The external form of new buildings that flank the Administration Building will be articulated. A series of balconies and gables varying in depth to provide breaks in the overall length of the building. Articulation will be further achieved by the separation of the existing and proposed building on either side of the Administration Building. The connection between the heritage building and the new buildings will be provided by a highly glazed, single storey, transparent link. The connection will provide a 6 metre wide view corridor between the buildings from Plenty Road. This will enable views into the site, from the street, and provides for glimpses between new and old buildings to existing heritage buildings within the site. These views have been identified in Bryce Raworth's Heritage Report.

As shown in Figure 78, the location of the building setback from Plenty Road has been considered in context to the heritage values of both the crescent road, and the adjoining Hospital Building located in Precinct 2C, and in context to the form, scale and heritage value of the existing Administration Building. A sense of address is provided by the building from the adjoining crescent road, to provide an appropriate entry feature to the building as part of its re-adaptive use to the building, and to allow safe and easy access to the community bus by residents of the building.

The proposed landscape treatment around the buildings will respond to the existing landscape context and heritage values of the site. The crescent shape of the original geometry of the road will be reinforced with a new avenue of stately street trees, to create a strong and attractive edge to the development and provide appropriate scale to the three storey elements of the buildings. Sweeping lawns and informal plantings of native and exotic trees will create a park like setting for the buildings to complement the existing character of the site. Areas of more intense landscape treatment will occur in high use areas adjacent to the building. An existing planted mound along Plenty Road will be remodelled and planted out with a display of landscaping and trees. The planted mound will provide a very effective buffer at ground level from the noise and sight of Plenty Road from Precinct 2D.

	Studio	1 Bed	2 Bed	Total
GND	9	24	5	38
L1	10	27	6	43
L2	10	28	6	44
Managers	-	1	1	2
<b>TOTAL UNITS</b>	<b>29</b>	<b>80</b>	<b>18</b>	<b>127</b>
<b>TOTAL BEDS</b>	<b>29</b>	<b>80</b>	<b>36</b>	<b>145</b>

Figure 77a Indicative Room Breakdown Table (Source: Gracious Living/Petrac, 2008)



Figure 78 Indicative Plan of Precinct 2D (Source: Deike Richards and HASSELL, 2008)

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## 4.5.2 Precinct 2D Continued

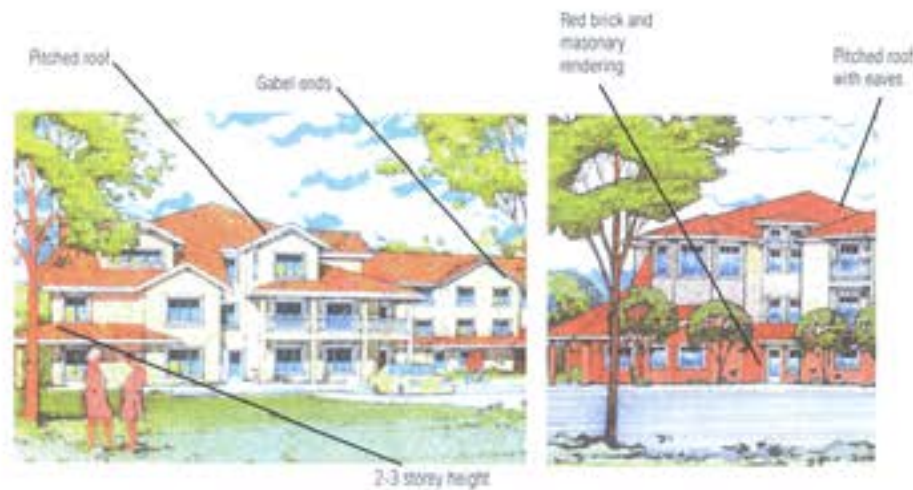
Native species street trees such are proposed for the Plenty Road site frontage. This treatment is a continuation of the existing Plenty Road treatment and complements the native planting environment created by Bundoora Park. It is intended that street trees be planted within the footpath on Plenty Road. Randomly planted native tree species are proposed to maintain the existing nature of the landscaped setback from Plenty Road. A medium sized, exotic canopy tree will be used to provide strong avenue planting on the existing Crescent Road. Refer to Figure 80 Landscape Plan for indicative species details.

Existing views from Plenty Road will be improved by the proposed development by adjustment to the levels and landscape. The current views are limited by the nature of the existing contours and landscaping. The visual impact of parking in the proposed development will be minimised through the use of undercroft parking to the southern end of the building.

The adjacent conceptual cross sections (Figures 79, 79a and 79b), Benchmark Image W and plan (Figure 78) provide an indication of the form, character, mass and materials of Precinct 2D.



Benchmark Image W  
An indicative representation of the transparent connection between the existing heritage building and the new development in Precinct 2D (noting that single storey connection is proposed for the Administration Building)



Figures 76a & 78b Typical architectural features of accommodation development that reinforces the primary architectural elements of existing heritage buildings on site (Source: Dieke Richards/Petrac, 2008)

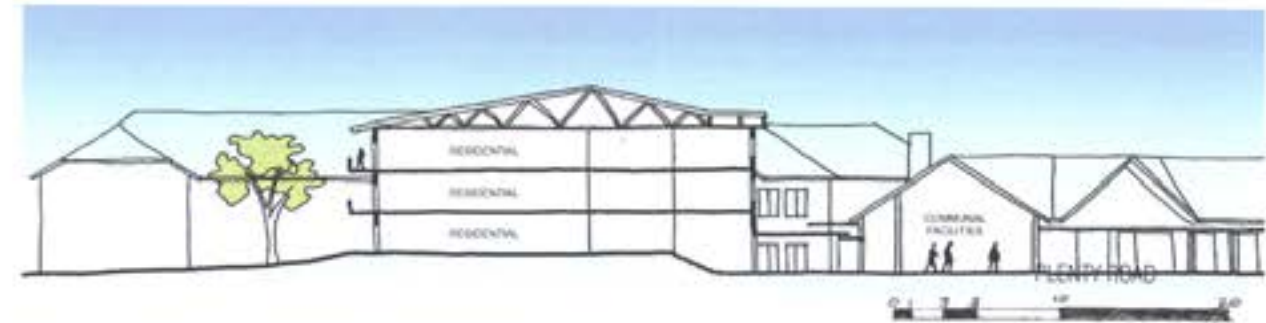


Figure 79 Indicative Section through Precinct 2D (Source: Dieke Richards, 2008)

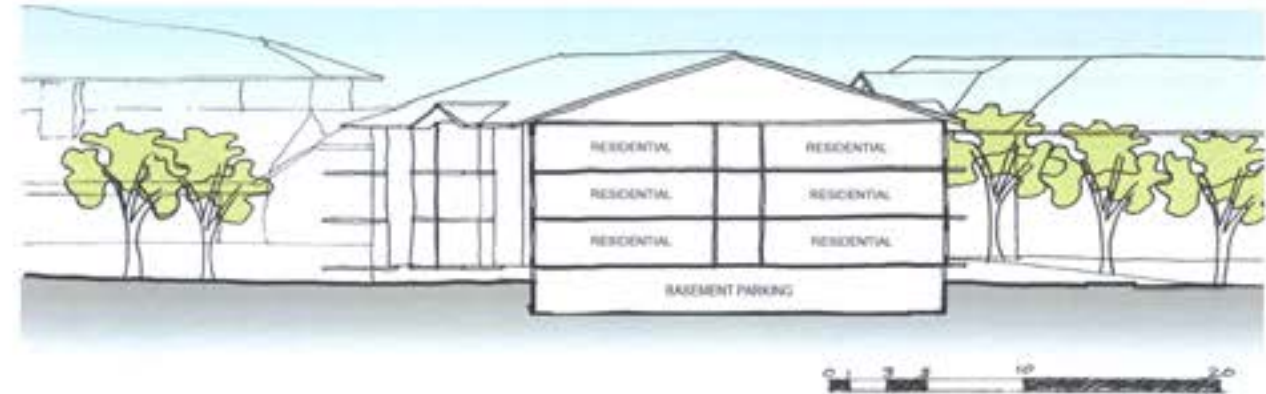


Figure 79a Indicative Section through Precinct 2D (Source: Dieke Richards, 2008)

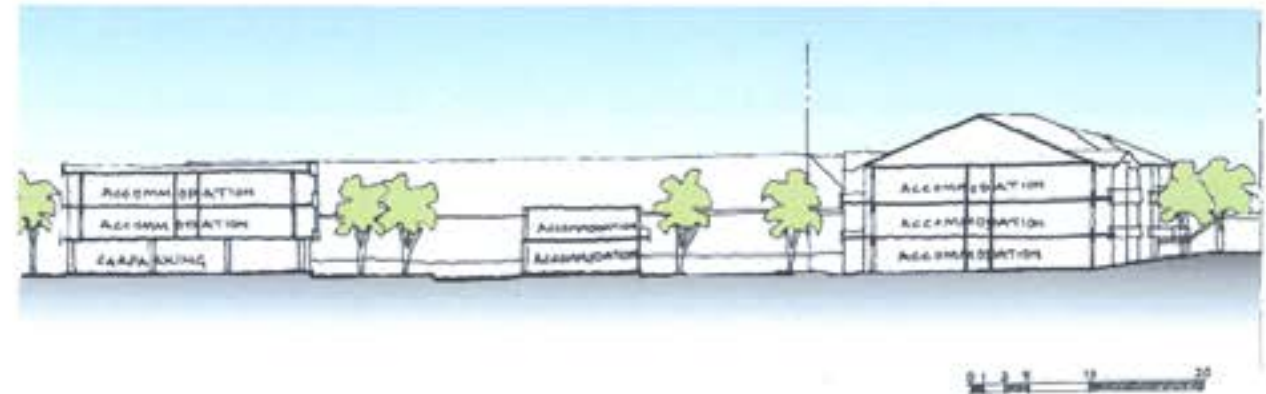


Figure 79b Indicative Section through Precinct 2D and Precinct 2C (Source: Dieke Richards, 2008)



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### 4.6 Connections to La Trobe University

The holistic and integrated design approach adopted for the PDP includes a new pedestrian and cycle link between the Polaris Neighbourhood Activity Centre (Precinct 1B) and La Trobe University located across Main Street to the south-east within Precinct 1B. This new pedestrian crossing facilitates greater access and permeability between La Trobe University and Polaris.

The new pedestrian crossing will encourage people to use alternate, more sustainable modes of transport rather than rely on vehicles, and will promote walkability within Polaris and the surrounding area.

The developer will consult with the responsible authority and La Trobe University with a view to discuss design options for a circuit around the oval.

A two storey height limit (above ground) for buildings adjacent to the existing residential areas and the university land may be required, but can be exceeded to a maximum of three storeys (above ground) to the satisfaction of the responsible authority.

The siting, scale, form and appearance of development (including car parking) adjacent to existing residential areas and La Trobe University must display an appropriate response to the adjoining residential area and to university land and buildings.

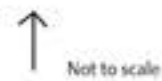
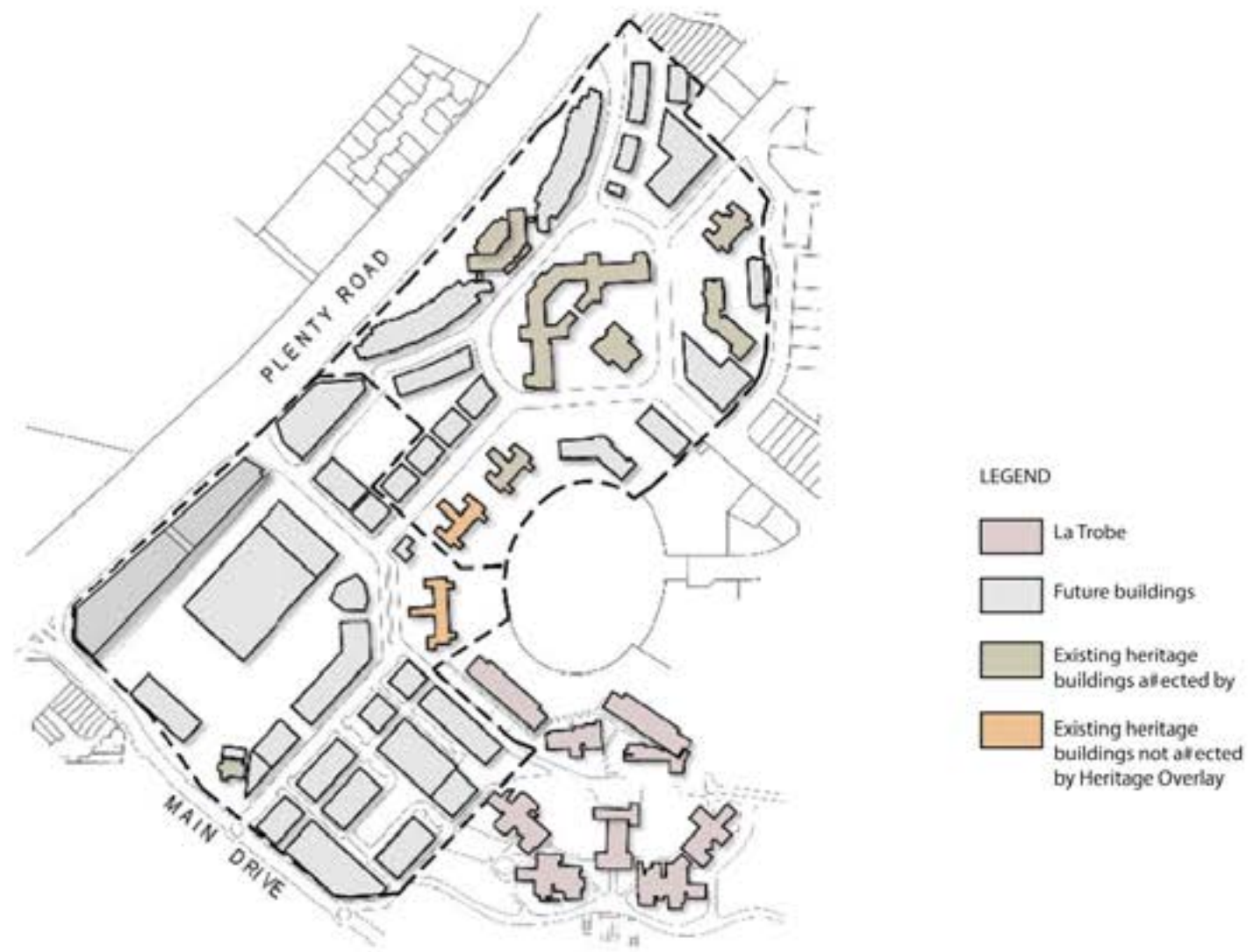


Figure 79C Connections to La Trobe University (Source: HASSELL, 2008)

# Polaris 3083 Development Plan

## 4.7 Urban Design Guidelines

The purpose of the Urban Design Guidelines is to:

- Allow for innovative viable refurbishment and re-use of the heritage buildings and the construction of infill/new elements within the site, which respect the character of these heritage buildings
- Create a sustainable Neighbourhood Activity Centre to serve service the local and broader retail/commercial needs within a high quality facility, as well as creating a mix of medium and higher density residential developments

Future use and development on the site must consider the urban design guidelines contained with the *City of Darebin Larundel Urban Village Design and Development Considerations (March 2001, revised August 2001)*.

Consideration must also be given to the following Urban Design Guidelines as follows in the next sections:

### 4.7.1 Development Envelope

#### Front Setback

Minimum front setbacks are intended to enhance a coordinated streetscape across each precinct and will ensure adequate distances between roads and new buildings and on-street car parking.

All lots are required to have a minimum front setback from the front title boundary as follows:

Precinct 1A - Accommodation 1 - 3 metres from the boundary.

Precinct 1B - Neighbourhood Activity Centre

Generally 0 metre setback for retail, residential and commercial buildings.

- Main Street frontage

Generally 0 metre setback for retail and commercial buildings. Varies for residential. At least 2 metres for residential buildings (Building 1B-16) for the upper levels above the lower three (podium) levels with generous setbacks to Linakers Cottage.

- Plenty Road frontage

Generally 0 metre setback for retail, residential and commercial buildings.

Precinct 2C - Accommodation

Varies, but should be complementary with existing buildings.

Precinct 2D - Accommodation

Varies, but should be complementary with existing buildings.

With the exception of vehicular access aisles established for the purpose of site entry, the front setback, where applicable shall be fully landscaped with paving, garden beds and/or grass as appropriate.

No building or storage shall be constructed within the setbacks above. New building setbacks from internal roads should respect the former hospital buildings on the site and the landscaped setting of the land, including provision area for the planting of canopy trees.

#### Side Setback

Buildings may be constructed to side boundaries. If buildings are not constructed on the boundary, the setback is to be a minimum distance of 3 metres, or as designated by Council ResCode (if residential building), or other relevant fire regulations.

Setbacks on corner lots must enable sufficient sightlines for traffic in accordance with the relevant Australian Standard (AS2980.1-1989).

#### Rear Setback

Buildings may be constructed to rear boundaries. If buildings are not constructed on the boundary, a minimum distance of 3 metres, or as designated by Council or other relevant fire regulations.

#### Building Heights

Generally, no building will exceed three storeys in height above ground level. Where it is necessary to exceed a building height of three storeys, for example due to the slope of the land, the proposal must be accompanied by appropriate urban design support to the satisfaction of the responsible authority.

In accordance with Section 3.8 and 3.9 of the PDP, there is an opportunity for building height of up to 8-9 storeys for Building 1B-01 and 5-6 storeys for Building 1B-02. The town centre's lowest topographic values are located along Main Drive and at the Main Drive and Plenty Road intersection and both building envelopes are identified as potential landmarks and focal points, creating a sense of arrival and place for the Polaris Estate.

Both buildings front Plenty Road which has a 42-metre road reserve width and has a designated upper height limit of 6 storeys proximate to the Polaris Estate site under the Design and Development Overlay Schedule 17 and in the Plenty Road Corridor Urban Design Framework. Furthermore, a 6-storey height applies to Lot 2D under the Village Precinct Development Plan.

Having regard to local context considerations and the future 7-10 storey, podium/tower built form character anticipated at the northern edge of La Trobe University under the MCMP, there is also opportunity for building height of up to 5 storeys for Building 1B-16 with generous setbacks to Linakers Cottage.

Unless otherwise specified, buildings will generally not exceed 11.5 metres in height as measured from natural ground level to the satisfaction of the responsible authority. Where the building incorporates a pitched roof, the roof may exceed this height provided it does not contain any habitable space. Where an overall building height of 11.5 metres is exceeded, the proposal must be accompanied by appropriate urban design support to the satisfaction of the responsible authority.



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### 4.7.2 Building Design Standards

#### General Approach

The architectural style at Polaris is intended to represent a high quality mixed use development.

Hard stands, loading, storage, plant and equipment areas should be located to the side and rear of the site buildings, and screened appropriately using landscape elements.

Environmental and sustainability factors should be considered in relation to building siting, materials and finishes, and ongoing energy requirements.

The provision, where appropriate, of a 'dual address' for buildings which abutt both a road and an open space area or the grounds of La Trobe University, or existing residential areas located within the adjoining Lancaster Gate Estate should be considered.

'Back to back' forms, where appropriate, should be considered in instances where the slope of land limits the ability to provide active/attractive frontages to both sides of buildings.

#### Architectural Style

Buildings should demonstrate excellence of design and attention to construction quality.

Building facades are to be of a simple contemporary architectural style.

The impact of the size of the development when viewed from the street is to be reduced by avoiding bulky roof forms or extensive blank facades in a single material or colour.

Architectural features are to be provided at ground level giving an entrance element to the building and addressing the primary street frontage.

Development on corner lots is to address both street frontages in terms of facade treatment and articulation of elevations.

Exposed blank walls on the boundary of a lot, and all exposed basement walls, are to be finished to the same standard as the rest of the building, and also to minimise the potential for graffiti or other vandalism. The visual impact of a large expanse of wall is to be reduced in scale by architectural treatment and/or use of colour.

The design statement of blank walls should be avoided and be treated to ensure an attractive and visually appealing presentation.

Outbuildings and ancillary installations should be compatible with the design theme established by the primary building of the lot.

The design and appearance of active frontages and activated edges should aim to achieve a high level of pedestrian amenity and engagement through a combination of uses, entrances, windows/glazing and other appropriate design details and treatments.

Plant and equipment will be concealed, or in the case of free-standing structures, appropriately screened from view.

Exterior elements, such as roof signage, plumbing, heating and ventilation systems are to be integrated into the building.

Development within the Neighbourhood Activity Centre (Precinct 1B) should be sited and designed to ensure that noise emissions and odours from activities/uses do not adversely affect the amenity of residential land uses within, and adjacent to, the activity centre. An acoustic assessment of likely noise impacts as a result of activities/uses, and incorporating recommended techniques to avoid and/or mitigate the impact of noise on residential amenity, must be provided as part of planning permit applications.

Development within the Neighbourhood Activity Centre (Precinct 1B) should be designed in consideration of the *Activity Centre Design Guidelines (2005)*, Department of Sustainability and Environment, (or any superseding guidelines), *The Safer Design Guidelines for Victoria (2005)*, Crime Prevention Victoria and Department of Sustainability and Environment (or any superseding guidelines), and the urban design guidelines contained within the City of Darebin Retail Activity Centres (December 2005, Essential Economics P/L, Planning by Design and David Lock Associates).

#### Materials and Finishes

A varied palette of materials is to be used to ensure a visually rich environment.

Materials and finishes are to be robust.

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## 4.7 Urban Design Guidelines Continued

### 4.7.3 Car Park Design and Loading Design

#### Car Parking and Circulation

Visually open sided sub-basement car parking areas including that of the supermarket should be avoided and be treated to ensure an attractive and visually appealing presentation.

Car parking areas and access ways should be designed to the relevant Australian Standard and the requirements of the Darebin Planning Scheme, particularly with regards to aisle widths and bay sizing.

At-grade car parking should be designed to ensure an attractive presentation and are appropriately landscaped.

Car parking areas must be suitably landscaped. Large parking areas should be designed with integral landscape islands to allow additional tree and shrub planting. Low hedging should be used to minimise views of car park from street. Refer to Figure 79d and Benchmark Images X, Y, Z and AA for a conceptual indication of a typical car park concept plan.

Entrances to all private car parks must be via concrete crossings, to the satisfaction of the responsible authority.

Where 90° angled parking is to be provided adjacent to a full width footpath, the width of the footpath must allow for vehicle overhang, to the satisfaction of the responsible authority.

Where there are to be kerbside cafes, a 1.5 metre clear path is to be maintained, to the satisfaction of the responsible authority.

'Shared spaces' for car parking as that term is defined in the Section 173 Agreement between Darebin City Council and VicUrban (dated 18 April 2007), shall be provided in accordance with that Section 173 Agreement to the satisfaction of the responsible authority.

Where parking is proposed adjacent to heritage buildings, the car parking design, layout and material construction shall have regard to heritage considerations.

#### Loading / Unloading

Loading areas and service bays should be designed to the relevant Australian Standards, and approved in accordance with the Darebin Planning Scheme.

Where possible, loading and servicing areas should be designed as an integral part of the development on each lot. It is preferable that loading bays be contained entirely within buildings. Where this is not possible, external loading and servicing areas should be fully screened from view of the street using materials consistent with the primary building.

Loading and service areas must be hard paved with concrete, asphalt or other all-weather surface with adequate drainage.

Entrances to all private car parks must be via concrete crossings, to the satisfaction of the responsible authority.



LEGEND

- LAWN
- GARDEN BEDS
- SCREEN - HEDGE
- EXISTING TREE
- STREET TREE PLANTING
- TREE PLANTING
- CARPARK AREA
- PATH



Benchmark Image X



Benchmark Image Y

Use of hedge planting within roadside verges and surrounding carparks, reduces visual dominance of parked cars.



Benchmark Image Z



Benchmark Image AA

Hedges and canopy trees create pleasant carpark zones.

Figure 79d Typical car park concept plan (Source: HASSELL, 2008). Not to scale

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## 4.7 Urban Design Guidelines Continued

### 4.7.4 Landscape Design Standards

#### General Approach

Landscape is considered an important element in unifying and harmonising the overall Polaris theme and character.

Polaris' aim is for landscape to reinforce and extend the existing character of the site.

Appropriate reference should be made to Darebin City Council guidelines in relation to landscape proposals.

Environmental and sustainability factors should be considered in relation to minimisation of surface run-off, species selection, materials and finishes, and ongoing energy requirements.

#### Design Issues

Landscape plans for each lot should be prepared by a qualified landscape architect.

Landscape schemes should be bold but simple.

Landscape schemes should recognise the theme of the adjoining streetscape as a primary driver for development of the scheme.

All areas of the precinct must be landscaped, with particular attention to a high quality frontage.

Appropriate species should be used to visually soften and reduce building bulk and screen visually obtrusive areas.

Landscape schemes should provide amenity by provision of summer shade, screening and windbreaks and green spaces for pedestrian retreat.

The ability to practically and easily maintain the landscape in a neat and orderly condition should be considered at the outset of the design process.

#### Existing Vegetation

Some existing trees of high retention value, along with all landscape provided by the developer (within streetscapes and building setbacks), is integral to the development and should be protected and maintained.

Where existing vegetation is to be retained, it must be protected during construction.

#### Preferred Plant Species

An indicative list of trees, shrubs and ground covers for use at Polaris has been prepared as part of the Landscape Master Plan (Refer to Figure 80 in Section 4.7). This will ensure a cohesive overlay of similar species further reinforcing a unified landscape setting.

Generally, massed plantings of single species are preferred, rather than a mixed variety of species.

Issues to be considered in selecting species for the landscape scheme include local soil and weather conditions, sun and ongoing moisture requirements, shade and drought tolerance and screening potential.

Fast growing screening plants are preferred between properties (along side boundaries). However the species selected should also have a moderate life span and standardised structure and habit.

#### Materials and Finishes

Consideration should be given to the sustainability of materials. Where possible, materials should be locally sourced from a sustainable source, re-used or recycled.

Timber used in landscape schemes should be from a sustainable source, and be classified as suitable for exterior use.

Topsoil should be scraped from site (where present) prior to construction, stockpiled and re-used during landscaping.

All metal components used in landscape schemes should be suitable for exterior use, by hot dip galvanising or equivalent.

A specific schedule, nominating all materials and finish treatments should be included in the landscape and architectural plans (as part of the planning permit stage for each precinct).

#### Retaining Walls

Any retaining wall that cannot be safely designed as a gravity wall only must be designed and certified by a registered engineer.

No single wall may be greater in height than 1.0 metres. If a greater retaining height is required, then a stepped wall with appropriate setback between walls must be designed by a registered engineer.

#### Advertising Signage

Principles for the location of advertising signage must be provided at the planning permit stage of the development. Consideration must be given to the height, appearance, scale and presentation of advertising signage in context to its location and potential visual impacts.

Advertising signage must not contribute to visual clutter, be visually obtrusive to the amenity of the area or detrimentally impact upon viewlines or vehicle traffic sightlines.

#### Private Open Space

Design techniques for the location, design and appearance of private open space should generally avoid locating private open space where front fences will then be required to provide privacy.

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## 4.7 Urban Design Guidelines Continued

### 4.7.5 Fencing

Front fencing within Polaris will be generally avoided where practicable. However, where front fences are either unavoidable or are appropriate to increase the safety and security of an allotment, the scale and solidness of the front fence will be minimised through the use of permeable (non solid) materials and appropriate construction techniques (refer to the examples of appropriately designed front fences in Figure 79e, 79f, 79g, 79h, 79i and 79j). These figures demonstrate successful sensitively designed low and /or visually transparent front fencing which positively contributes to the character of the area and which will assist the urban design outcomes for Polaris to be achieved.

The visibility of all fencing within Polaris including public realm areas will be minimised and an uninterrupted flow of streetscapes within Polaris will be encouraged. Fencing will be sited and designed to complement and respect the existing and proposed landscaping, including the use of appropriate tree and shrub species to soften the appearance of fences, as appropriate.

Front fences should generally not exceed 1.2 metres in height and all fencing must be designed and constructed to complement the landscape and park like character of the site.

Screen fencing of a minimum height of 2.5 metres will be provided and maintained around all storage areas within Precinct 1B.

The siting, scale, form and appearance of fencing adjacent to existing residential areas in Lancaster Gate and La Trobe University will display an appropriate response to the adjoining university land and buildings.

All fencing will be subject to approval by the responsible authority.



Figure 79e Low and visually permeable fence on title boundary (Source: HASSELL, 2008)



Figure 79h see-through fencing and double storey buildings allow visual surveillance of the street (Source: HASSELL, 2008)



Figure 79i solid but low fences combined with landscaping help define the street geometry (Source: HASSELL, 2008)



Figure 79f see-through fencing and double storey buildings allow visual surveillance of the street (Source: HASSELL, 2008)



Figure 79j solid but low fences combined with landscaping help define the street geometry (Source: HASSELL, 2008)



Figure 79g Low and visually permeable fence on title boundary (Source: HASSELL, 2008)

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### 4.7 Urban Design Guidelines Continued

#### 4.7.6 Heritage Buildings

The building numbering adopted in this section is consistent with the *Larundel and Linaker's Cottage Conservation Management Plan (September 2000)*.

##### General Guidelines

With regard to the heritage buildings, the existing contrast between finely resolved and detailed brickwork and white window joinery should be maintained, as should the general character and configuration of windows and glazing bars.

Given that the vast majority of windows panes and sashes are badly damaged, and quite likely beyond practicable repair, it should be acceptable to replace these with windows to match existing. External doors should match the original detail.

The proposed new buildings within Precinct 2D, which are relatively proximate to the centre of the heritage grouping, could employ a palette of materials that complements the predominantly brick character of the heritage buildings.

In terms of additions to heritage buildings, new elements should either adopt a modern interpretation of inter-war detailing and finishes, or a complementary but clearly modern equivalent. This relates to such aspects as the proposed additions to Linaker's Cottage, the design of balconies or external stairs, etc to the proposed residential conversions of heritage buildings, and the notional 'reconstruction' of Building 1, in Precinct 2D.

Development within the Heritage Overlay must have regard to, and be consistent with, the *Larundel and Linaker's Cottage Plenty Road Bundoora Conservation Management Plan* to the satisfaction of the responsible authority.

Planning permit applications for development within the Heritage Overlay must include a detailed heritage design response addressing the requirements of the *Larundel and Linaker's Cottage Plenty Road Bundoora Conservation Management Plan*.

Where parking is proposed adjacent to heritage buildings, the car parking design, layout and material construction shall have regard to heritage considerations.

The location, design and appearance of private open space areas should be sympathetic to the existing heritage buildings, and supplemented with landscaping. Visually permeable fencing is appropriate to create private open space areas adjacent to existing heritage buildings. Refer to Section 4.7.5 for further details.

The following guidelines are specific to individual heritage buildings on the site:

##### Hospital Building (Building 2)

It should be possible to introduce simple modern interpretations of dormer roof forms to the roof of the building in order to accommodate a mezzanine level. Any dormer roof to Building 2C-02 must not be applied as a continuous form or comprise more than one-quarter or one-third of the roof length. A similar approach should be possible to some of the other heritage buildings, in order to provide some attic accommodation or storage in these buildings if required.

The curved road layout of the site should be retained and reinforced, and the gardenesque setting that is central to the character of the place will be encouraged to be retained where possible.

##### Linaker's Cottage

The significance of this building needs to be respected by means of retention of the entry porch and the general bungalow character as viewed from the road/street to the south. While it is possible to make substantial alterations and additions along the lines suggested to the east and west, there should be some indent or point of transition indicated between the addition and the extant outline of the cottage.

Additions should be low-key and recessive in character. Additions may be contemporary in character, but nonetheless they should also be sympathetic in form, materials and detail.

##### Former Administration Building (Building 1)

The section of the building located further away from the Hospital Building, including the west face toward Plenty Road, could be rebuilt in a more modern, interpretive manner that facilitates adaptive re-use if desired.

Restoration of the east 'front' of the building is considered appropriate. A more modern, possibly more highly glazed, quality to the envelope at the extremities closest to the proposed adjacent buildings within Precinct 2D, with a more modern face and possibly some increase in envelope in a compatible manner on the west face of the building, toward the road is considered acceptable.

##### Community Centre Heritage Building

Future development should provide special visual prominence to the community centre building in terms of the orientation of new streets, so that its public, community role is made prominent.

##### Precinct 2C

The design of Building 2C-10 will be sympathetic to Building 2C-05, in particular, the western triangular portion. In the event that the proposed treatment is not to the satisfaction of the responsible authority, the triangular section of the building should not be built and the building envelope for Building 2C-10 shall not extend further westward than the building envelope for Building 2C-09.

##### Precinct 2D

The design and siting of Buildings 2D-02 and 2D-03, particularly at the interface of Building 2D-01 should be sympathetic in form, materials and detail. Additions may be contemporary in nature, but respectful of Building 2D-01. Some form of setback or indent or glazed link should be incorporated between any new building envelope and Building 2D-01. Buildings 2D-02 and 2D-03 may be three storeys in height, subject to a sympathetic design response to Building 2D-01.

## Polaris 3083 Development Plan

### \*High Retention Trees continued\*

Lot S3 (1B-16): As per agreement with Council, Tree 199 is to be removed. A tree amenity off-set payment is to be paid to Council (using the City of Melbourne method) prior to first occupation of an approved building.

Replacement trees are to be planted to off-set removal of Tree 199. A landscape plan showing the proposed location of the trees inclusive of complimentary vegetation is to be prepared in support of any planning permit application lodged with Council. Replacement tree species to be to Council requirements.



### 4.8 Landscape Master Plan

#### Vegetation Strategy

Stephen Fitzgerald Arboriculture, completed an assessment of all trees located within the subject site in 2007 (refer to Appendix E). In the 2007 report, the arborist was engaged to assess the existing trees in terms of very high, medium or low retention status and identify any trees that have died, are in poor health or dangerous, and should therefore be removed. Resulting from this assessment and in regard to the development vision for the site, a vegetation strategy for Polaris has been developed, which is to provide for a new mix of exotic and native trees species to complement the existing character of the site, while at the same time, retaining as many as possible of the existing high retention value trees.

It is noted that there is neither a Vegetation Protection Overlay nor an Environmental Significance Overlay that covers the site. In addition, the trees are not identified as significant or to be retained, protected or conserved under the Heritage Overlay that pertains to the site.

There are no tree specimens considered indigenous to the local area on the site; therefore the provision of net gain will not apply for any proposed tree removal. Existing trees are a mixture of exotic species and non-indigenous native species, all of which were hand planted within the grounds of the former Larundel Hospital site.

#### New Planting

All existing and proposed streets will feature new street tree planting. These will comprise both exotic and native species to reinforce the current character of vegetation on the site. Selection will be based on providing solar access to dwellings, habitat creation, trees that do not damage infrastructure and the need to provide shady streets for pedestrian amenity. The other area of significant public realm includes the land surrounding the oval. This area will be supplemented with new tree and lawn planting to provide an attractive and passive interface between the oval and the heritage buildings. A suitable tree list has been developed in conjunction with the arborist and may include *Eucalyptus melliodora*, *E. leucoxyloides*, *Brachychiton acerifolius*, *Melia azedarach*, *Jacaranda mimosifolia*, *Pyrus sp.*, *Quercus cerris*, *Ficus sp.* Species selected must be more resistant to drought compared to many of the existing species on the site.

#### Very High Retention Trees

No trees of very high retention value were found by the arborist on the subject site. Refer to Appendix E for details.

#### High Retention Trees

The 2007 arborist's assessment nominated a total of 47 trees as having high retention value. These are mostly large mature trees that have landscape significance. The development plan responds to this by proposing to retain high

retention value trees as often as possible. In particular, many of these trees fall within the Village Campus Precinct and surround the heritage buildings. The Development Plan seeks to retain the landscape setting of these heritage buildings, thus ensuring that the high retention trees can mostly be retained.

The other area with a significant collection of high retention value trees is the townhouse precinct. Where practical, plans for this area seek to retain as many of these trees as possible within the public realm, either within reserves/parkland or within streetscapes, having regard to the nature of the medium density development proposed on the site, the appropriateness of the tree species, site levels and location of trees in relation to services and infrastructure. Inevitably some of these trees will be required to be removed to allow for development on the site. In particular, the retail and supermarket area requires large floorplates on level ground and this will result in removal of trees from this area.

#### Medium and Low Retention Trees

The Development Plan where possible will also seek to retain trees of lower retention value, recognising that these still contribute to the character of the site but also having regard to satisfying other criteria as well, such as developable yield, logical urban form, access and circulation and opportunities for improved replacement species and locations. The 2007 arborist report recorded a total of 146 trees of moderate retention value and 308 low retention value trees. Out of a total of 501 trees assessed on site, the 2007 arborist report nominated 259 trees as recommended for removal due to very poor health and potential danger.

#### Avenue Planting

There are some contiguous groups of trees planted in avenues that flank the existing roads on the site. While these contribute to the site's amenity and reinforce the heritage road layout, the species is considered inappropriate (*Fraxinus*) due to these now being considered a noxious weed. This is supported by the arborist's assessment. It is the intention of the Development Plan to replace these avenues with more appropriate tree species.

The vegetation strategy outlined above has been prepared in consideration of the overall vision for the Polaris site. This vision includes the creation of a sustainable (both economically and environmentally) development offering a mix of uses in a new urban setting. In order to achieve this vision, inevitably some trees in addition to those recommended by the arborist, regardless of their health and significance, will require removal. However, it is the intention to extensively landscape the estate with appropriate new vegetation species which will ensure the rejuvenation of the landscape and result in a net increase in numbers of trees when the development is complete. The extent of vegetation proposed to be removed on site is not considered excessive in this context.

#### Public & Private Realm Areas

Refer to Figure 84 for the conceptual layout regarding future public and private

status of land located within the PDP.

The road reserves (including the future pedestrian paths and nature strips) as shown highlighted in red are designated to be public space. Land adjacent to the oval and the community centre building will be located within the public realm. These public spaces will be shared and enjoyed by the new residential community and will have a high quality of public amenity created via consistent streetscape treatments, landscaping and public art.

Public access will be available across public land, as shown on Figure 84. The private lots will be permeable for vehicles, pedestrians and cyclists.

Delineation between public and private spaces will be through the use of pathways, fencing, material change, level change and hedge planting.

The road alignments and lot boundaries are indicative only at this stage and are subject to further detailed design.

#### Treatment to Oval Edge

The Landscape Concept Plan shows an indicative treatment to the edge of the oval, including a circuit path and avenue of native trees within the PDP.

The developer will consult with the responsible authority and La Trobe University with a view to discuss design options for a circuit around the oval.

#### Main Street Civic Plaza

Principles to guide the design of the proposed Main Street Civic Plaza within Precinct 1B have been prepared to ensure that this is developed as a generous, sunlit public space. These principles should be addressed as part of any planning permit application, as follows:

- The Main Street Civic Plaza should be designed to orient to the north to maximise exposure to sunlight
- A shadow analysis of the surrounding proposed buildings once designed, should be undertaken to ensure that the majority of the plaza space remains in sunlight, especially in the critical times of 22 September between 11am and 3pm
- Consideration should be given to the use of deciduous trees within the plaza to allow for solar access during winter months and shade during the summer
- Placement of street furniture should take into consideration the areas of shade and light, so that a variety of seating opportunities are provided within this space
- The dimensions of the plaza should allow for the space to operate as a pedestrian thoroughfare as well as to provide useable spaces for congregation/ dining etc





Not to scale  
Figure 80 Landscape Concept Plan / Overall Master Plan (Indicative Only)



Precinct 1B building envelopes updated  
Refer to Figure 49a for current detail.

LEGEND	No of trees to be retained per category	Percentage of trees retained per category
Very High Retention Value Tree	0	0%
High Retention Value Tree	34	72%
Medium Retention Value Tree	42	29%
Low Retention Value Tree	9	3%
Recommended for removal by Arborist	-	-
Tree Number (refer to Arboricultural report)	-	-
<b>TOTAL</b>	<b>85</b>	<b>-</b>

Not to scale  
Figure B2 Tree Retention Plan - Indicative Only (Source: HASSELL, 2009)

PMP9719

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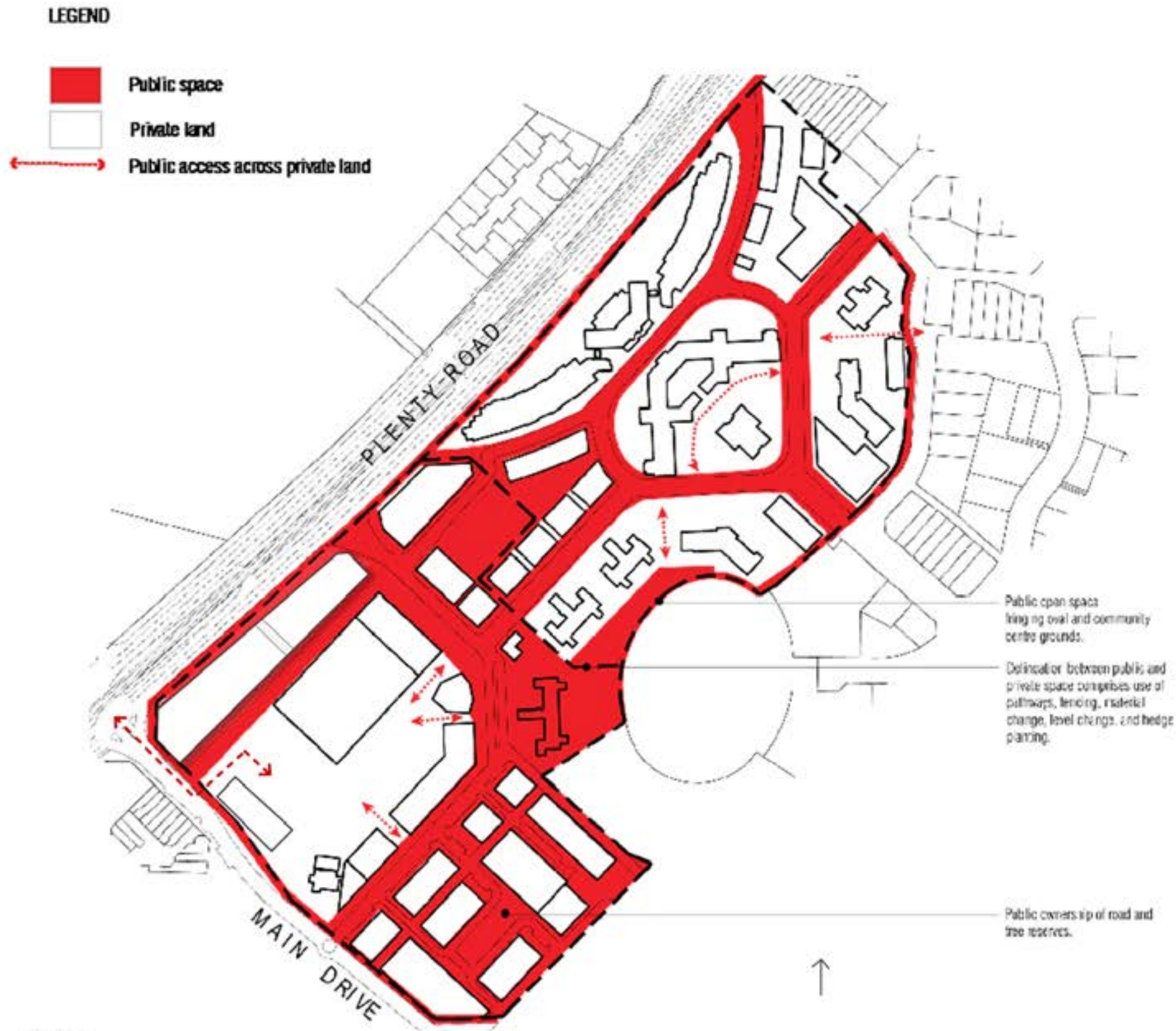


Precinct 1B building envelopes updated  
Refer to Figure 49a for current detail.

LEGEND	No of trees to be removed per category	Percentage of trees removed per category
Very High Retention Value Tree	0	0%
High Retention Value Tree	13	28%
Medium Retention Value Tree	104	71%
Low Retention Value Tree	299	97%
Recommended for removal by Arborist	-	-
Tree Number (refer to Arboricultural report)	-	-
<b>TOTAL</b>	<b>416</b>	<b>-</b>

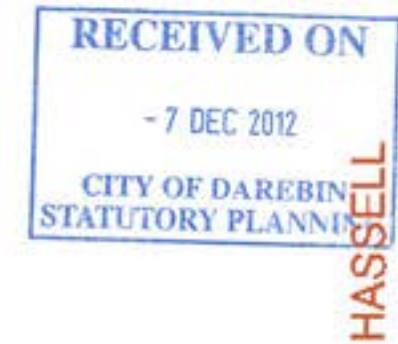
Not to scale  
Figure 83 Tree Removal Concept Plan - Indicative Only (Source: HASSELL, 2009)  
PMP5719





Not to scale  
Figure 84 Public Private Concept Plan - Indicative Only  
PMP0719

## Polaris 3083 Development Plan



### 4.9 Protection of Trees to be Retained

Tree Protection Zones (TPZ) must be generally established prior to construction around trees and/or groups of trees that are to be retained. The TPZ will need to be determined in consultation with an arborist and is dependent upon tree species and age for example, and also to proposed activities envisaged to occur as part of the development adjacent to these identified trees. Where Council guidelines and policies do not specify otherwise, the following measures are to be implemented to protect trees during construction works:

#### Generally

- Protect all existing trees located on the site and on adjacent properties from damage
- Where trees have become damaged due to construction activities, carry out repair by an approved arborist using approved procedures, or replace the entire tree with a tree of equivalent species to the satisfaction of the responsible authority

#### Marking and inspection

- Prior to the commencement of buildings and works, mark trees to be retained using suitable non-injurious, easily visible and removable means of identification
- Carry out a thorough inspection of all trees to be retained for disease and pest infection, and carry out appropriate treatment procedures

#### Pruning and removal of dead wood

- Prune trees to shape and remove dead branches under the supervision and in accordance with the recommendations of a suitably qualified arborist
- Remove limbs with a high percentage of dead wood, decay or damage
- Undertake weight reduction pruning as recommended by arborist

#### Fencing

- Prior to the commencement of construction, construct a minimum 1.0 metre high fence around the TPZ. Retain fencing for the duration of construction
- All building envelopes should remain clear of the tree protection zones as may be recommended by an arborist to the satisfaction of the responsible authority.

#### Mulching

- Mulch TPZ with minimum 75 mm depth of woodchips (except in areas where under-storey of the tree is to be protected)

#### Warning signs

- Display a warning sign in a prominent position at each entrance to the site, warning that trees to be retained within the site shall be protected during the progress of the works

#### Harmful materials

- Keep the area within the drip-line free of construction material and debris
- Do not remove or add topsoil to the area within the drip-line of trees
- Do not place bulk materials and harmful materials under or near trees to be retained
- Do not place spoil from excavations against tree trunks

#### Bark

- Prevent damage to bark of trees to be retained

#### Roots

- Do not cut tree roots exceeding 50 mm diameter. Where it is necessary to cut tree roots, use means such that the cutting does not unduly disturb the remaining root system. All cutting of tree roots will occur under the supervision of a suitably qualified arborist

#### Compacted ground

- Do not compact the ground under trees

#### Watering

- Water trees as required, and in accordance with the arborist's recommendations

A planning permit must require that if any tree which is identified in Figure 82 of the Development Plan is destroyed during and by construction works, a monetary amount shall be paid to the responsible authority unless the responsible authority agrees to revegetation "works in lieu" of an equivalent value. The monetary amount in respect of each tree is to be determined by an appropriately qualified person and is to be agreed upon between Council and the permit applicant. The payment of contributions and offset plantings is to compensate for the loss of amenity through the removal of additional trees from the previous approved development plan, with the calculation of the amenity value of any tree to be removed based upon the existing Arborist Report (Appendix E: Lancaster Precinct Arboricultural Report prepared by Stephen Fitzgerald Arboriculture June

2007) in accordance with Council's Tree Retention Policy 2007. If any monies are paid to the responsible authority under this provision, then monies must be used by the responsible authority for revegetation works on the land.

#### Landscape Plan Requirements

Any future planning permit granted for Polaris should contain a condition requiring that where trees are proposed to be retained, tree protection zones must be provided which are to be temporarily fenced during construction and will remain free of buildings and works, to the satisfaction of the responsible authority.

The alignment, design and construction/installation of roads, footpaths, bicycle paths and all utility services will be designed to minimise damage or interfere with the health of trees (including tree roots).

Detailed landscape plans will be prepared and submitted as part of future planning permit applications within Polaris. As a minimum, the landscape plans must include the following, to the satisfaction of the responsible authority:

- Details of all existing trees to be retained and all existing trees to be removed. The genus, species, height and spread of all trees must be specified
- A planting schedule of proposed vegetation detailing the botanical name, common name, size at maturity and quantities of all plants
- Details of all surfaces including lawns, mulched garden beds and hard paving (such as asphalt, concrete, brick or gravel)
- Street trees within the nature strip/s adjacent to the property
- All constructed items including retaining walls, letter boxes, garbage bin receptacles, outdoor furniture, lighting, clothes lines etc
- Edge treatment between grass (lawn) and garden beds
- An outline any building/s including any basement, the location of entry doors, windows, gates and fences
- The location of both existing and proposed overhead and underground services. Conflicts of such services with the existing and proposed planting must be avoided
- Clear graphics identifying trees (deciduous and evergreen), shrubs, groundcovers and climbers; and
- A scale, north point and appropriate legend

The species of all proposed plants selected must be to the satisfaction of the responsible authority.



DEAL CORPORATION

## Polaris 3083 Development Plan

### 4.10 Environmental Management Plan

Prior to the commencement of any development permitted by a planning permit, an Environmental Management Plan (EMP) must be prepared and submitted to the responsible authority for approval. The EMP must, as a minimum, include the following:

- Siltation, soil erosion and water quality control measures which are to be put in place before the commencement of any development and maintained during the development process
- Measures to prevent or control dust, smoke, noise, light or other potential sources of nuisance from construction works
- Details of the required standard of environmental management systems certification to be held by any contractor and to be specified in all works contracts
- Identification of all off-site impacts of construction works, and the mitigation procedures which are to be utilised to ensure such impacts do not unreasonably affect adjoining and nearby land
- Identification, assessment and removal/remediation of any contamination, including means by which any contaminated material will be contained and disposed of
- Measures to minimise/prevent soil erosion and the loss of soil to the surrounding environment, including the washing of vehicles and machinery and measures to minimise the deposit of soil from the site onto roads
- The means by which waste (including litter) will be contained, stored and disposed of
- Reference to compliance with any relevant Environment Protection Authority guidelines and State Environment Protection Policies. Construction activities must be consistent with EPA Publication 480 *Environmental Guidelines for Major Construction Sites*
- The *Environmental Management Plan for the Development of the Former Larundel Hospital Site, Plenty Road, Burdooora*, dated 12 November 2001

All development must be undertaken in accordance with the approved EMP to the satisfaction of the responsible authority.

### 4.11 Polaris Integrated Transport and Car Parking Plan

Ratio Consultants has prepared an *Integrated Transport Plan (July 2009)* (hereafter referred to as the 'Plan') that addresses the transport needs of the proposed mixed-use development, with emphasis on promoting the use of sustainable transport modes for access to/from the range of proposed land uses on the site, and reviewing transport issues associated with the development of the Polaris Neighbourhood Activity Centre.

The holistic and integrated design response adopted for the site ensures the support of public transport usage.

In accordance with Clause 52.36 - Integrated Public Transport Planning of the Darebin Planning Scheme, the PDP:

- Incorporates adequate transport networks within the site
- Has access to public transport facilities
- Is integrated into the transport system
- Incorporates and proposes new pedestrian linkages to the surrounding area (refer to Sections 4.2 and 4.6 for details).

The plan also provides a detailed overview of empirically derived car parking rates generated by various land uses envisaged to occur within Polaris. The Plan is intended to function as a flexible 'base' reference document for development and uses, particularly at the future planning permit stage for each precinct of the estate, where confirmation of car parking space numbers will be provided. There is also an opportunity to review at a later stage car parking rates based on updated empirical evidence that may become evident over time, or changes to car parking rates specified by the Victoria Planning Provisions.

It is intended for self sufficient car parking to be achieved on site within all precincts, via a combination of enclosed car parking, at grade car parking and on-street car parking and some basement car parks and/or semi-basement car parks. Deck, basement or semi basement car parks are likely to occur in both Precinct 1B (ie the Neighbourhood Activity Centre) and Precinct 2C. At grade car parks have been designed to be located, where practical, in the vicinity of the existing internal road system and close to buildings (intended to be re-used) and as part of new buildings. The configuration and layout of car parks are indicative only however, good design practice will ensure that the areas surrounding car parks are extensively landscaped, and will include canopy trees and water sensitive urban design methods of change. It is intended to avoid large areas to be devoted to car parks, which are obtrusively visible from main streets. Refer Section 4.7.3 for further detail and a typical car park concept plan.

Sufficient car parking for Precinct 2D will also be provided. However it is anticipated that the provision of a community bus made available to the aged residents of the retirement village in this precinct will significantly reduce the demand for on site car parks.

The design response is a balanced outcome to the features of the site and the surrounding area. The design maximises the benefit of a mixed land uses, as the car parking areas are able to be shared at different times during the day and week. In addition, the site has excellent access to the public transport system. A Green Travel Plan may be required as part of any planning permit applications.

Refer to Appendix C for details of the Plan.

#### Indicative benchmark images for car parking areas

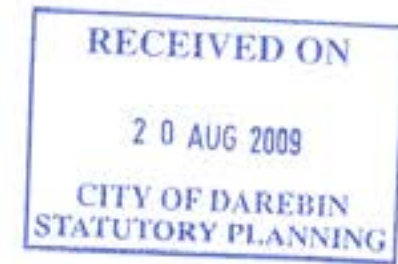


Benchmark Image BB  
Potential for paving paint to be used in car parks to improve pedestrian legibility and add visual interest.



Benchmark Image CC  
Appropriate water sensitive landscape design for effective stormwater management.

## Polaris 3083 Development Plan



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### 4.12 Polaris Engineering Services and Drainage Plan

The O'Neill Group has prepared an *Engineering Services Report (31 July 2009)* for the PDP (refer to Appendix G).

Appropriate consideration of the existing infrastructure and authority requirements at all design stages will provide the best opportunity to develop an economically viable and environmentally sensitive development that is sympathetic to the surrounding established areas and local community.

Subject to augmentation and upgrade works, Polaris can be adequately serviced by existing infrastructure. Consultation with all relevant services authorities at all design stages will ensure the coordination of utility services to minimise construction and maintenance costs and disruption to the public and landscaping.

The main points regarding roads, stormwater, drainage, sewerage reticulation, water supply, electricity, telecommunications and gas are outlined below:

#### Roads

- Darebin City Council is the responsible authority for roads, drainage and subdivisional works for this area and VicRoads is the responsible authority for works within Plenty Road. The internal road network, except for car park areas, will be developed as public roads in accordance with current adopted best practices to the requirements of Darebin City Council *Road Geometry and Design Guidelines* for subdivisional works.
- There is an extensive network of existing internal private roads within Polaris with a number of access points connecting to the adjoining public roads. Where appropriate, some of the existing roads may be suitable for rehabilitation rather than reconstruction.
- The design and construction of the street reserves and pavement widths for the new roads will provide for the required access and mobility, placement of services and allocation of adequate space for landscaping. The alignment, design and construction of all roads, footpaths, bicycle paths and utility services will be coordinated to minimise the impact to existing trees and vegetation identified for retention and/or protection.
- The existing access points in Plenty Road will be retained and upgraded where required subject to traffic engineering assessment in accordance with Council and VicRoads requirements.
- The road location within the PDP is indicative only.
- Details of road geometry and locations will be provided at the future planning permit stage of development of each precinct.
- Footpaths shall be provided at sufficient width for their intended service, to the satisfaction of the responsible authority.

#### Stormwater Drainage

- Darebin City Council and Melbourne Water are the responsible authorities for stormwater drainage in this area.
- The existing stormwater drainage infrastructure within the site will be decommissioned. The existing drains may serve both existing buildings and roads on the site as well as adjoining properties to the north and east of the site.
- Owing to the existing topography of the land Darebin City Council has advised that the legal point of discharge for this site is via the existing outfall stormwater drainage system in Main Drive near the corner of Plenty Road. The site currently discharges into La Trobe University land to the south via a 675 mm diameter drain below Main Drive which connects to a 1050 mm diameter drain in Plenty Road discharging into an open swale drain located within the university site. The existing outfall drainage system may need to be upgraded.

In relation to stormwater drainage, it is proposed that:

- The reticulated pipe component of the drainage plan will be designed for a 1.5 year flow for residential areas and 1:20 for commercial areas. The drainage discharge from the site is to be limited to the current capacity of the drainage at Main Drive. A 1:100 overland flow path is to be provided within the site in accordance with Melbourne Water and Darebin City Council *Drainage Design Guidelines* and retarded to pre-development conditions prior to discharge from the site. The internal property drainage systems and connections for the new and existing buildings will be designed in accordance with Australian Standards to the satisfaction of the responsible authority.
- All components of the drainage plan must be designed and constructed to the satisfaction of the responsible authority and in accordance with the document *Urban Stormwater – Best Environmental Management Guidelines* (CSIRO 1999) and the City of Darebin *Stormwater Management Plan*. This is to minimise surface run-off and discharge, to mitigate the adverse effects to the natural water balance and to promote retention and effective management of the stormwater drainage system with the use of increased soil infiltration and to reduce run-off.
- The capacity of the outfall stormwater system is primarily governed by the outfall drainage near the corner of Main Drive and Plenty Road which then discharges into La Trobe University. This drain does not have the capacity to cater for the fully developed site and options will need to be considered to either contain the discharge within the site by appropriate detention systems or by the creation of wetland systems.
- An on-site stormwater detention (OSD) system to reduce site discharges to pre-development conditions will be considered in lieu of downstream wetlands. The OSD

systems that can be considered include:

- Underground tank storage. This is particularly viable for the first stage of the retail development near the corner of Main Drive and Plenty Road.
- Graded car park areas which act as detention storage.
- Pipe storage, ie the stormwater drainage pipes can be oversized above the required capacity.
- Grassed and vegetated swales, filtration trenches and bio-retention systems together with gravel trenches and perforated pipes to increase subsoil infiltration.
- Rainwater harvesting and re-use for flushing of toilets and landscape irrigation will be provided with the use of above and/or below ground storage detention systems. Rainwater re-use for the irrigation of the existing council oval to the east also will be considered.
- The preferred method for stormwater drainage will be proposed as part of any future planning permit application for each precinct.

Prior to the commencement of any development permitted by a planning permit, a detailed drainage plan must be prepared to the satisfaction of the responsible authority. The drainage plan must include at a minimum, but is not limited to, the following details:

- Litter and silt removal treatment of stormwater prior to its discharge into the downstream drainage system.
- Measures to prevent flooding of surrounding properties by the 1 in 100 year overland flow path.
- Locations of rainwater harvesting where adopted and deemed appropriate by the environmental sustainable design report.
- Location and size of pipes.
- Location, type, access and cleaning arrangements for gross pollutant traps where necessary.
- Location, dimensions and design approach for overland flow paths.
- Identification of roads to be used as overland flow paths.
- Location, dimensions and design approach for any retarding basins and/or wetlands, where necessary, including design of inlets and outlets.
- Details of treatment proposed to control overland flow from the development adjacent to any areas of environmental sensitivity.

## Polaris 3083 Development Plan

### 4.12 Polaris Engineering Services and Drainage Plan Continued

- Performance specifications including flow frequencies, velocities and depths for overland flow paths (including roads where so used), length and volume of in-pipe storage of water, and frequency, depth and duration of water impoundment in any retarding basins and/or wetlands where necessary
- A management plan detailing procedures (including required expertise, equipment and materials), responsibility and cost estimates for installation and long term maintenance of any wetland system and/or retarding basin where necessary
- Details of who is responsible for the maintenance of any retarding basin where necessary
- The potential for on-site stormwater detention in lieu of downstream works. Details of proposed on-site stormwater detention, where necessary, must be provided

Other matters that should be considered at planning permit stage, where appropriate include:

- The relevant drainage requirements of Melbourne Water must be met.
- Development must be sited and designed to safely cater for overland flows through the site. All new lots and buildings must achieve appropriate freeboard in relation to any local overland flow paths to the satisfaction of responsible authority.
- Should any gross pollutant traps or other like measures be included in the Drainage Plan, these must be maintained and cleaned from the time of installation until at least 12 months following the completion of the development to the satisfaction of the responsible authority.
- All obsolete drains and service conduits are to be removed or abandoned to the satisfaction of the responsible authority.
- Before the responsible authority accepts maintenance responsibility for any element of the drainage system, the relevant element or elements of the drainage system must be to a standard and in a condition satisfactory to the responsible authority. In determining its satisfaction, the responsible authority will have regard to whether the elements of the system are in good repair, whether any siltation, rubbish or other blockage within the system has been removed and whether the element or elements of the system is or are built according to the approved Drainage Plan.
- Any rehabilitation is to be based on full pavement testing, with a minimum design life of 20 years and control of the 1 in 100 year overland flow to within the road reserves, to the satisfaction of the responsible authority.

#### Sewerage Reticulation

Yarra Valley Water is the responsible authority for the provision of sewerage in this area. The existing sewerage infrastructure within the site will be decommissioned and replaced with new authority sewerage reticulation.

In relation to sewerage reticulation it is proposed that:

- Any new sewerage reticulation shall be designed and constructed in accordance with the Sewerage Code of Australia, Melbourne Retail Water Agencies Edition, 2002 and Yarra Valley Water requirements.
- The sewer drainage and plumbing connections for the existing and new buildings will be designed in accordance with Australian Standards. A separate sewer connection point will be provided for each precinct or premise.

#### Water Supply

Yarra Valley Water is the responsible authority for the provision water in this area. The existing water supply infrastructure within the site will be decommissioned and replaced with new authority mains water reticulation.

- New water reticulation will be designed and constructed in accordance with relevant standards. This site is serviced by an existing Yarra Valley Water 225 mm diameter water main on the north side of Main Drive and 150 mm diameter water main on the north-west side of Plenty Road.

In relation to water supply, it is proposed that:

- New property connections and meters will be established for the existing and new buildings in accordance with Australian Standards. A separate connection supply point and water meter will be provided for each precinct or premise.
- The use of rainwater tanks for filling of toilet cisterns and for landscape irrigation including irrigation of the existing Council oval to the east will be considered.

#### Electricity

SPI Ausnet (SPI) is the responsible power supply authority for the provision of electricity to the development site.

The provision of electricity for the different stages of the development will be planned in conjunction with SPI to provide appropriate supply to serve the residential and retail/ commercial stages of the development. Supply connections to residential and commercial developments on the site will be in accordance with SPI conditions for making power available.

In relation to electricity supply, it is proposed that:

- Current High Voltage (HV) supply is available from SPI assets in Plenty Road. Multiple feed points from this HV electricity supply would be reticulated to serve the various stages of the development. Reticulation would generally be in the form of HV distribution, run underground to feed kiosk type substations and/or indoor type substations for power distribution to the various precincts.
- Generally, for the retail stage associated with Precinct 1, an indoor-type substation is required for power to the retail/commercial complex.
- For the residential lots, kiosk type substations will be located on nature strips of property boundaries, or in allocated reserves, from which low operating voltage electricity supplies will be reticulated underground to serve individual residential lots.
- Commercial and retail stages of the development will derive their electricity supplies from either appropriately located ground mounted kiosk substations or indoor type substations, depending on the electrical load for the type of development.
- Roadway and street lighting would be provided in accordance with appropriate Australian Standards and power supply authority and local Council codes.

#### Telecommunications

Telstra is the principal authority responsible for the provision of cable reticulation and telephone facilities for servicing the proposed stages of the development. Other providers such as Optus may also be required to distribute services within the complex.

Telephone underground conduit/pit systems and cabling would be provided throughout the development in accordance with Telstra's usual terms and conditions for residential and retail/commercial land development.

#### Gas

Origin Energy Asset Management plan indicates a 50 mm diameter distribution gas main adjacent to the site within Plenty Road and Main Drive. There are also 50 mm diameter distribution gas mains within Sanctuary Drive and Gonella Crescent to the east of the site.

Origin Energy is the principal authority responsible for the provision of gas reticulation to service the site. Supply can be readily provided to the development in accordance with its terms and conditions for residential and commercial land development.

## Polaris 3083 Development Plan



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### 4.12 Polaris Engineering Services and Drainage Plan Continued

#### General

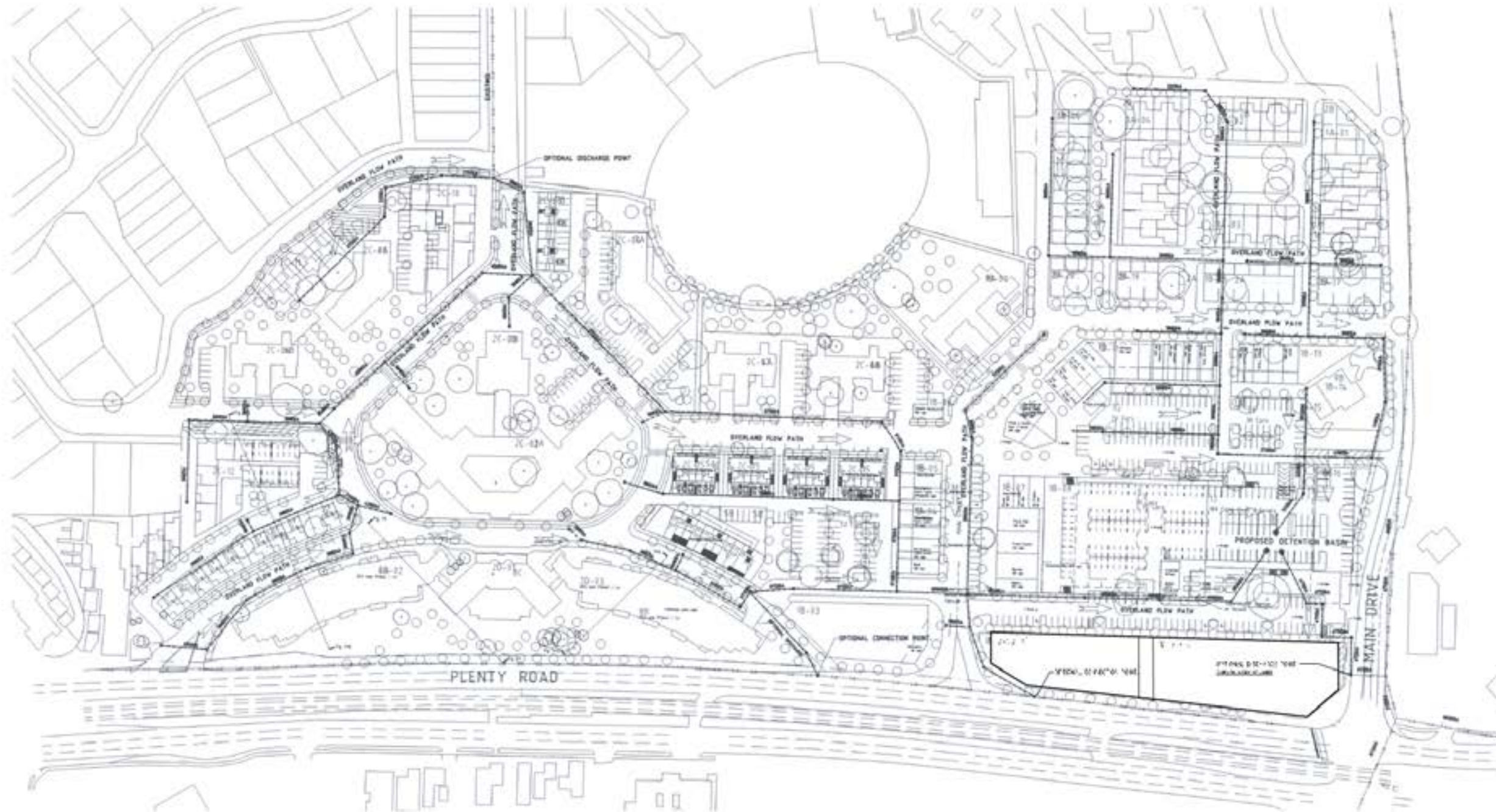
Street lighting along all roads and paths to the appropriate Australian Standard is to be provided within the development and at entry points, to the satisfaction of the responsible authority.

Fire hydrants must be located in accordance with the requirements of the Metropolitan Fire Brigade.

Roads and drains must be designed and constructed in accordance with the City of Darebin Road Geometry and Design Guidelines to the satisfaction of the responsible authority.

No buildings or works are to be constructed over any easement or other restriction on the land or any sewers, drains, pipes, wires or cables under the control of a public authority or the responsible authority without the prior written consent of the responsible authority and any relevant authority.

The provision of all services (electricity, gas, water supply, sewerage drainage etc.) must be undertaken to the satisfaction, and in accordance with the requirements/conditions of, the relevant authority. All telecommunication and electrical supply is to be provided underground.



Not to scale

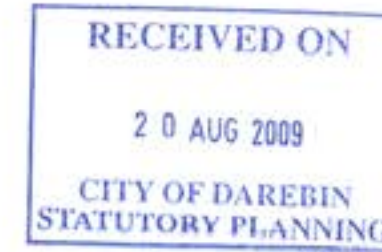
Figure 85 Drainage Concept Plan for Polaris - Indicative Only (Source: The O'Neill Group, 2008)

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## Polaris 3083 Development Plan



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### 4.13 Environmental Sustainable Development (ESD)

The holistic and robust design response of the PDP incorporates environmentally sustainable guiding principles and conservation strategies to be adopted in the future development stages of the site.

An ESD statement was prepared by Cundall (2007) for the PDP (refer to Appendix H).

Deal Corporation recognises the importance of achieving a strong sustainable outcome for the development. It also understands the key role it can play in achieving a strong environmentally responsive approach in the design and construction of the different sites within the development.

In consideration of this, a number of sustainability guidelines for the development have been prepared that addresses the following key environmental objectives:

1. Reduce its greenhouse gas emissions
2. Maximise its water conservation
3. Minimise waste both in construction and on-going
4. Improve the indoor environmental quality within the different facilities

In order to achieve these sustainability objectives for the development following Environmentally Sustainable Development (ESD) guiding principles have been established. These guidelines will be used to develop performance measures or targets to demonstrate how each stage of the development has met the sustainability objectives.

In addition to meeting the statutory Victorian ESD and BCA (Building Code Australia) energy efficiency requirements the respective design response for each stage of the development will address the following environmental design considerations:

- Minimise natural resource use through optimised energy efficient design responses
- Minimise potable water usage and effective management of stormwater
- Promote the use of sustainable and alternative transport systems
- Promote and create healthy living and working environments
- Minimise the environmental impact of construction materials
- Minimisation construction and ongoing waste
- Improve the external amenity and reduce its environmental impact

To determine the potential adoption of identified energy or environmental conservation initiatives will be investigated and assessed against the following key categories:

- Cost** e.g. capital, maintenance, energy life cycle and related building costs
- Flexibility** e.g. operational, layout adaptability, load adaptability and future expansion
- Reliability** e.g. ease of maintenance, resilience, simplicity and use of proven technology
- Environmental** e.g. greenhouse gas emissions, occupancy comfort and user controls

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#### ESD Guiding Principles

Guiding principles for the following functional areas within the PDP are included in the ESD statement:

- Energy consumption
- Potable water conservation
- Indoor environmental quality (IEQ)
- Materials
- Waste
- Site
- Atmosphere

Conservation strategies are outlined in the Cundall ESD Statement (2007) for each of the above mentioned Guiding Principles. Refer to Appendix H for further details.

An Environmentally Sustainable Development Management Plan (ESD Management Plan) should be prepared by a suitably qualified professional and must be provided as part of planning permit applications.

#### Environmentally Sustainable Development Management Plan

The ESD Management Plan, as a minimum, must address:

- Energy Management
- Water Conservation and Re-use
- Demolition and Construction Waste Management

Where appropriate, the ESD Management Plan should also:

- Identify relevant statutory obligations, strategic or other documented sustainability targets or performance standards
- Document the means by which the appropriate target or performance will be achieved.
- Identify responsibilities and a schedule for implementation, and ongoing management, maintenance and monitoring
- Demonstrate that the design elements, technologies and operational practices that comprise the ESD Management Plan can be maintained over time

A schedule for implementing and monitoring the ESD Management Plan must also be included in the ESD Management Plan.

**\*Refer to Darebin Planning Scheme for current ESD requirements.**

### 4.14 Retail & Commercial Economic Assessment

Charter Keck Cramer has prepared an independent retail and commercial (ie office) economic assessment for the proposed development (dated July 2008). This report assesses the impacts and benefits associated with the PDP including the Polaris Neighbourhood Activity Centre (ie Precinct 1B).

The report provides an overview of the demographic and socio-economic profile of the City of Darebin as a basis for the assessment. A thorough assessment is then undertaken on the amount of retail floor space and office floor space that is proposed as part of the PDP.

Charter Keck Cramer conclude that the amount of retail and office floor space proposed on the site as part of the Polaris Development Plan (formally known as and referred to in Charter Keck Cramer's report (2008) as the 'Lancaster' Development Plan) is acceptable and will result in a net community benefit for the City of Darebin. Refer to Charter Keck Cramer's Consolidated Retail and Commercial Economic Assessment (July 2008) for further details.



PLANNING COMMITTEE MEETING

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5.0 Strategic and Statutory Planning Review and Assessment



Polaris 3083 Development Plan

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5.1 Development Plan Overlay Schedule 1 Requirements (March 2001)

The following table demonstrates how the requirements of Schedule 1 of the Development Plan Overlay, of the Darebin Planning Scheme have been or are in the process of being satisfactorily addressed.

REQUIREMENT	HOW THE REQUIREMENT HAS BEEN ADDRESSED
The Development Plan should address the elements of the Darebin North East Corridor Strategic Plan relevant to the site and should include a plan drawn to scale which shows:	
The layout of existing and proposed buildings and works.	Building footprints of existing buildings on the site are provided in Section 3.0 of the PDP and are nominated on the PDP. (Figure 49a) The proposed building footprints are indicative only and the final location of proposed buildings will be determined during the planning permit application stages for each precinct. The layout of proposed buildings are show in Section 4.0 of the PDP.
All heritage buildings.	Building footprints of existing heritage buildings on the site are provided in Section 3.0 of the PDP and are nominated on the PDP.
Proposed uses on all parts of the land.	Proposed uses on all parts of the land are nominated in Section 4.0.
Elevation drawings of all buildings.	Indicative benchmark images of future developments of each precinct are located within Section 4.0. A concept height plan (Figure 86) is located in Section 5.3.
A drainage plan that details the proposed drainage scheme to service the development.	A preliminary <i>Stormwater Drainage Master Plan</i> (31 July 2009) was prepared by The O'Neill Group and is located in Section 4.12. Specific details relating to preparation of a Drainage Plan can be provided at the planning permit and subdivision stages for each precinct. Also refer to Figure 85.
Details of materials and finishes to all buildings and surfaces.	Indicative benchmark images of materials and finishes of buildings are provided in Section 4.0 of the PDP. Specific details of materials and finishes of future development will be provided at the planning permit and subdivision stage for each precinct.
An overall scheme for landscape development including the location, spread, height, species and proposed irrigation system for all existing and proposed planting	An assessment of the vegetation on the site is provided in the report <i>Lancaster Arboricultural Report (June 2007)</i> prepared by Stephen Fitzgerald Arboriculture. Refer to Section 4.8 and Figure 80 for the Landscape Master Plan. Further details will be provided at the planning permit and subdivision stage.
An environment survey which identifies significant stands of indigenous vegetation, individual trees and fauna habitats to be preserved.	An assessment of the vegetation on the site is provided in the report <i>Lancaster Arboricultural Report</i> prepared by Stephen Fitzgerald Arboriculture (June 2007). Refer to Appendix E for detail. Refer to Section 4.8 of the PDP for the Landscape Master Plan. Additional details will be provided at the planning permit and subdivision stage for each precinct.
Details of the treatment of all internal roads, road connections to the adjoining road network and the location and nature of other transport facilities to service the development.	A preliminary <i>Engineering Service Report</i> prepared by The O'Neill Group (31 July 2009), is located in Appendix G. An <i>Integrated Transport Plan, (July 2009)</i> prepared by Ratio in Appendix C. Section 4.0 of the PDP. Further details to be provided at the planning permit application stages.
The treatment and design of bicycle and pedestrian pathway network, including links to adjoining land and networks.	Section 3.2 of the PDP and the <i>Integrated Transport Plan, (July 2009)</i> (Appendix C). Details to be provided at the planning permit application stages.
The staging of all development.	Section 5.1 outlines the indicative future staging of all development on site.
The proposed subdivision of the development.	The proposed subdivision is shown on the PDP (Figure 49a and 49b) and is indicative only. Details will be provided at the planning permit and subdivision stage.
A traffic management plan outlining traffic requirements both within and outside the site.	Ratio Consultants <i>Integrated Transport Plan (July 2009)</i> and further details will be provided at the planning permit and subdivision stage. Section 4.11 provides a response to this requirement.
Details of vegetation retention and enhancement and vegetation removal.	An assessment of the vegetation on the site is provided in the report <i>Lancaster Precinct Arboricultural Report</i> prepared by Stephen Fitzgerald Arboriculture (June 2007). Landscape Master Plan in Section 4.8 of the PDP document. Details will be provided at the planning permit and subdivision stage.
Urban design guidelines.	Section 4.7 of the PDP document.

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5.2 Larundel Urban Village Design and Development Considerations March 2001 (revised August 2001)

The Lancaster Gate Development Plan (2001) requires the Development Plan to be amended prior to any future use and development of the Village Precinct and Mixed Use Precinct. The Polaris Development Plan is subject to Darebin City Council approval and has been informed by The Lancaster Gate Urban Village Design and Development Guidelines (2001).

In relation to The Lancaster Gate Development Plan (2001), the Mixed Use Precinct is referred to as Precinct 1, and the Village Precinct is referred to as Precinct 2 in the PDP.

Refer to the table below for an assessment of the Polaris Development Plan against the principles contained within the Lancaster Gate Urban Village Design and Development Guidelines (2001).

Development Principles	Assessment/Response
When considering an amendment to the approved development plan, consideration must be given to the relevant Development Plan Overlay provisions within the Darebin Planning Scheme and the provisions within the approved development plan. In addition, the following development principles will specifically apply where amendments relate to the mixed use/heritage precinct must:	Consideration has been given to the provisions of the Schedule to the Development Plan Overlay and to the provisions within the approved development plan (refer to Section 5.1 and 2.3.4 of this submission for a detailed assessments)
<ul style="list-style-type: none"> <li>Integrate and/or interface commercial, residential and community uses to create critical mass for activity levels in the village centre supporting recreational, community and commercial enterprises</li> </ul>	Precinct 1 and Precinct 2 create critical mass for activity loads to support both commercial, residential and community uses in accordance with this principle.
<ul style="list-style-type: none"> <li>Design each precinct to reflect sustainability principles</li> </ul>	Each precinct has been informed by ESD to reflect sustainability principles. Refer to Sections 4.10 and 4.13 for details regarding environmentally sustainable design principles that will be incorporated into the design response and future detailed design of each precinct.
<ul style="list-style-type: none"> <li>Provide a single trip/multi-activity destination</li> </ul>	The PDP provides a mixed use activity destination, specifically within the NAC/Precinct 1B.
<ul style="list-style-type: none"> <li>Support community sustainability for the new suburbs Larundel (Lancaster Gate), Mont Park, (Springthorpe), and the existing suburbs of Gresswell Grange and Mt Cooper</li> </ul>	The PDP will support the community sustainability of the site and will also serve the needs of the residential estates within the surrounding area by the provision of mixed use activities and open space opportunities as part of the holistic design response.
<ul style="list-style-type: none"> <li>Generate critical mass for the commercial centre viability as a neighbourhood centre</li> </ul>	Critical mass is generated and supported within Precinct 1B as a neighbourhood activity centre. The Consolidated Retail and Commercial Economic Assessment (Charter Keck Cramer, July 2008 – Appendix F) supports the amount of retail and commercial/office floor space proposed within Precinct 1B.

<ul style="list-style-type: none"> <li>Integrate the usage of the heritage buildings into a vibrant activity node</li> </ul>	All existing heritage identified buildings on site are to be retained and incorporated into the development. Each heritage building will be reused and repaired to improve the contribution that each building makes to the heritage significance of the site.
<ul style="list-style-type: none"> <li>Promote a 'main street' approach to the retail/commercial service node. Provide for a section of Main Street to include low key or SoHo commercial, service and residential use.</li> </ul>	Precinct 1B includes Main Street which intercepts with both Plenty Road and Main Drive. Main Street provides views of the heritage buildings from Plenty Road and Main Drive. Refer to Section 4 for the design response and further detail. Main Street will be activated by a variety of uses, including commercial, service and commercial/residential SoHo uses in accordance with this principle.
<ul style="list-style-type: none"> <li>Develop an integrated car parking solution, which maximises the use of available on street car parking bays, rather than massing of car parking in one location</li> </ul>	The Integrated Transport Plan, (July 2009) prepared by Ratio provides a car parking solution which maximises the use of street car parking, deck and basement and semi-basement rather than massing car parking in one location. In accordance with the Urban Design Activity Centre Guidelines of Melbourne 2030, single large tracts of areas devoted to car parks have been avoided. The design response for the car park maximises on street parking. Landscaping and built form has been designed to avoid large tracts of at grade car parking visible from adjacent streets. Views of the car parks has been minimised by the use of basement car parking being in response to the slope of the site. Refer to Section 4 and Appendix C for more detail.
<ul style="list-style-type: none"> <li>Build on public transport/bicycle network linkages by developing high quality, well lit, direct walking routes, shelters and bicycle parking</li> </ul>	Section 3.1 provides a detailed description regarding the opportunities for a permeable pedestrian and cycling network throughout the site that is linked to the remainder of the municipality. Refer to the Landscape Master Plan/Overall Master Plan (Indicative Only) for detail (Section 4.8).

Polaris 3083 Development Plan

5.2 Larundel Urban Village Design and Development Considerations March 2001 (revised August 2001) Continued

<ul style="list-style-type: none"> <li>Provide a range of services and facilities necessary to meet local needs</li> </ul>	<p>The holistic approach adopted provides for the opportunity to provide a range of services and facilities to meet local needs both in the form of accommodation, community services and other activities. Refer to Section 4 for more detail.</p>
<ul style="list-style-type: none"> <li>Use the development as an opportunity to maximize the long-term viability of the adjacent La Trobe University and provide a genuine hub for the students</li> </ul>	<p>Precinct 1B, which is located in close proximity to the university, will provide an activity centre hub for the students of La Trobe University. Precinct 2C is envisaged to contain student housing associated with La Trobe University.</p>
<ul style="list-style-type: none"> <li>Use the existing vegetation as a theme base for the urban village</li> </ul>	<p>The majority of the existing vegetation will be retained in Precinct 2 which will be the theme base for the urban village. Refer to Section 4.7.4, 4.7.6, 4.8 and 4.9 for discussion regarding tree removal, tree retention and replanting and landscape design standards.</p>
<ul style="list-style-type: none"> <li>Develop ingress and egress to the site, which respects the Main Street concept and minimises potential integration, conflicts with the heritage buildings and the adjoining La Trobe University site</li> </ul>	<p>The proposed ingress and egress points to and from the site minimise the potential for conflict with the existing heritage buildings and with La Trobe University. Refer to the <i>Integrated Transport Plan, (July 2009)</i> prepared by Ratio, (Appendix C) for details and assessment.</p>
<ul style="list-style-type: none"> <li>Provide a basis for multi-layered development approach to the town village including shop-top housing, apartments above commercial buildings and medium rise residential developments</li> </ul>	<p>A range of uses in the form of a 'multi layered development' are provided for within Precinct 1, including shop-top housing and medium density housing.</p>
<ul style="list-style-type: none"> <li>Create a landscaped car parking layout which provides minimal parking along the Plenty Road frontage and avoids a single car park 'sea' of cars approach</li> </ul>	<p>At grade car parking areas are well landscaped with a number of existing trees to be retained and incorporated into the development. Refer to the Landscape Master Plan (Section 4.8) for detail. Car parking located along the Plenty Road frontage in Precinct 1B and Precinct 2D will be integrated into the development. The natural topography of the site will be utilized to assist with the construction of the basement car parks.</p>
<ul style="list-style-type: none"> <li>Create a landscape frontage to Plenty Road which ensures retention of existing trees and high quality design of buildings, which address this major municipal gateway</li> </ul>	<p>Retention of existing trees will be encouraged along the Plenty Road frontage where appropriate. Buildings along Plenty Road will be of a high quality design. Refer to indicative benchmark images and concept images located within Section 4 (specifically 4.8) of this submission for detail.</p>

<ul style="list-style-type: none"> <li>Integrate open space in the mixed use and heritage precincts, which provides respite, landscape amenity, as well as create a sense of place</li> </ul>	<p>Refer to the Landscape Master Plan/Overall Master Plan (Indicative Only) (Sections 4.7.5 and 4.8) for details regarding the sensitive integration of open space and landscaping around the existing heritage buildings.</p>
<ul style="list-style-type: none"> <li>Provide facilities (seating, playground, public art/sculpture, cultural interpretation of the sites heritage) which create a sense of place and belonging</li> </ul>	<p>Public facilities such as these are to be provided. Details will be finalized at the planning permit application stage of the development for each precinct.</p>
<ul style="list-style-type: none"> <li>Create traffic flows which support and enhance Main Street activity levels</li> </ul>	<p>Main Street will generate appropriate traffic flows in context to its role in the Polaris Neighbourhood Activity Centre. Refer to the <i>Integrated Transport Report, July 2009</i> (prepared by Ratio Consultants) for a detailed assessment regarding traffic flows.</p>
<ul style="list-style-type: none"> <li>Create active high quality frontage to Plenty Road/Main Drive, which will include windowed frontages, advertising designed as part of the architecture of buildings, landscaped frontage including existing trees, contemporary building design, materials and finishes</li> </ul>	<p>Precinct 1B and 2D are designed in response to the opportunity of visible exposure of Plenty Road. Buildings will be of a high quality architectural design and will make a positive contribution to the Plenty Road streetscape. Refer to Section 4.7 of the Urban Design Guidelines which will assist in achieving this principle.</p>
<ul style="list-style-type: none"> <li>Create a flexible, but not intrusive usage boundaries between the Village Precinct and Mixed Use Precinct</li> </ul>	<p>The boundaries between Precinct 1 and Precinct 2 are seamlessly integrated.</p>
<ul style="list-style-type: none"> <li>Building density, form, height and setbacks within the mixed use precinct must take into account interface treatments with:                             <ul style="list-style-type: none"> <li>The existing oval</li> <li>La Trobe University student accommodation site</li> <li>Residential development within the Lancaster Gate development</li> <li>Heritage precinct</li> </ul> </li> </ul>	<p>The PDP generally complies with this principle.</p> <ul style="list-style-type: none"> <li>Refer to the Landscape Master Plan (Figure 60, Section 4.8) for detail</li> <li>Refer to the PDP (Figure 49a and 49b and Section 4.6) for details.</li> <li>Refer to Sections 4.4 and 4.5 (precinct overviews) and the Urban Design Guidelines contained in Section 4.7 for details.</li> <li>Refer to Sections 4.4 and 4.5 (Precinct 1B and 2C) and the Urban Design Guidelines contained in Section 4.7 for details.</li> </ul>
<ul style="list-style-type: none"> <li>Interface treatments must ensure the following are addressed within the design response:                             <ul style="list-style-type: none"> <li>Minimisation of conflicting land uses</li> </ul> </li> </ul>	<p>Land uses have been sensitively considered and co-located to avoid future uses having a detrimental impact on adjoining land uses. Refer to Section 4.2 - Design Response in this submission for a detailed explanation.</p>

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5.2 Larundel Urban Village Design and Development Considerations March 2001 (revised August 2001) Continued

— Building height/location and minimizing potential for overshadowing and overlooking	Refer to Section 4.2 - Design Response for a detailed explanation regarding the proposed building heights. Potential impacts such as overshadowing and overlooking will be assessed at the planning permit stage of the development when details of the design are determined. However, buildings will be designed to minimize the potential for overshadowing and overlooking. Refer to Section 5.3.3 for heights of future development (Figure 86).
— Protection of significant view lines	The significant view lines on the site (i.e. existing heritage buildings and vegetation) are protected and enhanced as a result of the PDP. Refer to Section 4.2 - Design Response for a detailed explanation.
— Protection of existing trees	Retention of the existing trees on the site is encouraged where practical. The design response of the development plan includes the retention of existing trees on the site. Refer to Section 4.2, 4.8 and 4.9 for details.
— Regard to the adjacent urban character and heritage in the design and layout of all buildings, car park areas and landscapes	The heritage character of the site has been respected, maintained and enhanced as a result of the PDP. Refer to Appendix D.
▪ The oval to be integrated and used as a distinct node depending on the role function and sizing of the village centre	The oval has been integrated into the PDP (refer to the Landscape Master Plan, Figure 80 for details). Land adjacent to the oval will be public open space, and the community centre will be located adjacent to the oval.
▪ A sports pavilion to be provided to support the role of the oval as a local recreation facility	There is the possibility of using the community centre building as a sports centre/sports focal point. In conjunction with the oval, the community centre is able to serve as a local recreation facility, should the City of Darebin desire.
▪ Housing diversity particularly lower cost housing opportunities to be pursued, including evidence of discussions with relevant housing providers and agencies	A variety of residential land use types are envisaged to be developed with the site to meet housing diversity and affordable housing. Discussions have been held with La Trobe University in relation to the demand for student housing.

1. Project Sizing (Heritage and commercial/mixed use precinct)	An indicative plan based on the vision and principles are indicated to provide guidance. It is based on the following project elements. These elements are indicative and do not preclude other land uses and mixes of uses, subject to Council approval. Further, site characteristics which must be taken into account when developing the precinct (ie protection of significant trees, landscape setbacks, open space provision, interface treatments etc), appropriate siting of new buildings, may result in changes to the following estimated project sizing.	The exact floor areas of each use are not known at this stage and the estimated floor areas are indicative only. These will be formalised at the planning permit stage for each precinct.
1.1 Residential		
▪ Student housing (x 400 beds)		Precinct 2C provides opportunity for the reuse of an existing heritage building for the residential type of student housing. Approximately 125 student housing units are envisaged.
▪ Town houses (2/3 storey including SoHo) (x 50) – Main Street		Precinct 1A provides the opportunity for medium density housing in the form of townhouses. Approximately 53 dwellings in the form of 2-3 storey townhouses are envisaged for Precinct 1A. Development now complete.
▪ Apartments above/integrated with retail (x 100) – encourage 3-storey walk-up		Precinct 1B provides opportunity for shop-top housing above retail or office use. Approximately 39 dwellings are proposed within Precinct 1B. Development now complete.
▪ Medium rise (2 blocks x 16 units per block ie 32 units). 11.5m height limit consistent with existing buildings		The Development Plan provides opportunity for medium density housing (ie approximately 69 dwellings within Precinct 1A and approximately 191 dwellings within Precinct 2C). There is opportunity to reuse the existing heritage buildings on site with residential uses. Development now complete.
▪ Apartments in existing heritage buildings/non-student housing – 30 units		It is envisaged that the heritage buildings have the opportunity to be reused with residential development. Refer to Section 4.5.1 for detail. Development complete or under construction.

Polaris 3083 Development Plan

5.2 Larundel Urban Village Design and Development Considerations March 2001 (revised August 2001) Continued

<ul style="list-style-type: none"> <li>Other residential in residual areas of mixed use zone as permitting in land areas</li> </ul>	The Development Plan provides for a variety of residential housing types on the site. Refer to Section 6.2 for detail of the dwelling yield and dwelling type breakdown.
<b>1.2 Retail</b>	
<ul style="list-style-type: none"> <li>5,300 sqm/plus car parking</li> </ul>	The gross lettable area of retail floor space is approximately 11,790 square metres for the Polaris Neighbourhood Activity Centre within Precinct 1B. The Retail and Commercial Economic Assessment (July 2008) prepared by Charter Keck Cramer supports 11,790 square metres of gross lettable retail floor area on the site.
<ul style="list-style-type: none"> <li>Supermarket 3,700 sqm</li> </ul>	The floor area for the supermarket is approximately 3,700 square metres.
<ul style="list-style-type: none"> <li>Specialty 3,700 sqm (say 20 – 25 shops including newsagent, pharmacy, restaurants (6 x 120 sqm), bakery, medical, hairdresser, video, take-away etc)</li> </ul>	The total floor area of specialty shops is approximately 3,700 square metres. The Retail and Commercial Economic Assessment (July 2008) prepared by Charter Keck Cramer supports 3,030 square metres of specialty shops on the site.
<ul style="list-style-type: none"> <li>Total site requirement approximately 8,000 – 12,000 sqm</li> </ul>	The gross floor area of other retail shops and speciality shops totals 7,680 sqm. The PDP meets this guideline, as the total retail floor area proposed is 12,600 square metres, which is generally in accordance with the maximum of 12,000 square metres of this guideline.
<b>1.3 Offices</b>	
<ul style="list-style-type: none"> <li>First floor and some shop front opportunities</li> </ul>	Precinct 1B provides opportunity for a variety of future office uses on the ground floor and the first floor of new buildings.
<ul style="list-style-type: none"> <li>First floor – 2,000 sqm services offices</li> </ul>	Precinct 1B provides opportunity for a variety of first floor office uses.
<ul style="list-style-type: none"> <li>Shop front including Telstra, employment etc (500 sqm)</li> </ul>	The development plan provides the opportunity for Telstra to be an occupant of the site if appropriate.
<ul style="list-style-type: none"> <li>Total 2,500 sqm</li> </ul>	There is opportunity within Precinct 1B to accommodate office use. A gross floor area of approximately 10,000 sqm is proposed. Refer to Appendix F.
<b>1.4 Entertainment/Short term accommodation/recreation</b>	

<ul style="list-style-type: none"> <li>Hotel including bistro(s), function room, take-away bottle shop (3,000 sq)</li> </ul>	There is the opportunity for a tavern (ie hotel use) to be located within Precinct 1B. There is also opportunity for a residential hotel (12,900 sqm) to be located within Precinct 1B.
<ul style="list-style-type: none"> <li>No additional gaming machines will be allowed within the City of Darebin</li> </ul>	No gaming machines are proposed as part of the PDP.
<ul style="list-style-type: none"> <li>Including (say) up to 200 rooms/serviced apartments</li> </ul>	The PDP provides opportunity for a range of residential land use types which may include residential hotel and/or serviced apartments within Precinct 1B.
<ul style="list-style-type: none"> <li>Site area requirement 8,000 sqm</li> </ul>	There is adequate land available on site for recreation, entertainment and short term accommodation purposes.
<ul style="list-style-type: none"> <li>Gymnasium – small scale say 300 sqm</li> </ul>	There is opportunity for a small gymnasium to be provided on site, perhaps ancillary to a residential or as part of another commercial use.
<b>1.5 Open spaces</b>	
<ul style="list-style-type: none"> <li>Place of respite including seating, lighting, shelter, sculpture and a small range of facilities to accommodate uses of the town centre. The space should be easily identifiable, secure and integrate with town centre uses.</li> </ul>	Public open spaces are extensively provided through the site. The civic pavilion to be located within Precinct 1B is easily identifiable and integrates seamlessly with the adjacent uses. Public art will be located on the site, and the open space areas will be well lit, and seating and shelters will be provided.
<b>1.6 Community facilities</b>	
<ul style="list-style-type: none"> <li>Best located in village area to support community needs, eg private childcare facility</li> <li>Allow 250sqm and site area of 500 sqm</li> <li>Provision of a sports pavilion in an appropriate location adjacent the existing oval will be required</li> </ul>	The community centre is located within Precinct 1B adjacent to the oval.

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5.2 Larundel Urban Village Design and Development Considerations March 2001 (revised August 2001) Continued

<p>The Development Plan shall identify a site for a future pavilion adjacent to and in support of public use of the oval. The size and location of the site and the provision of access to the future pavilion shall be to the Council's satisfaction. The opportunity of utilizing an existing building for the facility (such as the kiosk) shall be assessed in the first instance in the preparation of the plan.</p>	<p>Following the site analysis and design response, it is considered that the community centre and public open space adjacent to the oval provides opportunities for Darebin City Council to develop a pavilion if desired. Pavilion provided in the Town Centre.</p>	<p>Creation of an active frontage to Plenty Road is important, to maintain a visual link to the proposed activities from the road and promote a safe pedestrian friendly environment. This interface could be managed through applying design solutions such as landscaping and 'Main Street' design. Facilities such as car parking can be designed not to dominate the road frontage. Careful design of the car parking and access including basement areas to this part of the precinct will ensure that impact on future residents is minimised, and that a sympathetic interface is created with Plenty Road (where achievable). All buildings located with frontage to Plenty Road should have windowed frontages, appropriate signage &amp; landscape setbacks. Building design should be contemporary with regard to materials and finishes.</p>	<p>Refer to Section 4 of this submission for a detailed design response of the PDP. The PDP generally complies with these principles. The Precinct 1B Plenty Road frontage creates a frontage that is appropriate for a Neighbourhood Activity Centre and ensures that impacts on future residents are minimised and that a sympathetic interface is created with Plenty Road. Refer to Section 4.4.2 – Precinct 1B, for further detail. Refer to Figure 57a for details for how activation and articulation of the Plenty Road frontage is to be achieved. Refer to the Landscape Master Plan in Section 4.8 for further indicative details regarding the landscaping along the Plenty Road frontage. Building design will be contemporary with regard to materials and finishes as per the indication benchmark images in Section 4.0.</p>
<p>The site so identified shall be purchased by the Council at the time of completion of the relevant stage of development, subject to agreement on the price and terms of sale. The construction and/or fit out of the facility may by agreement be undertaken by the developer to the Council's specifications. The construction and/or fit out of the facility shall be at the Council's expense.</p>	<p>Further discussions with Council are required in relation to this. Council subsequently purchased the community centre.</p>	<p>All new buildings should include sustainability principles in their design, including achieving a 5 star energy efficiency rating.</p>	<p>Refer to Section 4.13 for Environmentally Sustainable Development Principles that will guide the future design and development of each precinct.</p>
<p><b>1.7 Car parking</b></p>		<p>Uses on the boundary between the Village Precinct and mixed use precinct should be sensitive to the nature of the Village Precinct, particularly the heritage component. The Village Precinct encourages high quality building design and architecture, and this will also apply to development within the Mixed Use Precinct. Building heights and setbacks must take into account the nature of nearby buildings within the Village Precinct.</p>	<p>Uses envisaged for each precinct incorporate sensitive interfaces where appropriate (i.e. – with the surrounding residential estates to the north and east) and 'urban rhythm' interfaces where appropriate (i.e. with Plenty Road, and to a lesser extent, Main Drive). Refer to Section 4 of this submission for the design response and further detail. Refer to Section 5.3.4 for indicative building heights of each building. Consideration has been given to the significance of the existing heritage buildings on the site and the site's topography, with the aim of increasing, maintaining and enhancing views of the existing heritage buildings.</p>
<p>Total requirement dependent on project role, function and sizing – estimate 300 – 400 public spaces plus appropriate on site (private) spaces.</p> <ul style="list-style-type: none"> <li>Supermarket 4.5/100 sqm (public)</li> <li>Specialty shop 3.0/100 sqm (public)</li> <li>Office 3.0/100 sqm (70% on site)</li> <li>Hotel 50 (public), 15 (private)</li> </ul> <p>A rate of 0.35 spaces (evening) and 0.2 spaces per seat (lunch) is recommended for Restaurants.</p> <p>A peak rate of 0.45 spaces per room is recommended for Residential Hotel.</p> <p>A peak rate of 0.2 spaces per student is recommended for Student Accommodation.</p> <p>A review of applicable car parking rates and temporal demand and supply in the town centre will be undertaken at the planning permit stage for each site.</p>	<p>Refer to the Integrated Transport Plan, July 2009 (Appendix C) prepared by Ratio Consultants for a detailed empirical assessment of the future car parking demand for the site.</p> <ul style="list-style-type: none"> <li>Ratio estimate a rate of 5.0/100 sqm for a supermarket</li> <li>Ratio estimate a rate of 3.0/100 sqm for shops</li> <li>Ratio estimate a rate of 3.0/100 sqm for offices</li> <li>Ratio estimate a peak parking demand of 0.35 spaces per seat for a tavern.</li> </ul> <p>For land uses not listed in PDP or the Planning Scheme, a empirical assessment will be required in support of any future planning application for the use.</p> <p>In instances where the Planning Scheme and the PDP car parking rates differ, the lower of the two rates applies.</p>	<p>Uses on the boundary between the Village Precinct and Mixed Use Precinct and La Trobe University site should be sensitive to the nature of the university site particularly the heritage and significant vegetation and landscape component. High quality building design and architecture, is required adjacent this interface which will ensure minimal conflict of land uses and appropriate building development which is not of an excessive height (i.e. not above 2 storey). Location of car park areas should be appropriately located so as not to adversely impact on the university site's landscape environs.</p>	<p>Precinct 1A has an interface with La Trobe University. Refer to Section 4.6 for details regarding this interface. The future residential use and development of Precinct 1A will not have a detrimental impact on the use of the adjacent land with La Trobe University. Future detailed design will ensure the development adjacent to this interface will be treated appropriately. High quality built form building design and architecture will characterise future development of this precinct. Car parking and road layouts will be integrated into the future development of Precinct 1A. Refer to Section 4 for the design response and future detail regarding Precinct 1A and La Trobe University.</p>
<p><b>1.8 Design considerations</b></p>			

1.7 Car parking - All vehicular access to the proposed basement car parking on Lot S9 [1B-01 & 1B-02] must be provided via Galileo Gateway.

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5.2 Larundel Urban Village Design and Development Considerations March 2001 (revised August 2001) Continued

<p>The area surrounding the oval, which has access to open space as well as local services and facilities, is a prime location for higher density residential development, which could include various forms of medium density housing, or alternatively student housing, as may occur within the Village Precinct. This development must not restrict view lines from the Mixed-Use Precinct across the oval to the Lancaster Estate. The oval's sports pavilion will need to be integrated with other development occurring in this location.</p>	<p>Refer to Section 4 of this submission for the design response in relation to the future use and development of land adjacent to the oval. The existing, heritage buildings adjacent to the oval are proposed to be retained. This maximises the views of the heritage buildings on the site from the eastern side of the oval and results in a uniform presentation of the heritage buildings from the surrounding residential development to the east. The community centre will serve the same purpose as was envisaged with the sports pavilion which was envisaged to be located adjacent to the oval.</p>
<p>The commercial/mixed-use precinct, while likely to contain a varied range of uses, will become an integrated precinct through the provision of careful design to ensure interface between all different uses is well designed and managed.</p>	<p>Precinct 1B is designed in response to the surrounding land uses and development, including the characteristics of Plenty Road. Refer to Section 4 of this submission for the design response, and further detail regarding the interface of Precinct 1B with the adjacent precincts of the PDP. A variety of land uses are proposed and are integrated through Precinct 1B.</p>
<p><b>1.9 Development and infrastructure contributions</b></p>	
<p>Development and infrastructure contributions are payable but depend on the nature of the proposed development.</p> <p>The developer will be responsible for:</p> <ul style="list-style-type: none"> <li>Off site transport works (road, pedestrian, cycle) generated as a result of the development</li> <li>Off site landscape works on nominated roads, generated as a result of the development</li> <li>Community infrastructure contribution per dwelling/ retail and commercial floor area for the provision of a community facility within the Lancaster Gate development</li> <li>Construction of a sports pavilion to Council satisfaction</li> </ul>	<p>The development contributions will be assessed and determined at the future planning permit application stage of each precinct.</p> <p>Further discussions with Council are required in relation to this item.</p> <p>Refer to Section 6.3 for detail.</p>
<p><b>1.10 Conservation management plan</b></p>	

<p>A Conservation Management Plan has been prepared to guide the detailed design of additional buildings within the Heritage Precinct.</p> <p>Any plans for the development of the Heritage Precinct will require the preparation of a detailed heritage design response addressing the requirements of this conservation management plan.</p>	<p>Consideration has been given to the Conservation Management Plan for the site. In addition to this refer to Appendix D for a detailed heritage assessment of the PDP by Bryce Raworth (2007).</p> <p>Future heritage assessments of any proposed buildings and works will be undertaken, as necessary, to form part of any future planning permit application.</p>
<p><b>1.11 Tree preservation</b></p>	
<p>A significant tree survey must be prepared and endorsed by Council.</p> <p>This survey must show all significant trees and an appropriate design response having regard to the location and ongoing retention of these trees as part of the proposed development.</p>	<p>Tree surveys have been prepared for the site. Refer to Appendix E for details.</p> <p>The survey prepared nominates significant trees. Refer to Section 4.8 for tree removal and Figure 82 tree retention plan.</p>
<p>Where possible significant trees should be retained within open space, road reservations, tree reservations, walk ways and landscape areas.</p>	<p>Refer to the Tree Retention Plan (Figure 82) in Section 4.8 of this submission for details.</p>
<p>Each tree must have an identified tree protection zone. No development will be permitted within the tree protection zone without Council consent.</p>	<p>Refer to Sections 4.8 and 4.9 and Appendix E for details. This item can be satisfactorily addressed as a condition of permit at the planning permit stage of each precinct.</p>
<p>Tree protection barriers will be erected prior to any development occurring on the site. A Council officer will inspect barriers once erected.</p>	<p>This item can be satisfactorily addressed as a condition of permit at the planning permit stage of each precinct.</p>
<p>An Environmental Management Plan (EMP) will need to be submitted and be to the satisfaction of Council for any planning permits issued for the site.</p>	<p>The necessity of preparing an EMP will be determined through the planning permit application process and can be required by a permit condition. Refer to Section 4.10, page 65 for further details.</p>
<p><b>1.12 Traffic/engineering/landscape</b></p>	
<p>As part of Council's consideration of any amendment to the Larundel Development Plan a revised landscape, engineering, traffic and landscape plan must be submitted to Council to form part of any amendment approval.</p>	<p>A revised landscape, engineering, drainage and traffic plan has been submitted as part of this submission to Darebin City Council as part of the proposed PDP.</p>

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## 5.3 Darebin North East Corridor Strategy Plan (August 1998)

### 5.3.1 Overview

The North East Corridor Strategic Plan (August 1998) (referred to hereafter as the 'NECSP') was reviewed during the preparation of the PDP and the objectives have influenced the holistic and integrated design response adopted for the site and incorporated into the PDP.

The design response is a balanced approach, responsive to the characteristics of the site, and will maximise the net community benefit to be achieved from development of the site. Additional buildings on the site that are not nominated for retention on the NECSP are to be retained and incorporated into the Neighbourhood Activity Centre located within Precinct 1B of the site.

The preparation of the NECSP involved owners and prospective purchasers of both the Mont Park and Larundel sites (Amendment L67, Independent Panel Report 2000). The NECSP was prepared in August 1998 and was adopted by Darebin City Council on 7 September 1998.

Since this time, a number of changes to state planning policy has occurred, significantly with the introduction of Melbourne 2030 Planning for Sustainable Growth into the State Planning Policy Framework of the Victorian Planning Provisions. The Lancaster site is now identified as a Neighbourhood Activity Centre, under which consolidation, high residential densities and a mix of uses is encouraged.

The NECSP (1998) is not an Incorporated Document pursuant to Clause 81 of the Darebin Planning Scheme. As a reference document (pursuant to Clause 43.04 of the scheme), its purpose is to provide background information and guidance to strategic planning outcomes and, it is not intended to be prescriptive.

Whilst the PDP has been informed and guided by the NECSP. However, certain elements of the proposal (particularly in relation to building heights and front setback along Plenty Road) have varied from the guidelines and the Larundel Urban Village Framework Plan. This is to achieve a contemporary, holistic and integrated design approach to the development.

### 5.3.2 Retention of Buildings

The NECSP includes 'precinct' framework plans for Larundel and Mont Park. Buildings plotted on these framework plans are deemed to be of significance and required to be retained. In accordance with the NECSP all the buildings on site affected, by the heritage overlay, are retained and incorporated into the PDP. In addition, a building associated with the former Larundel Hospital land use that are located to the southwest of the oval are also retained and incorporated into the PDP. The two buildings located to the southwest of the oval (one of which is shown in Figure 85) are not affected by an Heritage Overlay pursuant to the Darebin Planning Scheme. However, as they were constructed as part of Everett's Master Plan for the Estate, their retention on site will make a positive contribution to the character of the area and enhance the visual amenity of the site.

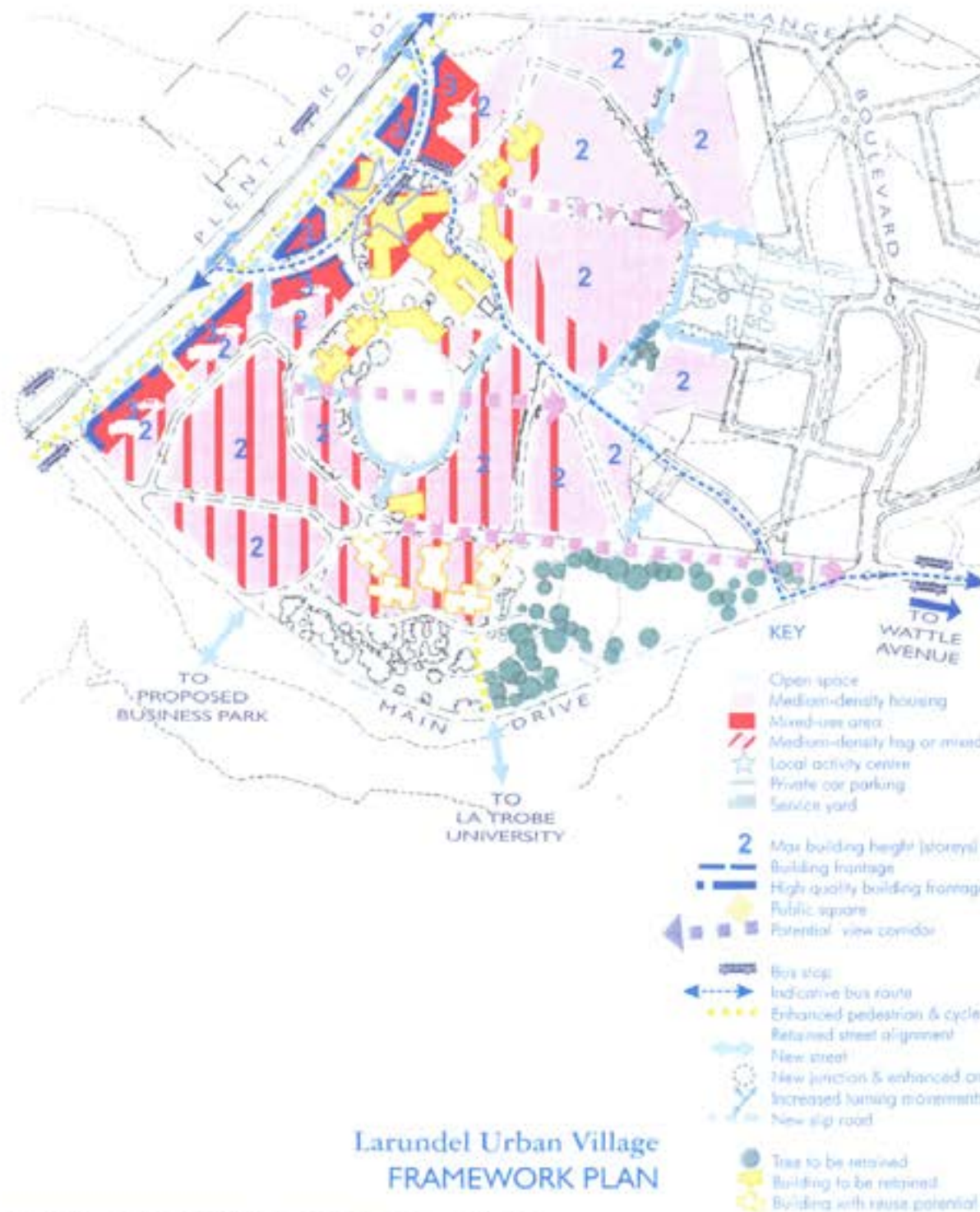


Figure 85 Darebin North East Corridor Strategic Plan (August 1998) (Source: City of Darebin)

## Polaris 3083 Development Plan

### 5.3.3 Larundel Urban Village

Section 7 of the NECSP provides guidance for the 'Larundel Urban Village.' The PDP is consistent with the fundamental land use and development guidelines contained within the NECSP as it:

- Includes a mixed-use development (predominately located next to Plenty Road), medium density housing and a variety of residential land use types and a new activity centre on the site
- Provides for shop-top residential or office accommodation, with a retail use located on the ground floor along Main Street within Precinct 1B
- **Allows opportunity for student housing to be located within the existing buildings within Precinct 2C.**
- Facilitates high quality urban design and architectural integrity with a contemporary character
- Provides for opportunity for non-residential uses to be located within existing heritage buildings (e.g. the community centre use and the possibility of Linaker's Cottage to be used as a child care centre)
- Encourages higher density development that could be located within Precinct 1A and Precinct 2. The height of this development will respect the height and scale of the adjacent development. The height and scale of development will be assessed at the future planning permit stage of each precinct
- Provides for new and existing buildings to overlook public open spaces, including recreational spaces, streets, pedestrian and bicycle paths. Access will be provided to all buildings from the internal street network
- Encourages new buildings to be developed with a high degree of energy efficiency in accordance with current legislative requirements
- Nominates for Precinct 1B, the Polaris Neighbourhood Activity Centre, to be the first stage developed on the site in accordance with the Staging Plan located within Section 6.1 of this submission. This is consistent with the objective of the North East Corridor Plan which is for the early stages of development to create an attractive gateway to the site from Plenty Road
- Creates a permeable, legible, safe and attractive street network. The existing roads will be reused to allow the retention of trees alongside the site where possible to connect the new community with the history of the site
- Minimises expansive views of car parking and of the loading bays which serve commercial uses

- Allows for consideration to be given to the retention of exotic trees unless, as specified in the North East Corridor Strategy Plan, these trees place a significant restriction on development
- Retains the oval and the land surrounding the oval will be suitably landscaped and treated to provide an effective recreation space
- Adopts a flexible approach to the future use of the heritage buildings, in order to ensure their long term preservation
- Incorporates adequate stormwater drainage measures and the developer will provide appropriate improvements to the physical infrastructure
- Provides for a new community centre located within an existing heritage building adjacent to the civic plaza, neighbourhood activity centre and oval.

It is noted that Linaker's Cottage is not included in the NECSP. However, the PDP retains and incorporates this existing building.

The NECSP Framework Plan for Lancaster Gate nominates trees with a moderate or high conservation status to be retained and incorporated into developments. The Framework Plan within the NECSP does not identify any trees to be retained on the site.

The mixed-use 'work, shop, live, play' nature of the development necessitates a variety of building heights; from single level shops, to triple storey accommodation.

Buildings should not generally exceed three (3) storeys in height (11.5 metres) above ground level unless greater height is otherwise anticipated in the PDP. Precinct 1A buildings should generally be of two (2) storeys above ground level. In Precinct 2C the building height should generally be of two (2) storeys above ground level so as to provide an appropriate transition to the nearby existing residential areas.

Refer to Section 5.3.4 for further details pertaining to Building Heights.

## Polaris 3083 Development Plan

### 5.3.4 Building Heights

The guidelines of the NECSP relating to building heights advises that buildings should be no higher than three storeys along Plenty Road and two storeys elsewhere as indicated in the Framework Plan (p.31).

The Larundel Urban Village Framework Plan is based on a past design and development vision encompassing a wider area including the Lancaster Gate site and part of the La Trobe University site. This vision, as well as state planning policy has changed over time, including that for Polaris. The NECSP and Larundel Urban Village Framework Plan are meant to provide guidance only and are not intended to apply prescriptive control to future development within this area.

Consideration has been given to the NECSP, the Framework Plan and the Plenty Road Corridor Urban Design Framework and incorporation of DDO17 into the Darebin Planning Scheme in June 2017. Future indicative building heights within Polaris are also informed by, and are responsive to, the physical attributes of the site (such as topography), the proximity of proposed buildings and existing buildings, existing and future building context, roads on site and in adjoining areas (such as Lancaster Gate) and the scale, symmetry and height of existing buildings on site.

Importantly, the building heights of PDP have been informed by the contemporary, holistic and integrated design adopted for the entire site, rather than the application of a piecemeal precinct by precinct development response.

The PDP provides the opportunity for a range of building heights on the site. A conceptual indication of these heights is provided in Figure 86. Although generally anticipated to be between one and three storeys (above ground), there is capacity for greater overall building height at the south-west of the site, along Plenty Road and Main Drive (Buildings 1B-0, 1B-02 and 1B-16). Three storey developments have been constructed in the adjoining Lancaster Gate development site which also varies the building height guidelines identified in the NECSP.

The significant level changes of the topography over the site contribute to this varied building height. Significant view lines of the existing heritage buildings will be protected, and the building heights will integrate with the built form and scale of built form on adjoining land.

The indicative building heights as shown in Figure 86 have been developed with the objective to respect the significance of the existing heritage buildings. These buildings are generally two storey in height, however are equivalent or comparable in height to a modern three or four storey building due to their high ceiling to floor dimensions. Steep pitched roofs, raised ground floor level above natural ground level and architectural symmetry and scale.

Generally, no building will exceed three storeys in height above ground level. Where it is necessary to exceed a building height of three storeys for example, due to the slope of the land, the proposal must be accompanied by appropriate urbandesign support, to the satisfaction of the responsible authority.

Generally, no building will exceed 11.5 metres in height as measured from natural ground level to the satisfaction of the responsible authority. Where the building incorporates a pitched roof, the roof may exceed this height provided it does not contain any habitable space. Where it is necessary to exceed a building height of 11.5 metres in overall floor height for example, due to the slope of the land or the development of a focal point or gateway location, the proposal must be accompanied by appropriate urban design support to the satisfaction of the responsible authority.

Precinct 1A buildings should generally be of two (2) storeys about ground level. In Precinct 2C the building height should generally be two (2) storeys aboveground level so as to provide an appropriate transition to the nearby existing residential areas.

There is an opportunity for building height of up to eight (8) to nine (9) storeys for Building 1B-01 and five (5) to six (6) storeys for Building 1B-02. Both buildings are located in areas of low topography. They both front Plenty Road, which has an indicative 6-storey upper height proximate to the Polaris Estate site, under the Plenty Road Corridor Urban Design Framework. A 6-storey height also applies to Lot 2D under the Village Precinct Development Plan. Building 1B-01 and Building 1B-02 are also identified as potential landmarks and focal points for the Polaris Town Centre, creating a sense of arrival and place for the activity centre.

**There is also opportunity for greater building height, up to five (5) storeys for Building 1B-16 having regard to local context considerations and in response to the future 7-10 storey, podium/tower built form character anticipated at the northern edge of LaTrobe University under the MCMP.**

Moreover, the built form context of the land to the south of Main Drive is evolving with the Plenty Road Corridor Urban Design Framework identifying the existing service station and townhouses as a future mixed use - commercial area and La Trobe University proposing its own town centre development.

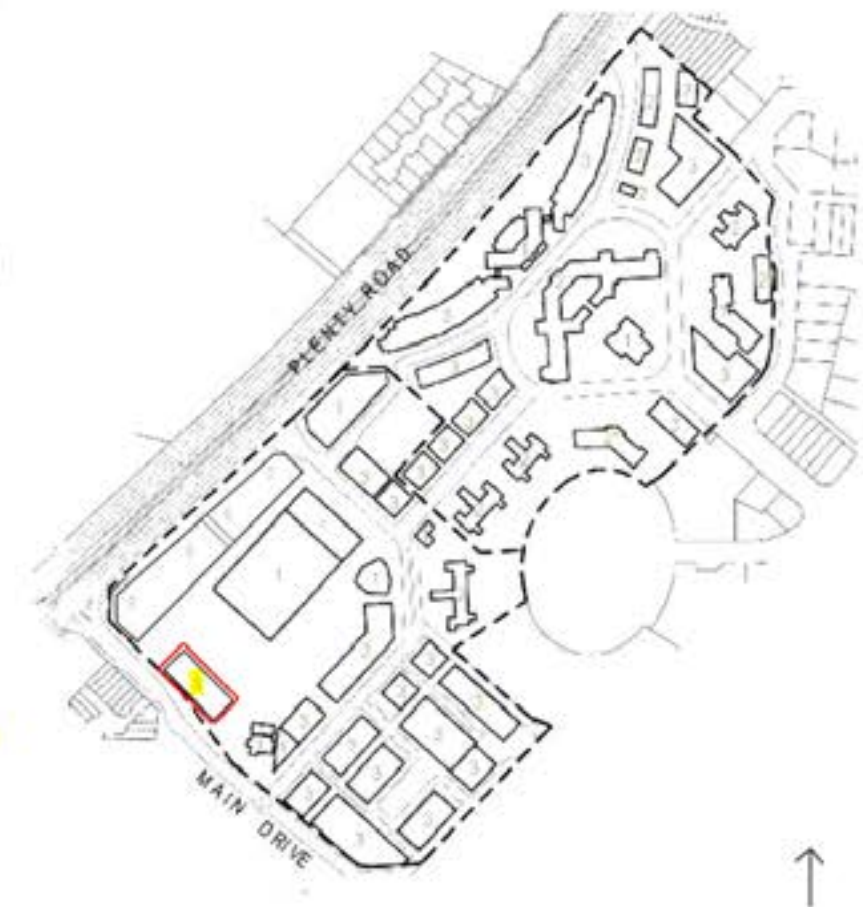


Figure 86 Indicative Conceptual Building Heights above ground (ie storeys)

Note (\*)

Building 2C-04: Maximum two storey building height above ground is permissible, provided the second storey is located within the roof space of the existing building.

## Polaris 3083 Development Plan

### 5.3.5 Front Setback from Plenty Road

Future buildings will be designed generally in accordance with the Urban Design Guidelines contained within Section 4.7 of this submission. The building heights shown in Figure 86 are indicative only and are subject to future detailed design and development which will be at planning permit stage, subject to the Responsible Authority's approval.

The NESCP (1998) does not nominate a specific front building setback to Plenty Road. However, the PDP is generally in accordance with the urban design guidelines of the NESCP (1998) as a high quality building frontage is proposed along Plenty Road, as varied front setbacks are proposed for all buildings fronting Plenty Road.

Ground level front setbacks from Plenty Road for buildings located within Precinct 1B vary between 0 metres and approximately 5.5 metres.

Front setbacks from Plenty Road for buildings located within Precinct 2D vary between a minimum of 2.5 metres to a maximum of 21.4 metres.

These front setbacks are indicative at this stage and are subject to further detailed design which will be included and assessed as part of future planning permit applications.

### 5.4 The Lancaster Gate Development Plan (2001, amended 2007)

It is the intention of the PDP to amend and replace parts of the Lancaster Gate Development Plan (1998) (LGDP) specific to the Polaris site. As such, development and design elements relating to Precinct 1B (formerly Mixed Use Precinct under the Lancaster Gate Development Plan), in relation to front setbacks from Plenty Road have been considered in context to the current holistic and integrated design approach of the PDP.

Under the LGDP it is nominated that buildings should be setback at least 10 metres from Plenty Road (p53). No specific front setback is nominated for buildings located within (proposed) Precinct 2D under the LGDP (formerly the Village Precinct under the LGDP).

The functionality of buildings located within Precinct 1B (ie the Polaris Neighbourhood Activity Centre), the layout and configuration of buildings and roads (both within the Precinct and across the entire estate), and the context of the precinct as part of the whole of the Lancaster development site significantly influences the front setback of buildings along Plenty Road.

In order to achieve a good design outcome and to promote the precinct's status as a Neighbourhood Activity Centre, a ground level front setback of between 0 and approximately 5.5 metres from Plenty Road is proposed. Refer to Figure 49b on page 40. A reduced front setback at the 'corner' locations within Precinct 1B creates a sense of place and a sense of arrival to the new Main Street of the Polaris Neighbourhood Activity Centre.

The very wide Plenty Road reserve fronting Precinct 1B demands some form of containment, as it is the 'front' of the Polaris Neighbourhood Activity Centre.

The Plenty Road setback frontage (along Precincts 1B and 2D) creates a setback rhythm of buildings coming forward to mark urban corners and buildings setback in between to further accentuate these landmarks. This setback response will ensure better activation of the street, promote passive surveillance, increase commercial opportunities for the Neighbourhood Activity Centre and ensure a more efficient use of land (ie a more sustainable outcome rather than the creation of a 'no man's land'), and a better and vibrant urban village character appropriate to this mixed use precinct.

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6.0 Implementation



# Polaris 3083 Development Plan

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## 6.1 Staging

It is anticipated that the site will be developed over approximately six (6) years from late 2007 and that development will commence in 2008. The site will be developed in a number of stages, as indicated in Figure 87.

The Staging Plan as depicted in Figure 87 is indicative only and is envisaged to commence with the Polaris Neighbourhood Activity Centre followed by the development of Precinct 2D in 2009. The development of the accommodation land use and development located within Precinct 1A and Precinct 2C will follow.

The staging of development may vary over time depending on market conditions.

The implementation of the sequence as shown in Figure 87 will aim to:



- Ensure that the essential services for a new residential community are provided
- Provide a diversity of housing styles to maximise housing choice
- Create sustainable and exceptional community environments
- Create acceptable living environments for the new residents.

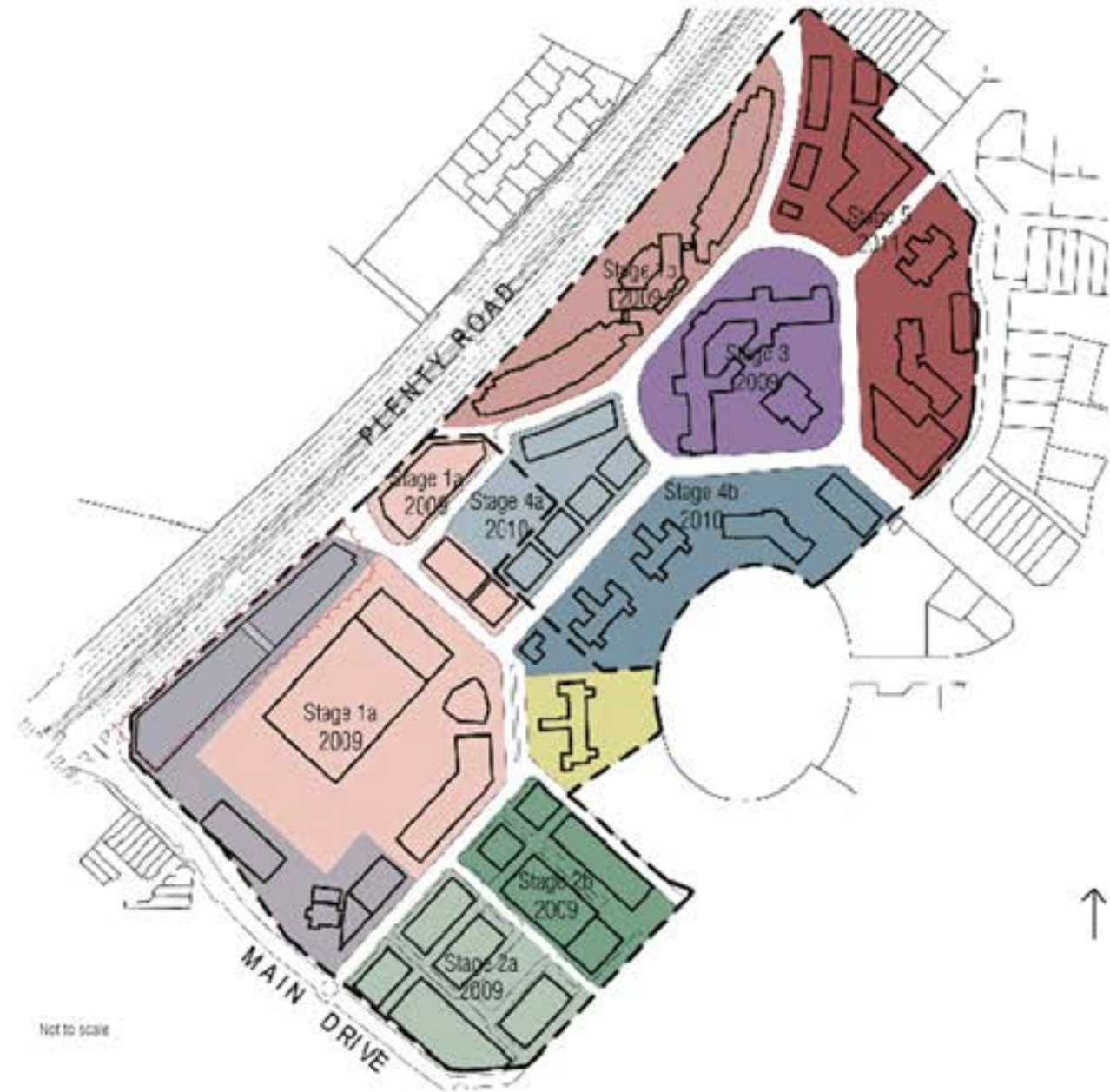
There are various methods that may be used to create an attractive presentation on the site during construction activities. As the development will progress in stages, there will be areas of the site where no construction is occurring. These areas will be bounded from construction activities by the use of temporary fencing, bunting or hoarding as appropriate. The grounds which are currently grassed and treed will be maintained in this condition during construction activities. Grass areas will be mowed and litter collected on a regular basis.

The use of temporary landscaping may be appropriate in certain areas to ensure an attractive site presentation to surrounding and adjoining areas. This will most likely be used following completion of early stages of the infrastructure development and prior to commencement of subsequent stage.

Contractors working on the site will also be expected to implement a Site Management Plan that covers includes mitigation of such as contamination of dust from construction, delivery times, parking location etc, to the satisfaction of the responsible authority.

LEGEND

	Stage 1a
	Stage 1b
	Stage 2a
	Stage 2b
	Stage 3
	Stage 4a
	Stage 4b
	Stage 5
	Stage 6
	Stage 7



Not to scale

Figure 87 Indicative Staging Plan for Polaris (Source: HASSELL, 2008)



## Polaris 3083 Development Plan

### 6.2 Yield

It is envisaged that the future land use and development of the site will provide the following dwelling yields. These yields are indicative only and are subject to change. The final yield will be determined following detailed design of each building, and will be finalised at the planning permit stage of the development.

#### Precinct 1A - Accommodation

Approximately 69 dwellings, comprising both 2 and 3 storey townhouses with a variety of dwelling types (i.e. a variety of a number of bedrooms) is proposed within this precinct.

#### Precinct 1B - Accommodation

The following provides an approximate break down of potential dwellings envisaged within Precinct 1B:

Home Office Apartments 19

Shop-top apartments 20

**Dwellings within a residential building 200**

The home office apartments and shop-top apartments will be located adjacent to Main Street.

#### Precinct 2C - Accommodation

Student Accommodation: 125 beds

Dwellings/Apartments: A total of approximately 191 dwellings are to be provided within Precinct 2. This total will include a variety of dwelling types, including, 1, 2 and 3 bedroom dwellings.

A break down of the envisaged number of dwelling types is provided in the table below:

1 bedroom	56
2 bedroom	95
3 bedroom	40

Table 6.1.1a: Indicative Dwelling Type breakdown for Precinct 2C (Source: HASSELL, 2008)

#### Precinct 2D - Accommodation

Precinct 2D is comprised of a variety of room types within the future retirement village to be developed within the precinct.

	Studio	1 Bed	2 Bed	Total
GND	9	24	5	38
L1	10	27	6	43
L2	10	28	6	44
Managers	-	1	1	2
<b>TOTAL UNITS</b>	<b>29</b>	<b>80</b>	<b>18</b>	<b>127</b>
<b>TOTAL BEDS</b>	<b>29</b>	<b>80</b>	<b>36</b>	<b>145</b>

Table 6.1.1b: Indicative Room Type breakdown for Precinct 2D (Source: Gracious Living/ Petrac, 2007)

## Polaris 3083 Development Plan

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### 6.3 Development Contribution & Open Space Contribution

The Darebin Development Contribution Plan relates to all development types, however, non-residential developments are exempt from Community and Parkland Infrastructure charges.

The PDP is located within Charge Area 2331009 under Schedule 1 to the Development Contributions Plan Overlay.

Development contributions will be paid in accordance with the Development Contributions Plan Overlay.

### 6.4 Future Planning Approvals within Polaris

The process for development of Polaris has commenced with the submission of the PDP to Darebin City Council. The next steps in the approval process are envisaged to be:

- The layout of all proposed buildings will be finalised during detailed design and development of each future precinct and will be approved at the planning permit application stage for each precinct
- An assessment of the structural stability of the existing heritage buildings on the site will be undertaken during the detailed design and development of each future precinct at the planning permit stage for each precinct
- Indicative uses are nominated in the PDP (Section 4). The final uses for each precinct and stage will be finalised at the planning permit application stage.
- The location of all passive and open space (i.e. public realm areas) will be finalised during the detailed design of each future precinct at the planning permit stage for these precincts. An indicative location of the areas of passive and open space is shown in Figure 80
- Specific design detail pertaining to vegetation landscaping can be provided at the future planning permit stage for relevant precincts. The Polaris Conceptual Landscape Plan (Figure 80) of this submission provides an indicative layout for the future landscaping for each precinct
- Detailed drainage plans for each future precinct will be provided at planning permit stage for relevant precincts. An indicative drainage plan is included in Section 4.12 (Figure 85)
- Confirmed details of the future detailed subdivision of each precinct will be provided at the subdivision permit stages of each future precinct
- Specific details for the provision of infrastructure of each precinct will be provided at planning permit stage for each future precinct. This will include consideration of adjoining precincts
- Street design and treatment details can be provided via secondary conditional approval of the PDP. Specific details can be provided at the planning permit stage for each future precinct
- Detail relating to construction management, site management and environmental management will be provided at the planning permit stage for each future precinct, and may be required as conditions on a planning permit
- Specific details regarding traffic management works, drainage, public art, any works-in-kind, financial contribution to the renovations of the community centre building and public open space contribution (including improvements to the oval) will be provided at the planning permit stage for each relevant future precinct
- Specific details regarding a public art plan can be provided as a secondary conditional approval of the PDP
- A survey and detailed assessment of vegetation will be provided at the planning permit stage for each precinct, prior to the detailed design of each precinct being undertaken.
- **Plenty Road interface design and treatment details to be provided at the planning permit stage for Building 1B-01 & 1B-02. Indicative building envelopes are provided in Section 4.2.2 (Figure 57a).**

### 6.5 Reference Documents

The preparation of this Development Plan has been informed by key reference documents.

The preparation of a permit application for approval by Darebin City Council and the actual development of the land should be generally in accordance with any relevant recommendations covered in the following reference documents:

- Integrated Transport Plan, prepared by Ratio Consultants (July 2009)
- Heritage Issues, Report to Council, prepared by Bryce Raworth (June 2007)
- Lancaster Precinct Arboricultural Report, prepared by Stephen Fitzgerald Arboriculture (June 2007)
- Consolidated Retail & Commercial Economic Assessment, prepared by Charter Keck Cramer, (July 2008)
- Engineering Service Report, prepared by O'Neill Group Consulting Engineers (31 July 2009)
- ESD Statement, prepared by Cundall (3 July 2007)

These reference documents are contained in the Appendices of this Development Plan.

**\* These documents are intended to act as 'base' reference documents and are to be used as a guide only for the future development within the Polaris Village Precinct.**

#### \*6.4 Future Planning Approvals within Polaris continued...

Lot S3 (1B-16): As per agreement with Council, Tree 199 is to be removed. A tree amenity off-set payment is to be paid to Council (using the City of Melbourne method) prior to first occupation of an approved building.

Replacement trees are to be planted to off-set removal of Tree 199. A landscape plan showing the proposed location of the trees inclusive of complementary vegetation is to be prepared in support of any planning permit application lodged with Council. Replacement tree species to be to Council requirements.

**5.4 PLANNING SCHEME AMENDMENT C198DARE - REQUEST FOR INTERIM HERITAGE OVERLAY****Author:** Senior Strategic Planner**Reviewed By:** Acting General Manager City Sustainability and Strategy

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**EXECUTIVE SUMMARY**

This report seeks endorsement from Council to submit Planning Scheme Amendment C198dare (C198) to the Minister for Planning for preparation and approval, pursuant to section 20(4) of the *Planning and Environment Act 1987*. C198 proposes an interim heritage overlay to two properties within the Thornbury Park Estate precinct.

Council resolved on 26 April 2021 to prepare and exhibit a planning scheme amendment to introduce permanent heritage controls (C191) within the proposed heritage precinct, 'Thornbury Park Estate'. At the same time, Council resolved to seek an interim heritage overlay (C197), which would provide protection for the precinct while the permanent controls were introduced.

Council received authorisation from the Minister for Planning, on 11 June 2021 to prepare and exhibit amendment C191 (permanent heritage overlay), however the request for the interim heritage overlay (C197) for Thornbury Park Estate is still being considered by the Minister. This means that there are no heritage protections in the planning scheme for the properties within the precinct.

In recent weeks, Council's building department has received two new applications for demolition of contributory buildings under Section 29A of the *Building Act 1993*, at 27 Keon Street (full demolition) and 39 Keon Street Thornbury (partial demolition).

Under Section 29B of the *Building Act 1993*, suspension of a demolition permit can only occur if a planning scheme amendment is requested within the prescribed time. Even though Council has already requested an interim heritage overlay for this precinct, it has not been approved in time to suspend these two applications. Officers recommend these applications are suspended to prevent demolition permits from being issued without due consideration of heritage impacts.

Until Amendment C197 (interim heritage overlay) is decided by the Planning Minister, Council officers propose to continue to seek the application of interim heritage overlays for individual sites (identified as significant or contributory), at the time a new demolition application is sought. This approach is necessary to protect the identified heritage values until an interim heritage precinct overlay is approved and reflects current practice in managing heritage amendments. This report includes a recommendation that authorises the Chief Executive Officer to continue to make these amendment requests for this heritage area in the meantime.



assessed by Council having regard to the purpose and decision guidelines of the Planning Policy Framework and Heritage Overlay. Planning permit exemptions exist for minor works not deemed to impact on the heritage values, this is outlined in the *City of Darebin Heritage study Incorporated Plan – Permit exemptions (2011, amended 2021)*.

On 11 June 2021, Council received authorisation to commence the preparation of C191. The authorisation was granted subject to several minor conditions which seek to improve the assessment process for heritage places within the proposed heritage precinct:

Council officers are currently addressing the conditions of authorisation and preparing for public exhibition of C191.

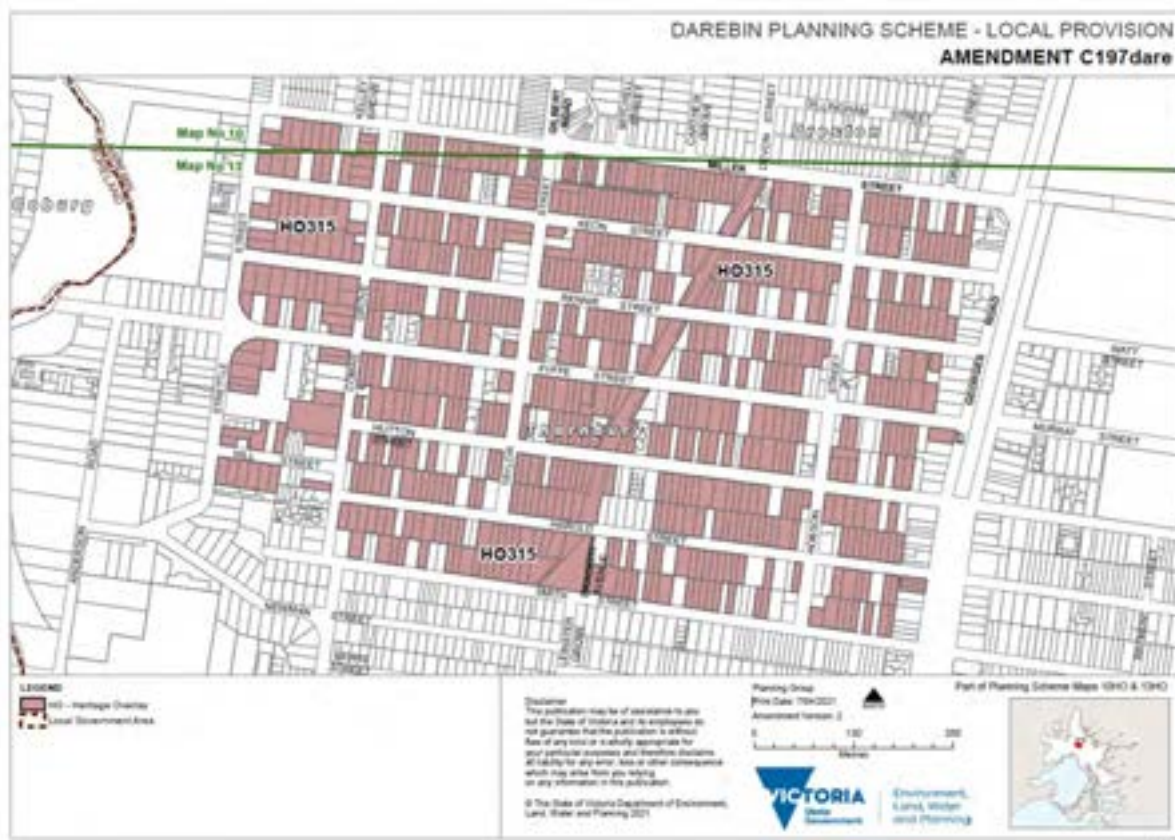
**Amendment C197 – interim heritage overlay**

At the time of submitting Amendment C191, Council sought Ministerial intervention to apply an interim heritage overlay (Amendment C197).

Amendment C197 seeks to apply an interim heritage overlay over those properties identified as: significant, contributory and excludes non-contributory properties and those properties with demolition permits (see map below) until April 2022.

The interim heritage overlay seeks to protect sites within the precinct while the permanent heritage overlay (Amendment C191) is being considered. The application of interim heritage overlays while more permanent heritage overlays are implemented reflects current best practice in managing local heritage.

Council is still currently awaiting Ministerial approval of the interim heritage controls (Amendment C197), which is why your further resolution is sought for these demolition applications.



**Applications under s29A of the Building Act 1993**

An application for demolition is made under Section 29A of the *Building Act 1993*. This application requires referral to the local authority and advice is sought as to whether a planning permit is required before the issuing of a demolition permit. The Responsible Authority is required to provide a response within the prescribed time of 15 days.

Since Council's resolution of the 26 April 2021 in relation to Amendment C191 and C197, Council has received two applications for demolition within Thornbury Park Estate. The two sites, 27 Keon Street Thornbury and 39 Keon St Thornbury are both identified as "Contributory" to the Thornbury Park Estate precinct.

Council may suspend an application for demolition under Section 29B of the *Building Act 1993*, where an application is made to the Minister for Planning to prepare an amendment to a planning scheme affecting the land.

In line with Council's resolution of the 26 April 2021, and in the absence of approval of interim controls via C197, officers have tentatively lodged a planning scheme amendment (C198dare) applying an interim heritage overlay to 27 Keon and 39 Keon Street Thornbury, to suspend applications temporarily. If this report is approved, Council will be able to formally suspend the demolition permit applications until a decision is made by the Minister for Planning.

Affected landowners/applicants have been notified of the temporary suspension, and that they can appeal the decision under Section 29B to the Building Approval Board (BAB). If landowners/applicants are not satisfied with the BAB decision, a further opportunity for appeal is available at the Victorian Civil Administrative Tribunal (VCAT).

**Previous Council Resolution**

At its meeting held on 26 April 2021, Council resolved:

*That Council*

1. *Requests the Minister for Planning authorise the preparation and exhibition of Amendment C191dare to the Darebin Planning Scheme, pursuant to section 8A of the Planning and Environment Act 1987.*
2. *When authorised by the Minister for Planning, exhibit Amendment C191dare to the Darebin Planning Scheme in accordance with notice requirements under section 19 of the Planning and Environment Act 1987.*
3. *Seeks community and stakeholder feedback as part of the exhibition period, on the proposed changes to the planning scheme to implement the Thornbury Park Estate Precinct heritage study.*
4. *Request the Minister for Planning prepare and approve Amendment C197dare to apply an interim heritage overlay to the Darebin Planning Scheme, pursuant to section 20(4) of the Planning and Environment Act 1987.*
5. *Authorises the Manager City Futures to make minor alterations and corrections, where necessary, to Amendment C191dare and Amendment C197dare material as attached.*

## COMMUNICATIONS AND ENGAGEMENT

### Consultation

This proposed amendment seeks to respond to an urgent need to protect sites with identified heritage value and to ensure that heritage considerations form part of the decision making. It has not been through a consultation process given the time limitations and the forthcoming consultation for the permanent heritage controls.

More detailed engagement is proposed for the permanent heritage controls of Amendment C197 and Council has previously been advised of this engagement process.

The following units/organisations have been consulted in developing this report:

- DELWP;
- Building Unit;
- Statutory Planning; and
- Council Business.

### Communications

Further notice will be provided during engagement for the permanent heritage controls and if approval is given by the Minister for Planning for the interim heritage overlay. A communications plan has been prepared to support community queries on this matter.

More detailed engagement is proposed for the permanent heritage controls of Amendment C197 and Council has previously been advised of this engagement process.

## ANALYSIS

### Alignment to Council Plan / Council policy

#### Strategic Direction 2 - Prosperous, Liveable and Flourishing

We will ensure development, and the built environment, is designed for our liveability and sustainability

#### Environmental Sustainability Considerations

The proposed planning scheme amendment seeks to protect heritage places and supports the retention and conservation of heritage elements.

#### Climate Emergency

The retention of heritage fabric supports the reduction of emissions by supporting reuse and adaptation of existing building stock. New development will be delivered within Council's environment and climate policy framework, including ESD Building Policy 2018 which sets out ESD standards for different types of building projects.

#### Equity, Inclusion and Wellbeing Considerations

The heritage protection of a precinct of historic significance within the City of Darebin will provide net benefit to the community.

### **Cultural Considerations**

The amendment is expected to have a positive social impact by providing protection for buildings identified as being of heritage significance. The recognition of heritage streetscapes and precincts will contribute to an understanding of Melbourne's social and architectural history, for present and future generations.

### **Economic Development Considerations**

The application of an interim Heritage Overlay may create additional cost for landowners/developers in circumstances where there were previously no planning permit requirements.

### **Financial and Resource Implications**

There are no budget requirements for this proposal. The preparation of the amendment has been accommodated within existing project budget.

### **Legal and Risk Implications**

Council is obligated to conserve places of cultural heritage value and not applying appropriate controls places a risk to the significance of the heritage precinct.

The owners of 27 and 39 Keon Street have been informed of the suspension of their demolition application. There is a risk that these owners, and any future applicants for demolition within the precinct, are not satisfied with the process/outcome. Council officers are currently forming a communications approach across the Strategic Planning, Statutory Planning and Building teams to ensure there is clear and consistent communication regarding these matters.

### **Operational Impacts**

The proposed application of an interim Heritage Overlay may increase the number of planning permit applications because additional permit triggers will exist.

## **DISCUSSION**

### Applying the Interim Heritage Overlay

Under the *Planning and Environment Act 1987*, local government has a responsibility to protect local cultural heritage. These protections are generally in the form of a heritage overlay in the planning scheme. Interim Heritage Overlays seek to protect sites with identified heritage values while more permanent controls are being considered.

Development pressure in the Thornbury Park Estate puts the precinct at risk of being eroded over time. Timely action, including the application of interim heritage overlays on a site-specific basis, is required to ensure that heritage is considered when assessing applications within the proposed heritage precinct.

The proposed interim Heritage Overlay for 27 Keon Street and 39 Keon Street Thornbury assists in preventing loss of heritage values in Thornbury Park Estate by suspending the demolition permits and requiring heritage consideration for new planning applications.



Further demolition applications until Amendment C197 (Interim Heritage Overlay) is approved

The timing of the Minister's consideration of Amendment C197 is unknown. Council may receive additional demolition applications until it is approved.

To enable officers to respond to these requests in a timely manner, this report recommends that Council authorise the Chief Executive Officer to lodge requests for planning scheme amendments for properties within the precinct. This would no longer be necessary if the interim precinct heritage overlay is approved by the Minister, which Council has already requested through Amendment C197.

This authority is considered to be aligned with Council's established position to seek to apply the Heritage Overlay to the precinct. It will enable any subsequent amendments, if required, to be undertaken at an operational level as it is enacting the resolution already made by Council in seeking interim heritage controls for the area through Amendment C197.

## **OPTIONS FOR CONSIDERATION**

### **Option 1 – (Recommended).**

Request that the Minister for Planning prepare and approve amendment C198, which seeks to apply interim heritage overlay to two properties (27 Keon Street and 39 Keon Street Thornbury).

This will enable Council to suspend the current demolition applications until a decision by the Minister is made.

Provide authority to the Chief Executive Officer, in response to any demolition applications received for significant or contributory buildings, to continue to lodge requests for preparation and approval of planning scheme amendments to properties within Thornbury Park Estate. This authority would apply until the time the Minister approves Amendment C197 (Interim Heritage Overlay) if that is his decision. Council would be notified of any instances of this authority being acted upon.

### **Option 2**

Process Section 29A Demolition applications for 27 Keon Street Thornbury and 39 Keon Street Thornbury without requesting Ministerial intervention for an interim heritage overlay.

This will mean the properties can be demolished either full or in part, which will impact upon the heritage value of the precinct. This would have the potential to erode the significance of the precinct at large during the time Council is progressing permanent controls.

## **IMPLEMENTATION STRATEGY**

### **Communication**

The applicants have been notified that their application for demolition has been suspended as a planning scheme amendment to apply an interim heritage overlay has been made to the Minister for Planning.

Applicants will be notified of the Minister for Planning's decision on whether an interim heritage overlay is granted. Internal liaison between the building department, statutory planning and strategic planning is in place to ensure consistent messaging and communication from council is given to affected property owners.

## Timeline

Next steps are expected to occur as follows:

- June 2021 – Council receives Ministerial approval to apply an interim heritage overlay to Thornbury Park Estate.
- August-September 2021 – Public exhibition of Amendment C191 (Permanent heritage overlay) for 6 weeks.
- October 2021 – Council officers and heritage consultants review submissions received to proposed planning scheme amendment.
- November 2021 – Council either adopts the amendment (with or without changes) and submits for Minister’s approval or refers unresolved submissions to a Planning Panel.
- December 2021 – Planning Panel.
- February 2022 – Council adopts amendment (with or without changes) and submits for Minister’s approval (timing subject receiving Planning Panel report).

## RELATED DOCUMENTS

- Applying the Heritage Overlay – Planning Practice Note 1 (August 2018)
- Ministerial Direction – The Form and Content of Planning Schemes
- Ministerial Direction No. 9 – Metropolitan Planning Strategy
- Ministerial Direction No. 11 – Strategic Assessment of Amendments
- Ministerial Direction No. 15 – The planning scheme amendment process
- Plan Melbourne 2017-2050
- *Planning and Environment Act 1987*
- *Building Act 1993*
- Guidelines for Preparing Planning Scheme Amendment Documentation – September 2014

## Attachments

- Darebin C198dare - Map Ministerial s20(4) (**Appendix A**) [↓](#)
- Darebin C198dare - Explanatory Report (**Appendix B**) [↓](#)
- Darebin C198dare - Instruction Sheet (**Appendix C**) [↓](#)

## DISCLOSURE OF INTEREST

Section 130 of the *Local Government Act 2020* requires members of Council staff and persons engaged under contract to provide advice to Council to disclose any general or material interest in a matter to which the advice relates.

The Officer reviewing this report, having made enquiries with relevant members of staff, reports that no disclosable interests have been raised in relation to this report.



*Planning and Environment Act 1987*

## **DAREBIN PLANNING SCHEME**

### **AMENDMENT C198 DARE**

#### **EXPLANATORY REPORT**

##### **Who is the planning authority?**

This amendment has been prepared by the Minister for Planning, who is the planning authority for this amendment.

The Amendment has been made at the request of Darebin City Council.

##### **Land affected by the Amendment**

The amendment applies to the contributory properties within the Thornbury Park Estate with current S29A applications.

The Amendment applies to 27 Keon Street and 39 Keon Street Thornbury



Map 1 – Darebin C198dare (Map 13HO\_Ministerial 20(4) )

**What the amendment does**

The Amendment applies the Heritage Overlay (HO315) to the subject properties on an interim basis until 16 June 2022.

Specifically, the Amendment makes the following interim changes to the Darebin Planning Scheme:

1. Amends the Schedule to Clause 43.01 (Heritage Overlay) and Planning Scheme Map 13HO to apply an Interim Heritage Overlay (HO315) to 27 and 39 Keon Street Thornbury.

**Strategic assessment of the Amendment****Why is the Amendment required?**

The amendment is required to place interim heritage controls over the proposed contributory heritage places (27 and 39 Keon St Thornbury) within the Thornbury Park Estate that are subject to current S29A applications, while an amendment for permanent heritage controls (Amendment C191dare) is exhibited and assessed and before the interim request for contributory and significant properties within the proposed precinct (C197dare) is approved. This will ensure protection of the heritage values of the individual places until a final decision is made by the Minister on permanent controls.

These properties are subject to development pressure in the form of S29A demolition applications being lodged for properties in the precinct, and development approved in recent years that has degraded the intactness of the precinct. Concern for loss of integrity and intactness of the proposed heritage precinct during the time it takes to progress the permanent heritage overlay through Amendment C191dare has prompted this request for the Interim Heritage Overlay. Given the imminent threat of demolition and resultant degrading of the heritage significance of the Thornbury Park Estate Heritage Precinct, intervention by the Minister for Planning is considered necessary and justified.

The interim protection of properties within a precinct of historic significance will provide net benefit to the community.

**How does the Amendment implement the objectives of planning in Victoria?**

The amendment implements the objectives of planning in Victoria, under Section 4 of the *Planning and Environment Act 1987*, in particular:

- *To conserve and enhance those buildings, areas or other places which are of scientific, aesthetic, architectural or historical interest, or otherwise of special cultural value.*
- *To balance the present and future interests of all Victorians.*
- *To facilitate development in accordance with the objectives outlined above.*

These objectives are implemented through the identification and interim protection of heritage built form while an amendment for permanent protection is in train.

**How does the Amendment address any environmental, social and economic effects?**

The amendment is expected to have a positive environmental impact by providing interim protection of places of historic significance and thereby supporting the reuse of existing building stock when permanent controls come into effect.

The amendment will have positive social effects by helping to safeguard benefits to the community (while the amendment for permanent controls is in train) by protecting fabric that adds to the understanding of cultural history, providing a link to the past and giving a sense of place.

The Heritage Overlay does not prohibit development, but instead requires the significance of a place to be considered when assessing applications. These impacts are offset by the benefit to the community provided through the protection of heritage places over many generations. Further, planning permit exemptions for minor works triggered by the Heritage Overlay are included and updated in the Incorporated Plan.

**Does the Amendment address relevant bushfire risk?**

The municipal area of Darebin does not have any designated bushfire prone areas.

**Does the Amendment comply with the requirements of any Minister's Direction applicable to the amendment?**

The amendment is consistent with the Ministerial Direction on the Form and Content of Planning Schemes under Section 7(5) of the Act.

The amendment is consistent with Ministerial Direction No.9 – Metropolitan Strategy pursuant to Section 12 of the Act, that requires planning authorities to have regard to the Metropolitan Strategy (Plan Melbourne 2017-2050).

The amendment is consistent with Direction 4.4 of Plan Melbourne: Respect Melbourne’s heritage as we build for the future. The amendment is consistent with this direction as it proposes to make minor changes to enable the Planning Scheme to continue to guide appropriate development in the municipality, and that the built heritage of the municipality is maintained.

The amendment is consistent with Ministerial Direction No.11 – Strategic Assessment of Amendments, as the requirements of this direction have been followed in the preparation of this amendment.

**How does the Amendment support or implement the Planning Policy Framework and any adopted State policy?**

The amendment supports the Planning Policy Framework (PPF), in particular Clause 15.03-1S (Heritage Conservation) by providing interim heritage protection for identified heritage places to minimise the loss of local heritage whilst more permanent heritage protection is considered.

The amendment assists in implementing the strategies of 15.03-1S for heritage conservation, through the application of an interim heritage overlay which implements the Thornbury Park Estate Precinct heritage study. The study identifies, assesses and recommends protection of heritage places significant or contributory to the Thornbury Park Estate heritage precinct.

**How does the Amendment support or implement the Local Planning Policy Framework, and specifically the Municipal Strategic Statement?**

The amendment supports the Planning Policy Framework (PPF), in particular Clause 21.02-4 (Heritage) of the Darebin Planning Scheme. The amendment supports and is consistent with the local planning policy framework by seeking to protect, conserve and enhance places noted as being of local significance applying an interim heritage overlay. The amendment also seeks to discourage demolition or relocation of locally significant heritage buildings.

The amendment implements state and local planning policy as it seeks to protect identified heritage places on an interim basis whilst permanent heritage controls are being assessed.

The amendment will assist in conserving Darebin’s built heritage while not significantly impacting upon the broader housing development objectives of the municipality as only those places identified as being of local heritage significance (significant and contributory to the precinct) are affected by the amendment.

**How does the amendment support or implement the Municipal Planning Strategy?**

The Municipal Planning Strategy is yet to be introduced into the Darebin Planning Scheme, and therefore the Municipal Strategic Statement and local policy applies (as addressed above).

**Does the Amendment make proper use of the Victoria Planning Provisions?**

The amendment makes proper use of the Victoria Planning Provisions by utilising the Heritage Overlay to protect places of local heritage significance; an approach consistent with *Planning Practice Note 1 Applying the Heritage Overlay* and the Ministerial Direction - The Form and Content of Planning Schemes.

**How does the Amendment address the views of any relevant agency?**

The views of the Department of Environment, Land, Water and Planning have been sought in the preparation of this amendment.

**Does the Amendment address relevant requirements of the Transport Integration Act 2010?**

The amendment is not likely to have a significant impact on the transport system.

**Resource and administrative costs****What impact will the new planning provisions have on the resource and administrative costs of the responsible authority?**

The amendment will increase the number of sites (2) subject to the provisions of the Heritage Overlay. The anticipated increase in planning applications is not expected to have a significant impact on resourcing and administrative costs.

**Where you may inspect this Amendment**

The Amendment is available for public inspection, free of charge, during office hours at the following place:

City of Darebin (Planning Counter), 274 Gower Street, Preston and website at [www.darebin.vic.gov.au/haveyoursay](http://www.darebin.vic.gov.au/haveyoursay)

The Amendment can also be inspected free of charge at the Department of Environment, Land, Water and Planning website at [www.planning.vic.gov.au/public-inspection](http://www.planning.vic.gov.au/public-inspection).



*Planning and Environment Act 1987***DAREBIN PLANNING SCHEME****AMENDMENT C198****INSTRUCTION SHEET**

The planning authority for this amendment is the Minister for Planning.

The Darebin Planning Scheme is amended as follows:

**Planning Scheme Maps*****Overlay Maps***

1. Amend Planning Scheme Map No 13HO to incorporate properties 27 Keon Street and 39 Keon Street Thornbury "Darebin Planning Scheme, Amendment C198".

**Planning Scheme Ordinance**

The Planning Scheme Ordinance is amended as follows:

2. In **Overlays** – Clause 43.01, replace the Schedule with a new Schedule in the form of the attached document.

End of document

## 6. OTHER BUSINESS

### 6.1 GENERAL PLANNING INFORMATION: SCHEDULED VCAT APPLICATIONS

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The General Planning Information attached at **Appendix A** contains lists of:

- Scheduled VCAT appeals for the information of the Planning Committee. The table includes appeals heard as well as those scheduled for the coming months (but does not include mediations and practice day hearings).

<b>Recommendation</b>
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**That** the General Planning Information attached as **Appendix A** be noted.

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#### Related Documents

Nil

#### Attachments

- Scheduled VCAT Applications (**Appendix A**) [↓](#)

### Delegate Decisions before VCAT

OCTOBER 2020					
Date of Hearing	App. No.	Property/Ward	Proposal	Council Decision/Nature of Appeal	VCAT Decision
6/10/2020	D/145/2018	480 Gilbert Road, Preston  Cazaly	S87A Amendment request to VCAT remove roof decks to 3 dwellings	Council taking position of opposition	Council's position of refusal set aside – Permit granted.
<b>Result</b>	In deleting the permit condition requiring the provision of rooftop terraces, the Tribunal disagreed with Council's position that providing private open space within the front setback would not provide adequate privacy and size for the recreation needs of future occupants.				
19/10/2020 (Compulsory Conference)	D/111/2020	439 Gilbert Road, Preston  Cazaly	Proposed medium density development of six (6) double storey dwellings above a basement carpark and a waiver of the car parking requirements	Refusal – Applicant Appeal	Council's decision set aside (by consent) – Permit granted
<b>Result</b>	The parties were able to reach agreement as to a suitable form of development, as such, they were in position that Council's refusal could be set aside by consent.				

PLANNING COMMITTEE MEETING

12 JULY 2021

NOVEMBER 2020					
Date of Hearing	App. No.	Property/Ward	Proposal	Council Decision/Nature of Appeal	VCAT Decision
9/11/2020 (Compulsory Conference)	D/672/2019	61 Edwardes Street, Reservoir  La Trobe	A medium density housing development comprising the construction of 15 triple storey dwellings and one (1) double storey dwelling and a basement level; and Reduction of the car parking requirements	Notice of Decision – Objector Appeal	Council's decision varied (by consent) – Permit granted
<b>Result</b>	This matter was listed for an administrative mention on 15 December 2020 where Council advised the Tribunal of the outcome of the 14 December 2020 Planning Committee meeting.				
11/11/2020 (Compulsory Conference)	D/605/2019	14-16 Storey Road, Reservoir  La Trobe	Proposed construction of 12 double storey dwellings and a waiver of the visitor car space requirements	Refusal – Applicant Appeal	
<b>Result</b>	The matter did not settle.				

PLANNING COMMITTEE MEETING

12 JULY 2021

DECEMBER 2020					
Date of Hearing	App. No.	Property/Ward	Proposal	Council Decision/Nature of Appeal	VCAT Decision
16/12/2020 (Compulsory Conference)	D/668/2019	552-558 Plenty Road, Preston  Cazaly	Use of land for the purpose of a childcare centre (100 places) and food and drink premises; construction of a four-storey building with food and drink premises at Ground Floor, five (5) dwellings and childcare centre at the upper floors; a reduction in the car parking requirement and alteration of access to a road in a Road Zone Category 1	Failure Appeal – Council is yet to formalise a position on the Application.	
<b>Result</b>	The matter did not settle.				

PLANNING COMMITTEE MEETING

12 JULY 2021

<b>JANUARY 2021</b>					
<b>Date of Hearing</b>	<b>App. No.</b>	<b>Property/Ward</b>	<b>Proposal</b>	<b>Council Decision/Nature of Appeal</b>	<b>VCAT Decision</b>
	<b>No matters determined by Council delegates were heard in January 2021</b>				

PLANNING COMMITTEE MEETING

12 JULY 2021

FEBRUARY 2021					
Date of Hearing	App. No.	Property/Ward	Proposal	Council Decision/Nature of Appeal	VCAT Decision
17/02/2021	D/790/2018	600-606 Plenty Road, Preston  Central	Proposed use of land as a food and drink premises together with part demolition of the existing building, the construction of buildings and works for 11 dwellings and a reduction of the required car parking provision	Refusal – Applicant Appeal	Withdrawn
<b>Result</b>	This application was withdrawn by the Applicant prior to the scheduled hearing.				
19/02/2021 (Compulsory Conference)	D/668/2019	78 Shaftesbury Parade, Thornbury  South West	Medium density housing development comprising: •Double storey extension to the existing dwelling; •Construction of two triple storey dwellings (where the third storeys are roof decks and covered access areas) to the rear of the existing dwelling.	Notice of Decision – Objector Appeal	Council's decision varied (by consent) – Permit granted
<b>Result</b>	The parties were able to reach agreement as to a suitable form of development, as such, they were in position that Council's decision could be varied by consent.				
24/02/2021	D/762/2018	560-562 High Street, Preston  Central	Proposed five (5) storey (plus basement and rooftop terrace) mixed use development and a waiver of the car parking requirements	Refusal – Applicant Appeal	Council decision set aside
<b>Result</b>	A permit was granted subject to some additional conditions.				
26/02/2021	D/522/2019	14-16 Wardrop Grove, Northcote  Rucker	A 2-3 storey building comprising seven (7) dwellings	Conditions Appeal	Council's decision varied (by consent) – Permit granted
<b>Result</b>	The parties were able to reach agreement prior to the hearing as to a suitable form of development, as such, Council's decision was varied by way of consent order.				

PLANNING COMMITTEE MEETING

12 JULY 2021

MARCH 2021					
Date of Hearing	App. No.	Property/Ward	Proposal	Council Decision/Nature of Appeal	VCAT Decision
1/03/2021	D/533/2019	191 Bell Street, Preston  Central	A two (2) storey rooming house comprising 14 rooms.	Refusal – Applicant Appeal	Council's Decision set aside.
<b>Result</b>	The tribunal determined that the proposal was acceptable.				
3/03/2021 (Compulsory Conference)	D/349/2020	171-175 High Street, Northcote  South	Proposed buildings and works to the existing building including extensions and an additional two storey's use of the for offices and a reduction in the standard car parking requirement	Failure Appeal – Council has formed a position to oppose the application.	
<b>Result</b>	The matter did not settle.				
4/03/2021 (Compulsory Conference)	D/188/2019/A	6A Christmas Street, Northcote  South Central	Permit No. D/188/2019 for Construction of a single storey dwelling, a reduction in the standard car parking requirement and associated works	Refusal – Applicant Appeal	
<b>Result</b>	The matter did not settle.				
5/03/2021	D/211/2019	232 Albert Street, Reservoir  North Central	A medium density housing development comprising the construction of four (4) dwellings over three (3) levels and alteration of access to road zone 1	Refusal – Applicant Appeal	Council's decision set aside (by consent) – Permit granted
<b>Result</b>	The parties were able to reach agreement prior to the hearing as to a suitable form of development, as such, Council's decision was varied by way of consent order.				



PLANNING COMMITTEE MEETING

12 JULY 2021

MARCH 2021					
Date of Hearing	App. No.	Property/Ward	Proposal	Council Decision/Nature of Appeal	VCAT Decision
9/03/2021	D/384/2019	279-281 St Georges Road, Northcote  South West	Buildings and works comprising the construction of a four (4) storey residential development (28 dwellings) and alteration of access to a road in a Road Zone Category 1 in accordance with the endorsed plans.	Notice of Decision – Objector Appeal.	Council's decision varied (by consent) – Permit granted
<b>Result</b>	The parties were able to reach agreement as to a suitable form of development, as such, Council's decision was varied by way of consent order.				
11/03/2021 (Compulsory Conference)	D/327/2019	72 James Street, Northcote  South	In accordance with the endorsed plans: • Demolish the existing dwelling, and • Construct a medium density housing development comprised of two (2) double storey dwellings.	Notice of Decision – Objector Appeal.	
<b>Result</b>	The matter did not settle.				
18/03/2021 (Compulsory Conference)	D/311/2003/A	73A Victoria Road, Northcote  South Central	A medium density housing development comprised of a double storey dwelling to the rear of an existing dwelling	Notice of Decision – Objector Appeal.	
<b>Result</b>	The matter did not settle.				
18/03/2021	D/677/2019	71 Northernhay Street, Reservoir  North Central	Proposed construction of two (2) double storey dwellings with double garage and studio above to rear of site	Refusal – Applicant Appeal	Council's decision set aside – Permit granted
<b>Result</b>	The Tribunal determined that it was appropriate to issue a Permit subject to conditions.				

PLANNING COMMITTEE MEETING

12 JULY 2021

MARCH 2021					
Date of Hearing	App. No.	Property/Ward	Proposal	Council Decision/Nature of Appeal	VCAT Decision
19/03/2021	D/284/2019	53 William Street, Preston  Central	Proposed construction of a second dwelling to the rear of the existing dwelling on land within a residential zone	Refusal – Applicant Appeal	Councils Decision to refuse the application is affirmed.
<b>Result</b>	No permit issued.				
19/03/2021	D/33/2019/A	158 Mansfield Street, Thornbury  South Central	Amendment to Planning Permit D/33/2019 as follows: <ul style="list-style-type: none"> <li>•Relocate the rear extension so that the new works are along the eastern boundary;</li> <li>•Reduce the length of wall along the western boundary to accommodate a west-facing courtyard;</li> <li>•Create a new room to the rear (south) of the block for the purposes of a main bedroom;</li> <li>•Create a car space within the front setback;</li> <li>•Changes to the internal layout changes.</li> </ul>	Refusal – Applicant Appeal.	Withdrawn
<b>Result</b>	This application was withdrawn by the Applicant prior to the scheduled hearing.				

PLANNING COMMITTEE MEETING

12 JULY 2021

APRIL 2021					
Date of Hearing	App. No.	Property/Ward	Proposal	Council Decision/Nature of Appeal	VCAT Decision
6/4/2021 (Compulsory conference)	D/668/2019	552-558 Plenty Road Preston South	Use of land for the purpose of a childcare centre (100 places) and food and drink premises; construction of a four-storey building with food and drink premises at Ground Floor, five (5) dwellings and childcare centre at the upper floors; a reduction in the car parking requirement and alteration of access to a road in a Road Zone Category 1	Failure Appeal – Council has formed a position to oppose the application	Withdrawn
<b>Result</b>	This application was withdrawn by the Applicant prior to the scheduled hearing.				
12/04/2021 (Compulsory conference)	D/311/2003/A	73A Victoria Road Northcote South	A medium density housing development comprised of a double storey dwelling to the rear of an existing dwelling	Notice of Decision – Objector Appeal.	Vacated
<b>Result</b>	Compulsory conference was vacated				
16/4/2021	D/601/2019	12 Faye Street Reservoir La Trobe	Construction of five dwellings on the lot in an RGZ3/DDO19	Refusal – Applicant Appeal	Decision adjourned for expert evidence
<b>Result</b>	Decision adjourned for expert evidence				

PLANNING COMMITTEE MEETING

12 JULY 2021

APRIL 2021					
Date of Hearing	App. No.	Property/Ward	Proposal	Council Decision/Nature of Appeal	VCAT Decision
21/04/2021	D441/2019	108 Darebin Boulevard Reservoir Latrobe	A medium density housing development comprised of the construction of five (5) triple storey dwellings	Refusal – Applicant Appeal Plans amended to address grounds of refusal	Councils Decision Upheld
<b>Result</b>	No Permit Issued. Issues surrounding visual bulk, insufficient and overshadowed open space provision, excessive screening and overdevelopment of the site.				
21/04/2021	D/511/2011/A	76 & 78 Hutton Street Thornbury  Cazaly	The amendments to the permit which have been refused are the following changes to the endorsed plans: <ul style="list-style-type: none"> <li>• Enclosed garages to both dwellings</li> <li>• Non-permeable concrete driveways</li> </ul>	Refusal – Applicant Appeal	Awaiting decision
<b>Result</b>					
21/04/2021	D/601/2019	61 Edwardes Street Reservoir La Trobe	<ul style="list-style-type: none"> <li>• A medium density housing development comprising the construction of 15 triple storey dwellings and one (1) double storey dwelling and a basement level; and</li> <li>• Reduction of the car parking requirements</li> </ul>	Notice of Decision – Objector Appeal	Vacated
<b>Result</b>	Settled by consent. Final Order dated 21 December 2020				

PLANNING COMMITTEE MEETING

12 JULY 2021

APRIL 2021					
Date of Hearing	App. No.	Property/Ward	Proposal	Council Decision/Nature of Appeal	VCAT Decision
23/04/2021	D/349/2020	35 Dean Street, Preston  Cazaly	A medium density housing development comprised of the construction of three (3) double-storey dwellings, in accordance with the endorsed plans.		Vacated
<b>Result</b>	Withdrawn by applicant				
26/04/2021	D/72/2020	1 Taylor Avenue Reservoir  West	Construction of a double storey dwelling to the rear of the existing dwelling,	Notice of Decision – Objector Appeal	Vacated
<b>Result</b>					

MAY 2021					
Date of Hearing	App. No.	Property/Ward	Proposal	Council Decision/Nature of Appeal	VCAT Decision
7/05/2021	D/188/2019	6A Christmas Street, Northcote South Central	Permit No. D/188/2019 for Construction of a single storey dwelling, a reduction in the standard car parking requirement and associated work.	Refusal – Applicant Appeal	Awaiting decision
<b>Result</b>					
12/05/2021	D/789/2018	566-568 High Street, Preston  Cazaly	Proposed construction of a five-storey (plus basement and rooftop terrace) mixed use development and a waiver of the car parking requirements	Refusal – Applicant Appeal	Application withdrawn by applicant.

PLANNING COMMITTEE MEETING

12 JULY 2021

MAY 2021					
Date of Hearing	App. No.	Property/Ward	Proposal	Council Decision/Nature of Appeal	VCAT Decision
<b>Result</b>	Application withdrawn.				
13/05/2021	D/349/2020	171-175 High Street, Northcote  South	Proposed buildings and works to the existing building including extensions and an additional two storey's use of the for offices and a reduction in the standard car parking requirement	Refusal – Applicant Appeal	Awaiting decision
<b>Result</b>					
12/05/2021 (Compulsory Conference)	D/264/2020	133A Victoria Road, Northcote  South	Extension to a dwelling on a lot containing two dwellings	Notice of Decision – Objector Appeal	
<b>Result</b>	Matter settled.				
27/05/2021 (Compulsory Conference)	D/472/2020	11 Highview Road, Preston  Central	Use of land (Lots 2 & 3 on PS007741 and Lots 1 & 2 on TP868744T) for a place of worship and buildings and works for the construction of a reconfigured and extended carpark	Notice of Decision – Objector Appeal	
<b>Result</b>	Matter settled				
31/05/2021	VS/127/2020	34 Crawley Street, Reservoir  West	Two (2) Lot Subdivision as shown on the plans accompanying the application	Permit issued – Applicant Appeal against conditions	Councils Decision is affirmed.
<b>Result</b>	Councils decision is affirmed by the Tribunal.				
31/05/2021	D/933/2001	205 Bell Street & 5 Harold Street, Preston  Cazaly	Proposed Student Accommodation and associated car park	Refusal – Applicant Appeal	Adjourned to August 2021.
<b>Result</b>					

PLANNING COMMITTEE MEETING

12 JULY 2021

JUNE 2021					
Date of Hearing	App. No.	Property/Ward	Proposal	Council Decision/Nature of Appeal	VCAT Decision
3/6/2021	D/356/2006/G	205 Bell Street, Preston  Cazaly	Use and develop the land for the purpose of a residential building (student accommodation), office, conference centre, food and drink premises, licensed 140 seat restaurant and motel and to waive the car and bicycle parking requirements	Refusal – Applicant Appeal	Adjourned to August 2021.
<b>Result</b>					
3/6/2021 (Compulsory Conference)	D/407/2020	43 O'Connor Street, Reservoir  North West	Construction of 3 double storey dwellings.	Notice of Decision – Objector Appeal	Additional CC scheduled for 30/6/2021
<b>Result</b>	Did not settle				
26/04/2021	D/809/2018	771 Plenty Road Reservoir  North Central	A residential development comprised of the construction of nine (9) dwellings and alteration of access to a road in a Road Zone Category 1	Refusal – Applicant Appeal	Awaiting Decision
<b>Result</b>					
9/06/2021	D/511/2019	85 Summerhill Road Reservoir  Cazaly	Construction of a three (3) storey apartment building comprising four (4) dwellings	Refusal – Applicant Appeal	Council's decision set aside – Permit granted
<b>Result</b>	Permit Issued.				
11/06/2021	D/243/2020	31 Fairfield Road, Fairfield  South Central	Construction of 2 dwellings.	Refusal – Applicant Appeal	Adjourned until 25 October 2021
<b>Result</b>					
15/06/2021	D/384/2019	279-281 St Georges Road	Buildings and works comprising the construction of a four (4) storey	Notice of Decision – Objector Appeal	Vacated

PLANNING COMMITTEE MEETING

12 JULY 2021

JUNE 2021					
Date of Hearing	App. No.	Property/Ward	Proposal	Council Decision/Nature of Appeal	VCAT Decision
		North Central	residential development (28 dwellings) and alteration of access to a road in a Road Zone Category 1 in accordance with the endorsed plans.		
<b>Result</b>	Matter settled at CC				
17/06/2021	D/49/2020	38 Mansfield Street, Thornbury Rucker	Proposed construction of two (2) triple storey dwellings	Refusal – Applicant Appeal	Awaiting Decision
<b>Result</b>					
17/06/2021 (Compulsory Conference)	D/696/2020	48 High Street, Northcote South	Proposed multi storey mixed-use development (6 Storey)	Refusal – Applicant Appeal	
<b>Result</b>	Did not settle				
18/06/2021 (Compulsory Conference)	D/501/2020	86-88 Summerhill Road, Reservoir North Central	Proposed construction of twelve (12) three storey dwellings	S79 – Applicant Appeal	Did not settle. A further CC scheduled for 12/7/2021
<b>Result</b>					
30/06/2021	D/407/2020	43 O'Connor Street Reservoir North west	Construction of 3 double storey dwellings	Notice of Decision – Objector Appeal	
<b>Result</b>	Matter Settled				
30/06/2021	D/311/2003	73A Victoria Road, Northcote South	A medium density housing development comprised of a double storey dwelling to the rear of an existing dwelling	Notice of Decision – Objector Appeal	Awaiting Decision
<b>Result</b>					
30/06/2021	D/809/2018	72 James Street, Northcote	In accordance with the endorsed plans:	Refusal – Applicant Appeal	Vacated



PLANNING COMMITTEE MEETING

12 JULY 2021

JUNE 2021					
Date of Hearing	App. No.	Property/Ward	Proposal	Council Decision/Nature of Appeal	VCAT Decision
		Rucker	<ul style="list-style-type: none"> <li>• Demolish the existing dwelling, and</li> <li>• Construct a medium density housing development comprised of two (2) double storey dwellings.</li> </ul>		
<b>Result</b>	Matter settled at CC				

PLANNING COMMITTEE MEETING

12 JULY 2021

### Planning Committee Decisions before VCAT

SEPTEMBER 2020					
Date of Hearing	App. No.	Property/Ward	Proposal	Council Decision/Nature of Appeal	VCAT Decision
No matters determined by the Committee were heard in September 2020					

PLANNING COMMITTEE MEETING

12 JULY 2021

OCTOBER 2020					
Date of Hearing	App. No.	Property/Ward	Proposal	Council Decision/Nature of Appeal	VCAT Decision
7/10/2020 (Compulsory Conference)	D/722/2018	800-802 & 820 Plenty Road, Reservoir  Cazaly	Use and development of the land for the purpose of a retirement village and residential aged care facility, removal of native vegetation and alteration of access to a road in a Road Zone Category 1	Committee (contrary to Officer Recommendation)	Council's decision set aside (by consent) – Permit granted
<b>Result</b>	This matter was listed for an administrative mention on 15 December 2020 where Council advised the Tribunal of the outcome of the 14 December 2020 Planning Committee meeting.				

PLANNING COMMITTEE MEETING

12 JULY 2021

NOVEMBER 2020					
Date of Hearing	App. No.	Property/Ward	Proposal	Council Decision/Nature of Appeal	VCAT Decision
30/11/2020 (Compulsory Conference)	D/550/2019	35 Dean Street, Preston  Cazaly	A medium density housing development comprised of the construction of three (3) double-storey dwellings, in accordance with the endorsed plans.	Notice of Decision (in line with Officer Recommendation) – Objector Appeal	
<b>Result</b>	The matter did not settle.				

PLANNING COMMITTEE MEETING

12 JULY 2021

DECEMBER 2020					
Date of Hearing	App. No.	Property/Ward	Proposal	Council Decision/Nature of Appeal	VCAT Decision
7/12/2020	D/722/2018	800-802 & 820 Plenty Road, Reservoir Cazaly	Use and development of the land for the purpose of a retirement village and residential aged care facility, removal of native vegetation and alteration of access to a road in a Road Zone Category 1	Committee (contrary Officer Recommendation)	Adjourned
<b>Result</b>	Hearing date vacated.				
8/12/2020	EOT/31/2020 and EOT/32/2020	Preston Market, Cramer & Mary Street Cazaly	Extension of Time (Refusal)	Committee (in line with Officer Recommendation)	Council's decision set aside – Extension of Time granted
<b>Result</b>	In balancing the relevant considerations, VCAT determined that there was insufficient reason to refuse the request for an extension based on the relevant legal principles. In particular, VCAT did not share Council's concern that the recent strategic work undertaken by the VPA on new planning controls or the LXP's work on elevated rail would prejudice the future vision for the site. Nor did VCAT share Council's concern that the Applicant was 'sitting on their hands' to see what the updated planning controls would entail. It was also noted that the circumstances of the COVID-19 Global Pandemic weighed in favour of extending the permits, because in VCAT's view, this may have contributed to the delays in commencing the development.				

PLANNING COMMITTEE MEETING

12 JULY 2021

JANUARY 2021					
Date of Hearing	App. No.	Property/Ward	Proposal	Council Decision/Nature of Appeal	VCAT Decision
16/12/2021 (Preliminary Hearing)	D/266/2019	10 Benambra Street, Preston  West	Development of seven (7) double storey (plus basement) dwellings.	Committee (contrary Officer Recommendation)	Application Allowed
<b>Result</b>	The purpose of this preliminary hearing was to consider a question of law raised by an objector as to whether VCAT had jurisdiction to consider the Application having regard to Chapter III of the Australian Constitution. The question followed a recent decision by the Victorian Supreme Court of Appeal, which found that VCAT would not have jurisdiction in certain circumstances where Chapter III of the Australian Constitution was a relevant consideration to the 'matter'. The Tribunal found that current application requires the exercise of the Tribunal's administrative power and not judicial power, therefore Chapter III is not a relevant consideration. The Tribunal found that there is no jurisdictional issue preventing VCAT from hearing this Application.				

PLANNING COMMITTEE MEETING

12 JULY 2021

FEBRUARY 2021					
Date of Hearing	App. No.	Property/Ward	Proposal	Council Decision/Nature of Appeal	VCAT Decision
No matters determined by the Committee were heard in February 2021					

PLANNING COMMITTEE MEETING

12 JULY 2021

<b>MARCH 2021</b>					
<b>Date of Hearing</b>	<b>App. No.</b>	<b>Property/Ward</b>	<b>Proposal</b>	<b>Council Decision/Nature of Appeal</b>	<b>VCAT Decision</b>
	<b>No matters determined by the Committee were heard in March 2021</b>				



PLANNING COMMITTEE MEETING

12 JULY 2021

APRIL 2021					
Date of Hearing	App. No.	Property/Ward	Proposal	Council Decision/Nature of Appeal	VCAT Decision
7/04/2021	D/266/2019	10 Benambra Street, Preston  West	Development of seven (7) double storey (plus basement) dwellings.	Committee (contrary Officer Recommendation)	Awaiting decision
<b>Result</b>	Decision Pending				
19/04/2021	D/722/2018	800-802 & 820 Plenty Road, Reservoir Cazaly	Use and development of the land for the purpose of a retirement village and residential aged care facility, removal of native vegetation and alteration of access to a road in a Road Zone Category 1	Committee (contrary Officer Recommendation)	Vacated
<b>Result</b>	Hearing vacated as parties consented at compulsory conference and VCAT order a permit to be issued on 22 December 2020.				

PLANNING COMMITTEE MEETING

12 JULY 2021

MAY 2021					
Date of Hearing	App. No.	Property/Ward	Proposal	Council Decision/Nature of Appeal	VCAT Decision
3/05/2021	POD/1/2006/H	205 Bell Street, Preston  Cazaly	Use and develop the land for the purpose of a residential building (student accommodation), office, conference centre, food and drink premises, licensed 140 seat restaurant and motel and to waive the car and bicycle parking requirements	Committee (contrary Officer Recommendation)	Awaiting decision
<b>Result</b>	Decision Pending				
14/05/2021	D/550/2019	35 Dean Street Preston Cazaly	A medium density housing development comprised of the construction of three (3) double-storey dwellings, in accordance with the endorsed plans.	Committee (inline Officer Recommendation)	Awaiting decision
<b>Result</b>	Awaiting Decision				

PLANNING COMMITTEE MEETING

12 JULY 2021

JUNE 2021					
Date of Hearing	App. No.	Property/Ward	Proposal	Council Decision/Nature of Appeal	VCAT Decision
18/06/2021	POD/1/2019	1, 3, 5, 7, 9, 11, & 13 Railway Place, 1-6, 8- 15, 17, 19, 21, 23-27 Stokes Street, 1-14 & 16 Penola Street, 56- 66 Showers, 1-12 Kenwood Court, 20-30 Oakover Road, Preston  Cazaly	Application for a Development Plan.	Refusal – Applicant Appeal	Withdrawn
<b>Result</b>	Withdrawn				


All matters listed until 30 June 2021

**7. CONSIDERATION OF REPORTS CONSIDERED CONFIDENTIAL**


**8. CLOSE OF MEETING**

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